

**DRAFT**

# Land Use Recovery Plan

Te Mahere Whakahaumanu Tāone

## Volume 2

### Appendices



## ***Appendix 1 – Minister’s Direction to develop a Land Use Recovery Plan***

### **Direction to Canterbury Regional Council to develop a Land Use Recovery Plan for greater Christchurch**

Pursuant to section 16(4) of the Canterbury Earthquake Recovery Act 2011 (“the Act”), the Minister for Canterbury Earthquake Recovery gives the following notice.

#### **Notice**

##### **1.0. Title**

This notice is a direction to the Canterbury Regional Council to develop a Land Use Recovery Plan.

##### **2.0. Direction**

Pursuant to section 16(1) of the Act, I direct the Canterbury Regional Council to prepare a Recovery Plan to deal with the following matters within greater Christchurch:

##### **2.1. Geographic extent**

The Recovery Plan will focus on the metropolitan urban area and towns stretching from Lincoln and Rolleston in the south to Rangiora and Woodend in the north, including Christchurch City (and to the extent necessary, on across-boundary matters relating to residential land use and transport, this would include the area covered by the Christchurch Central Recovery Plan).

##### **2.2. Matters to be dealt with –**

- A. Identification of the location, type and mix of residential and business activities within specific geographic areas necessary for earthquake recovery, including
- i. the priority areas to support recovery and rebuilding in the next 10 to 15 years, and
  - ii. enabling and informing the sequencing and timescales for the delivery of infrastructure and transport networks and hubs to support the priority areas.
- B. The Recovery Plan will make changes necessary for earthquake recovery to residential and business land use policy and planning provisions and related funding instruments, in order to provide for:
- iii. the matters in A; and
  - iv. a diverse range of housing types, including social and affordable housing.
- C. To the extent possible in the timeframe for the development of the Recovery Plan, the Recovery Plan will make changes or identify a programme of further work to be undertaken before



changes are made, to residential and business land use policy and planning provisions and related funding instruments, necessary for earthquake recovery, in order to:

- v. provide for intensification of use and comprehensive development on suitable brownfield areas;
- vi. support the recovery and rebuilding of the network of centres of activity such as the central city, suburban and satellite town centres.

D. In making these changes consideration is to include avoiding or mitigating the changed or heightened risks of natural hazards.

**2.3.** The Recovery Plan must state the funding implications of its implementation and indicate the proposed funding sources. This may include the identification of options for different funding sources.

**2.4.** If the Canterbury Regional Council considers that amendments to documents and instruments prepared under the Resource Management Act 1991, Local Government Act 2002 and the Land Transport Management Act 2003 may be necessary to implement the Recovery Plan it must state this in the Plan and describe the nature of those amendments. The Recovery Plan may identify programmes of further work to be undertaken before amendments to address specific matters are proposed to it.

**2.5.** The Canterbury Regional Council must ensure provisions of the Recovery Plan are consistent with the Christchurch Central Recovery Plan. Consideration of across boundary issues relating to settlement patterns and transport matters must be developed consistently with the Christchurch Central Recovery Plan. Amendments to the Christchurch Central Recovery Plan shall not be made.

**2.6.** Canterbury Regional Council must have regard to any other Recovery Plans that are in force or being developed. It must consult the Canterbury Earthquake Recovery Authority to ensure that the Land Use Recovery Plan is consistent with, and supports, existing or developing Recovery Plans.

**2.7.** In developing the Recovery Plan, the Canterbury Regional Council will consider the relationship of the Plan with other recovery decision-making processes. The Recovery Plan will inform decision-making in relation to infrastructure provision and associated community services such as public transport, health services, educational facilities and recreational facilities and spaces. The Recovery Plan may not direct or implement changes to these matters.

**2.8.** The following matters may be considered by the Canterbury Regional Council in the development of the Recovery Plan but will not be specifically addressed in the Recovery Plan: recovery of non-land use resources such as water, air, soil, minerals and energy and all forms of plants and animals (being all other “natural and physical resources” as defined in section 2 of the RMA except for land and structures); future use of “red-zoned” land; and long-term provisions for growth and development in greater Christchurch.

**2.9.** In this Direction, “business” or “business activities” means activities that include retail, office, industrial and other commercial and any ancillary activity.

### **3.0. Development of Recovery Plan**

Pursuant to section 19(1) of the Act, and having had regard to the matters in section 19(2) of the Act, I have determined that the Land Use Recovery Plan is to be developed in the following manner; including the following consultation requirements:

**3.1.** The Canterbury Regional Council must develop the Recovery Plan through a collaborative multi-agency approach with Christchurch City Council, and Selwyn and Waimakariri District Councils, Te Rūnanga o Ngāi Tahu, New Zealand Transport Agency and Canterbury Earthquake Recovery Authority. This is to ensure that the draft Recovery Plan reflects their needs and perspectives.

**3.2.** Canterbury Regional Council must ensure the draft Recovery Plan reflects to the extent possible the views of greater Christchurch communities generally. Canterbury Regional Council must use targeted stakeholder consultation sessions, at least five community based “open forum sessions” and consultation in a manner agreed with Te Rūnanga o Ngāi Tahu, to achieve this. The Canterbury Regional Council is to determine the exact nature, timing and location of the consultation sessions, except public consultation must not occur over the Christmas/New Year period. There is no requirement to hold public hearings.

**3.3.** The Canterbury Regional Council must prepare and publish a report on all consultation undertaken, including a summary of what was heard during consultation and how it influenced the draft Recovery Plan. The Canterbury Regional Council must provide me with a copy of the consultation summary report when sending me the draft Recovery Plan.

**3.4.** The Canterbury Regional Council must develop a draft Land Use Recovery Plan within seven months after the date of this direction.

**3.5.** The Canterbury Regional Council must ensure that all public information relating to the draft Recovery Plan is freely and easily available.

**3.6.** As required under section 20 of the Act, I intend to publicly notify the draft Land Use Recovery Plan. I will do this after receiving the draft Recovery Plan from Canterbury Regional Council. When I notify the draft Recovery Plan, I will invite comments from the public within a period of a minimum of four weeks.

### **4.0. Other Recovery Plans**

The Christchurch Central Recovery Plan is the only other Recovery Plan in force.

Dated at Wellington this 6th day of November 2012.

**HON GERRY BROWNLEE**

Minister for Canterbury Earthquake Recovery



## **Appendix 2: Amendments to the Canterbury Regional Policy Statement**

The changes proposed in this Appendix are to insert a new Chapter 6 into the Canterbury Regional Policy Statement that provides statutory provisions for enabling rebuilding and redevelopment, including priority areas. This will achieve an efficient, sustainable, functional and desirable greater Christchurch. The chapter will identify provisions including:

- The location, type and mix of residential and business activities, within the geographical extent of greater Christchurch, including priority areas for development through to 2028;
- The network of Key Activity Centres needed to provide a focus for commercial activity, medium-density housing, community facilities, public greenspace and public and active transport networks;
- The methods to ensure integration of land use with natural, cultural, social and economic outcomes, transport and other infrastructure including stormwater management planning;
- Areas where rebuilding and development may not occur within the period of the Recovery Plan, including recognizing specific constraints including natural hazards and environmental constraints such as the protection of people's health and well-being relating to development under the airport noise contour, with an exception for Kaiapoi reflecting the special circumstances following the earthquakes;
- Minimum residential densities relating to different housing locations, Greenfield and Brownfield;
- Require urban design matters to be addressed at a range of scales for business, housing and mixed-use development; and
- Development of housing options on Maori Reservation 873.

## CHAPTER 6 – Recovery and rebuilding of Greater Christchurch

### INTRODUCTION

The insertion of this chapter into the CRPS was directed by the Minister for Earthquake Recovery in the Land Use Recovery Plan for Greater Christchurch and under section 27 of the Canterbury Earthquake Recovery Act 2011. The chapter is consistent with the Recovery Strategy for Greater Christchurch and the Christchurch Central Recovery Plan, and supports their implementation.

The geographic extent of Greater Christchurch, for the purposes of this chapter, is shown in Map A. The Ashley River/Rakahuri lies to the north, the Waimakariri River cuts through the centre, the Port Hills and Selwyn River lie to the south and Pegasus Bay and Lyttelton Harbour/Whakaraupo are to the east. It excludes the area of Banks Peninsula as indicated in Map A. In Waimakariri District, Two Chain Road is the western boundary of the sub-region and in Selwyn District the western boundary follows Highfield and Station Roads.

Chapter 6 provides a resource management framework for the recovery of Greater Christchurch, to enable and support earthquake recovery and rebuilding, including restoration and enhancement, for the area through to 2028. Recovery in Greater Christchurch is also supported by provisions in Chapter 5 – Land use and infrastructure that are notated “Entire Region”. The provisions in the remainder of the Canterbury Regional Policy Statement (CRPS) also apply.

The purpose of Chapter 6 is to enable recovery by providing for development in a way that achieves the purpose of the Resource Management Act 1991.

#### **[Box out in text]**

**Rebuilding** includes:

- (a) extending, repairing, improving, subdividing, or converting any land, infrastructure, or other property; and
- (b) rebuilding communities.

**Recovery** includes restoration and enhancement.

Chapter 6 sets the objectives to be achieved for Greater Christchurch during the recovery period, which includes the intended land use distribution for Greater Christchurch for the planning period through to 2028. In particular, the land use and infrastructure framework includes:

- the location, type and mix of business and residential activities within specific geographical areas to support recovery and rebuilding;
- priority areas for development; and
- provision for the integration of infrastructure and transport networks to support recovery.

It also determines the areas where rebuilding and development should not occur, recognising specific environmental constraints including natural hazards. In the period through to 2028, it is essential for recovery that resources within local

government are focused on delivering the outcomes based on the land use and infrastructure framework.

The provisions of Chapter 6 deliver a framework that will ensure Greater Christchurch recovers to be a well-planned, attractive, prosperous, culturally rich and socially diverse modern metropolitan area. A key component of the recovery will be ensuring that the Greater Christchurch area maximises opportunities for its residents, service providers and visitors through better transport, supporting businesses and services, and attracting investment, all operating within a safe and healthy environment. This also means efficiently rebuilding and providing new infrastructure, optimising the use of existing infrastructure, protecting its operation, and making sure that development properly integrates with it.

The Recovery Strategy, as well as the Land Use Recovery Plan, contain the following goals for the built environment which are relevant to this chapter:

*Develop resilient, cost effective, accessible and integrated infrastructure, buildings, housing and transport networks - by:*

- *coordinating and prioritising infrastructure investment that effectively contributes to the economy and community during recovery and into the future;*
- *supporting innovative urban design, buildings, technology and infrastructure to redefine Greater Christchurch as a safe place built for the future;*
- *rebuilding infrastructure and buildings in a resilient, cost-effective and energy-efficient manner;*
- *developing a transport system that meets the changed needs of people and businesses and enables accessible, sustainable, affordable and safe travel choices;*
- *zoning sufficient land for recovery needs within settlement patterns consistent with an urban form that provides for the future development of Greater Christchurch;*
- *having a range of affordable housing options connected to community and strategic infrastructure that provides for residents participation in social, cultural environmental and economic activities; and*
- *drawing on sound information about ongoing seismic activity and environmental constraints, including other natural hazards and climate change.*

The CRPS needs to provide an overarching land use framework for Greater Christchurch, in order to be consistent with the Recovery Strategy and the Land Use Recovery Plan, and to achieve the purpose of the Resource Management Act. Provision of the framework will ensure the efficient and effective application of limited financial and human resources, enable timely recovery decisions to be made with confidence, deliver value for money and make the most of the opportunities for the redevelopment of damaged urban areas and infrastructure, and the development of new areas. Failure to provide guidance to the community and markets could result in decision-making with short-term horizons and lead to the inefficient and ineffective allocation of resources.

Land use planning processes need to support, facilitate and enable recovery of Greater Christchurch in an efficient and environmentally sustainable way. Plans, policies, processes and the right incentives need to be in place to respond to the impacts and effects arising from the earthquakes. This includes responses to issues created by Red Zone decisions and the loss of accommodation, business

relocations, emerging building trends, changing demographics, changes in flood levels, sea-level rise and liquefaction risk, and many other issues that are influencing the speed of recovery.

The recovery of Greater Christchurch will involve large investments in core infrastructure by central government, local government, and other infrastructure providers and developers. It is essential that decision-making on the location and timing of land use development is taken in the context of a thorough understanding of the practical implications of infrastructure supply. The availability of public funds and associated human resources is insufficient to meet all development demands that might occur concurrently. The pattern of development during the recovery period needs to focus on those areas that are the most efficient to service. A sequenced approach to the development of greenfield land is required. Sequencing of delivery of infrastructure will be left to territorial authorities through their long term and annual planning processes. In relation to transport infrastructure and services, the likelihood of lower levels of service is acknowledged as recovery takes place.

For discussion and provisions regarding specific resource matters (for example energy, biodiversity and landscape), further reference should be made to other chapters in the CRPS.

## 6.1 ISSUES

### Issue 6.1.1 – Enabling recovery, rebuilding and development

**How to provide certainty to the community and businesses around how Greater Christchurch will accommodate expected population and household relocation and growth, housing needs and economic activity during the recovery period in an efficient and environmentally sustainable manner. This includes providing for a diverse community with a range of incomes, needs and business types.**

#### Explanation

While the needs for Greater Christchurch in the long term are important, recovery and rebuilding in the short term is critical.

The community requires certainty around where recovery development will take place during the recovery period to enable planning for delivery of infrastructure and protection of key resources such as strategic transport networks, water supply, and other significant natural and physical resources. In particular, it is important that resources are directed to specific geographic areas, to enable efficient and effective public investment in strategic, network and social infrastructure. Without certainty and forward planning, recovery for the Central City, Key Activity Centres, and neighbourhood centres will be slower, and will result in inefficient investment decisions being made by infrastructure providers and developers, and incur unnecessary additional costs for local authorities.

When making decisions around accommodating residential and business relocation and growth over the recovery period, it is recognised that there are a range of needs among the community; both in terms of residential accommodation and business provision. A spectrum of housing types needs to be available to accommodate people on different levels of income and with different requirements, including a possible temporary working population, as well as providing for diversity among the

different business types that operate within Greater Christchurch, from small offices and retail, through to large industrial sites. It is important that the functions of different types of business zones are protected to ensure that lower value industrial land is not competing with potentially higher value office and retail development.

### **Issue 6.1.2 - Adverse effects arising from development**

**Development can result in adverse effects on the environment, which if not identified and avoided, remedied or mitigated where appropriate, could result in inappropriate outcomes for the region's natural and physical resources, and reduce Greater Christchurch's resilience and ability to provide for the needs of people and communities. Poorly planned development can increase risk from natural hazards and the effects of climate change, create resource use conflicts, increase community isolation, prevent the efficient and effective delivery of infrastructure and services, reduce economic viability and result in greater overall energy consumption.**

#### **Explanation**

There are a number of environmental challenges to providing for recovery, rebuilding and development in Greater Christchurch. These need to be recognised and provided for through a clear planning framework. For Greater Christchurch, the key resource management issues include:

- (a) The potential for contamination of Christchurch City's drinking water as a result of inappropriate development over the unconfined aquifer to the west of the city;
- (b) The negative effects of stormwater being discharged directly into waterways without land-based or wetland treatment;
- (c) The potential to compromise or lose significant natural resources, character and amenity, and lost opportunities for enhancement;
- (d) The potential to undermine the role and function of the Central City and Key Activity Centres, together with the investment made in these centres.
- (e) Risk to people and property from natural hazards such as flooding, coastal inundation, earthquakes, rockfalls / slippage or coastal erosion;
- (f) Sea-level rise and the effects of climate change
- (g) Conflicts between legitimately established activities and sensitive activities which seek to locate in proximity to these (reverse sensitivity);
- (h) Efficient and effective provision for maintenance, upgrade and delivery of services and infrastructure, in particular strategic infrastructure;
- (i) Minimising energy consumption;
- (j) Providing for development in the right place, at the right time, to meet the needs of the community;

Within these issues lies an opportunity to plan for better outcomes and make better decisions about the resources that are used for Greater Christchurch to rebuild and recover.



A key element in successful recovery and rebuilding is the recognition of existing infrastructure and service delivery. It is important that relocation and growth during the recovery period does not compromise the efficient operation of infrastructure, particularly strategic infrastructure.

Recovery can be more effectively and efficiently achieved if it supports existing centres of activity, such as the Central City, Key Activity Centres and neighbourhood centres. Some recent urban development has not utilised the opportunities available to integrate effectively with existing urban centres.

The links between the size of a future urban footprint and the level of energy used needs to be given weight. Urban growth has occurred before the earthquakes across Greater Christchurch in a way that resulted in accelerated energy use, in particular where development has created a more dispersed and fragmented urban form. Smaller and consolidated urban footprints encourage the use of less energy, especially those areas where travel patterns can be reduced through optimum relationships between residential, employment, shopping, educational and recreational activities. They also provide better opportunity and choice for people in terms of transport modes.

The costs of the infrastructure necessary to sustain rebuilding and recovery is significant. There are advantages in extending existing services and encouraging a scale of growth sufficient to promote servicing economies rather than meeting the demands of dispersed development.

Costs of growth must be factored into location decisions, as unplanned growth can impact on the rural land resource, existing rural industries and rural character.

### **Issue 6.1.3 - Transport effectiveness**

**Urban land use and development in inappropriate locations, or that is poorly integrated with transport networks, can adversely affect the efficient use, development and recovery of transport infrastructure and services, through:**

- (a) the location of residential and other sensitive activities close to strategic transport networks;**
- (b) high energy use associated with private car dependency and the need to travel greater distances;**
- (c) inefficient development and operation of strategic transport networks;**
- (d) less opportunities for modal choice for transport;**
- (e) adverse public health outcomes;**
- (f) reduced safety; and**
- (g) a failure to optimise the use of available capacity within the existing transport network**

### **Explanation**

An efficient and effective transport system through the period of the Greater Christchurch rebuild, and continuing on through its recovery, will deliver much greater economic returns to Canterbury. This period will be challenging, and it is acknowledged that there are likely to be reduced levels of service which new development is likely to exacerbate.

Helping ensure environmental sustainability from a transport perspective means that existing key transport hubs such as airports and ports must be safeguarded. Land use patterns need to be organised so that energy requirements are minimised and the efficient functioning of strategic transport networks is not compromised by traffic associated with local development or reverse sensitivity concerns. Reduced efficiency in the transport network will increase costs for businesses, as well as commuters. Poorly integrated development, or development in inappropriate locations can also affect the accessibility and uptake of public transport and active modes of transport, and combined with increased air pollution can reduce the potential for improved public health.

Well-designed development that integrates with transport networks, and which makes efficient use of existing capacity, is essential to providing for business growth and access to community services, as important components of rebuilding and recovery.

#### **Issue 6.1.4 – Amenity and urban design**

**While the speed of recovery is important, so too is the quality of the built form. Poorly designed development can adversely affect urban amenity values, rural amenity values, historic heritage, health and safety, integration with community, educational, social and commercial facilities, and overall liveability. These matters are important for retaining population and attracting skilled workers and new business opportunities. They will affect the timing and the success of recovery.**

#### **Explanation**

Sometimes the desire to rebuild quickly competes with the desire to build well or build back better. Enabling timely and appropriate development during the recovery period in a manner which does not compromise the key values of either existing or future communities is a challenge which must be recognised at a Greater Christchurch, city, district and neighbourhood level. Rebuilding can also impact on issues of significance to Ngai Tahu, affecting their relationship with ancestral lands, water, sites, waahi tapu and other taonga. In particular, good urban design will contribute to vibrant and renewed centres and help support wider well-being objectives such as quality of life, economic vitality and crime reduction.

#### **Issue 6.1.5 - Rural residential impacts**

**Rural-residential development, if unconstrained, has the potential to change the character of rural areas and to create adverse effects on established rural, farming (including agricultural research farms) and quarrying activities through reverse sensitivity. It also can result in dispersed settlement patterns, and inefficient form of development and provision of services.**

#### **Explanation**

Many of the rural western areas of Greater Christchurch remained undamaged during the earthquakes and are also located out of the area identified as being prone to liquefaction, making them more desirable locations to live. However, rural residential development is associated with reverse sensitivity effects and can give rise to requests for the extension of urban services and exacerbates dispersed settlement patterns, leading to inefficient use of infrastructure and impacts on rural production. This can lead to pressures for future urbanisation, which is difficult to achieve in an effective manner given that the land use pattern has been established for a different purpose.

## **6.2 OBJECTIVES**

### **Objective 6.2.1 - Recovery framework**

**Recovery, rebuilding and development is enabled within Greater Christchurch through a land use and infrastructure framework that:**

- (1) identifies priority areas for urban development within Greater Christchurch;**
- (2) identifies Key Activity Centres which provide a focus for high quality, and where appropriate, mixed use, development that incorporates the principles of good urban design;**
- (3) avoids urban development outside of existing urban areas or priority areas for development, unless expressly provided for in the CRPS;**
- (4) protects outstanding natural features and landscapes including those within the Port Hills and Banks Peninsula from inappropriate subdivision, use and development;**
- (5) protects and enhances indigenous biodiversity and public space;**
- (6) maintains or improves the quantity and quality of water in groundwater aquifers and surface water bodies, and quality of ambient air;**
- (7) maintains the character and amenity of rural areas and settlements;**
- (8) protects people from unacceptable risk from natural hazards and the effects of sea-level rise;**
- (9) integrates strategic and other infrastructure and services with land use development;**
- (10) achieves development that does not adversely affect the efficient operation, use, development, appropriate upgrade, and future planning of strategic infrastructure and freight hubs;**
- (11) optimises use of existing infrastructure; and**
- (12) provides for development opportunities on Māori Reserves in Greater Christchurch.**

The following policies implement this objective:

### **Principal reasons and explanation**

The purpose of this objective is to provide for an outcome where appropriate urban development is enabled within specified spatial areas around Greater Christchurch, so that resources can be focused on rebuilding, and delivering growth and recovery to those priority areas. This provides certainty to all resource users as to locations for development, enabling long term planning and funding for strategic, network and social infrastructure (such as schooling and healthcare), and protection of Greater Christchurch's natural and physical resources.

The recognition of existing constraints in terms of natural and physical resources is a critical part of successful growth management. This objective identifies the key elements of natural and physical resources in Greater Christchurch that must be protected in order to ensure that harm to the natural environment is minimised.

### **Objective 6.2.2 - Urban form and settlement pattern**

**The urban form and settlement pattern in Greater Christchurch is managed to provide sufficient land for rebuilding and recovery needs and sets a foundation for future growth, with an urban form that achieves consolidation and intensification of urban areas, and avoids unplanned expansion of urban areas, by:**

- (1) aiming to achieve the following targets for intensification as a proportion of overall growth through the period of recovery:**
  - (a) 35% averaged over the period between 2013 and 2016**
  - (b) 45% averaged over the period between 2016 to 2021**
  - (c) 55% averaged over the period between 2022 and 2028;**
- (2) providing higher density living environments including mixed use developments and a greater range of housing types, particularly in and around the Central City, in and around Key Activity Centres, and larger neighbourhood centres, and in greenfield and brownfield areas;**
- (3) reinforcing the role of the Christchurch central business district within the Greater Christchurch area as identified in the Christchurch Central Recovery Plan;**
- (4) providing for greenfield development on the periphery of Christchurch's urban area, and surrounding towns at a rate and in locations which meets anticipated demand and enables the efficient provision and use of network infrastructure;**
- (5) promoting intensification within identified urban areas and brownfield sites;**
- (6) encouraging sustainable and self-sufficient growth of the towns of Rangiora, Kaiapoi, Woodend, Lincoln, Rolleston Prebbleton and consolidation of the existing settlement of West Melton;**

- (7) Managing rural residential development outside of existing urban and priority areas; and**
- (8) Providing for development opportunities on Māori Reserves.**

The following policies implement this objective:

Policies 6.3.1, 6.3.2, 6.3.3, 6.3.4, 6.3.5, 6.3.6, 6.3.7, 6.3.8, 6.3.9, 6.3.10, 6.3.11

### **Principal reasons and explanation**

The rebuilding and recovery of Greater Christchurch relies on appropriate locations, quantity, types, and mixes of residential and business development to provide for the needs of the community.

Consolidation of existing urban settlements is the form of development most likely to minimise the adverse effects of travel for work, education, business and recreation, minimise the costs of new infrastructure and avoid adverse effects of development on sensitive landscapes, natural features and areas of high amenity. This will enable Greater Christchurch to build back better, and support the recovery of central Christchurch. Greater intensification within Christchurch's urban area through infill (particularly in the Central City, around Key Activity Centres, and neighbourhood centres) and brownfield redevelopment will reduce the need for further expansion of peripheral areas, and some intensification of the centres of smaller towns is also expected to meet changing needs. It is recognised that a significant proportion of intensification will take place in the city rather than Selwyn and Waimakariri, however the contribution of these areas to the overall growth pattern is important. The objective sets targets for the contribution of infill and intensification as a proportion of overall growth, and aligns with the growth management approach in the Greater Christchurch Urban Development Strategy. Where monitoring indicates that these levels are not being achieved, further policy responses may be required to increase intensification within existing urban areas.

Changing demographic patterns, including an ageing population and smaller households, are expected to increase the desirability of higher density housing. The demolition and ageing of housing stock in older parts of Christchurch provides an opportunity for redevelopment at higher densities and an increased range of housing types that not only provides choice for those needing to relocate, but also for future generations. Increased intensification is anticipated to occur over time as rebuild opportunities are realised, requiring appropriately located and designed greenfield development which also provides for medium density housing during the time of transition.

Rural residential development is to be managed in a way that provides for it to a limited extent during the recovery period in recognition of the desirability of providing a range of choice in housing types for those needing to relocate as a result of the earthquakes, without compromising the overall intent of consolidation in the CRPS.

Following the earthquakes and the subsequent damage and red zoning of properties, a number of Māori have sought to return to and live on the Māori Reserves set aside by the Crown in the 19<sup>th</sup> Century for the then present and future needs of local Ngāi Tahu. Providing for development opportunities on those reserves will enable the descendants of the original grantees to return and realize the original intent of those reserves.

### **Objective 6.2.3 - Sustainability**

**Recovery and rebuilding is undertaken in Greater Christchurch that:**

- (1) provides for quality living environments incorporating good urban design;**
- (2) retains identified areas of special amenity and historic heritage value;**
- (3) retains values of importance to Tangata Whenua;**
- (4) provides a range of densities and uses; and**
- (5) is healthy, environmentally sustainable, functionally efficient, and prosperous.**

The following policies implement this objective:

Policies 6.3.1, 6.3.2, 6.3.3, 6.3.4, 6.3.5, 6.3.6, 6.3.7, 6.3.8, 6.3.9, 6.3.11

#### **Principal reasons and explanation**

Recovery is defined as both “restoration and enhancement” and includes all aspects of the well-being of the communities of Greater Christchurch. Rebuilding includes extending, repairing, improving subdividing or converting land, infrastructure and other property and rebuilding communities. These actions need to maximise environmental, social, cultural and economic wellbeing over the recovery period.

Intensification and consolidation of residential development in Christchurch needs to protect areas of special amenity and historic heritage value, as these contribute to the area's identity and character. For Greater Christchurch particular attention should be paid to the provision of open space, maintenance and promotion of a sense of identity and character, and the availability of community and recreation facilities and appropriately located business centres, so as to ensure the maintenance and/or provision of high quality living environments. The focus on quality living environments also necessitates giving consideration to environmental aspects that contribute to health and wellbeing, such as energy efficiency in housing and sunlight access.

### **Objective 6.2.4 – Integration of transport infrastructure and land use**

**Prioritise the planning of transport infrastructure so that it maximises integration with the priority areas and new settlement patterns and facilitates the movement of people and goods and provision of services in Greater Christchurch, while:**

- (1) managing network congestion;**
- (2) reducing dependency on private motor vehicles;**
- (3) reducing emission of contaminants to air and energy use;**
- (4) promoting the use of active and public transport modes;**
- (5) optimising use of existing capacity within the network; and**

## **(6) enhancing transport safety.**

The following policies implement this objective:

Policies 6.3.1, 6.3.2, 6.3.3, 6.3.4, 6.3.5, 6.3.6, 6.3.7, 6.3.8, 6.3.9, 6.3.11

### **Principal reasons and explanation**

Land use patterns that are integrated with transport infrastructure minimise energy use through network optimisation, operation and maintenance, and provide for the social and economic wellbeing of the community, and peoples' health and safety. Land use patterns that are integrated with transport infrastructure create a network with shorter journey times for all modes. This integration enables greater travel mode choice. Recovery development that is not well integrated with transport infrastructure can result in increased car dependency, higher energy use, greater traffic volumes, and inefficient freight movement.

### **Objective 6.2.5 - Key activity and other centres**

**Support and maintain the existing network of centres below as the focal points for commercial, community and service activity during the recovery period:**

- (1) The Central City**
- (2) Key Activity Centres**
- (3) Neighbourhood centres.**

**These centres will be high quality, support a diversity of business opportunities including mixed-use development, and incorporate good urban design principles.**

**The development and distribution of commercial activity will avoid significant adverse effects on the function and viability of these centres.**

The following policies implement this objective:

Policies 6.3.1, 6.3.2, 6.3.4, 6.3.5, 6.3.6, 6.3.7, 6.3.8, 6.3.11

### **Principal reasons and explanation**

It is important to maintain the existing network of Key Activity Centres and Central City as focal points for business, community and service activity during the recovery phase and to support the identified priority areas. This recognises the investment made in these places, and their preference as a location for future development as businesses shift around the city over the period of recovery. In addition, by virtue of their density, mix of activities and location on strategic transport networks, Key Activity Centres support the provision of public transport and intensification of residential activity within surrounding residential areas. This intensification will provide housing choice for those households needing to relocate, and will accommodate growth during the recovery period. Inappropriate development outside of Key Activity Centres may undermine the community's investment in these existing centres and weaken the range and viability of the services they provide.

The role of neighbourhood centres is also recognised for the service role they play to local communities, and as a location for appropriate small-scale development.

## **Objective 6.2.6 - Business land development**

**Identify and provide for Greater Christchurch's land requirements for the recovery and growth of business activities in a manner that supports the settlement pattern brought about by Objective 6.2.2, recognising that:**

- (1) The greenfield business priority areas in Christchurch City provide for the accommodation of new, primarily industrial business activities, restricting the development of these areas for office and retail activities;**
- (2) Except where identified for brownfield redevelopment, areas used for existing industrial activities are to be used primarily for that purpose, rather than as a location for new office and retail activities;**
- (3) New office and retail activities are primarily to be directed to the Central City, Key Activity Centres, and neighbourhood centres;**
- (4) A range of other business activities are provided for in appropriate locations; and**
- (5) Business development adopts appropriate urban design qualities in order to retain business, attract investment, and provide for healthy working environments.**

The following policies implement this objective:

Policies 6.3.1, 6.3.2, 6.3.3, 6.3.4, 6.3.5, 6.3.6, 6.3.8, 6.3.11

### **Principal reasons and explanation**

The provision of adequate land for recovery and future business activities is important for long-term economic growth and the provision of both employment and services for the sub-region's existing and future communities. Enabling appropriate new business activity close to existing and future residential development helps achieve a greater range of travel options, promote accessibility, and reduce energy usage. The locations selected for industrial business land development are also key for rebuilding and the forward planning of the transportation network and associated freight hubs. While there is some capacity for the demand for further industrial business land to be met through the redevelopment of existing zoned land, particularly within Christchurch City, the priority areas provide for the accommodation of new, largely industrial business activities in appropriate greenfield locations. There may also be requirements for relocation of business activities to better land, or areas with potential for expansion where land is currently constrained.

In relation to different types of business land, it is important for recovery of the Central City, Key Activity Centres and neighbourhood centres that certain types of business are directed to certain locations. This will ensure that markets compete fairly for similar uses, rather than lower cost land being developed for higher value uses and reducing availability of land for activities such as industrial use. There will, however, be circumstances where redevelopment of brownfield industrial land is appropriate for a range of uses, without impacting on recovery of the Central City or Key Activity Centres as sought in Objective 6.2.5. Some retail activities will have particular locational constraints and are not suitable for centres, such as yard-based retailers and car-yards, and the need to identify a place for these activities is recognised in 6.2.6(4).



Achieving high quality urban design is as important in business areas as to other areas of the city, but there may be some business areas that require greater design focus depending on their role and function.

## **6.3 POLICIES**

### **Policy 6.3.1 – Development within the Greater Christchurch area<sup>1</sup>**

**In relation to recovery and rebuilding for Greater Christchurch:**

- (1) give effect to the urban form identified in Map A which identifies the location and extent of urban development that will support recovery, rebuilding and planning for future growth and infrastructure delivery, and the location of Key Activity Centres;**
- (2) enable development of existing urban areas and priority areas, including intensification in appropriate locations, where it supports the recovery of Greater Christchurch (subject to other relevant policies in the CRPS);**
- (3) ensure new urban activities only occur within existing urban areas or identified greenfield priority areas as shown on Map A, unless they are otherwise expressly provided for in the CRPS;**
- (4) provide for educational facilities in rural areas in limited circumstances where no other practicable options exist within an urban area; and**
- (5) avoid development that adversely affects the function and viability of, or public investment in, the Central City and Key Activity Centres.**

This policy implements the following objectives:

Objectives 6.2.1, 6.2.2, 6.2.3, 6.2.4, 6.2.5, 6.2.6

#### **Methods**

**The Regional Council:**

##### ***Will***

- (1) Have regard to Policy 6.3.1 in relation to any consents relating to urban activities outside of existing urban areas or priority areas in Greater Christchurch, and consider deferral under s91 where other consents are required from another local authority, so that the effects of a proposal can be considered together.
- (2) Initiate any changes required to Map A of the CRPS where monitoring indicates a need for further priority areas to be included to enable the release of new greenfield land, prior to the review of the CRPS.

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<sup>1</sup> Note – changes required to Policy 11.3.1, dependent on final policy

## **Territorial Authorities:**

### ***Will***

- (3) Provide for the rebuilding and recovery of Greater Christchurch in accordance with the Land Use Recovery Plan for Greater Christchurch, Policy 6.3.1 and Map A, and include objectives, policies and rules in district plans that avoid urban activities outside of existing urban, or priority areas, unless they are expressly provided for in the CRPS.
- (4) Include objectives, policies and rules, and zone land as set out in Map A, in order to achieve the residential yield as set out in Policy 6.3.7.
- (5) Include objectives, policies or rules in district plans that identify and support development of Key Activity Centres, and neighbourhood centres, and manage the distribution of commercial activities that could adversely affect the function or viability, or investment in, those centres.
- (6) Identify areas suitable for future business land, and suitable for redevelopment of underutilised existing business land, within existing urban areas.
- (7) Map the extent and location of Key Activity Centres and include these in district plans.
- (8) Investigate and implement methods in district plans for promoting development and enhancement of Key Activity Centres.

### ***Should***

- (9) Consider appropriate administrative and financial methods to enable and encourage Key Activity Centres to fulfil their function, and to promote intensification of identified urban areas and brownfield development.

## **Local authorities:**

### ***Should***

- (10) Provide for sequencing, provision and funding of infrastructure that supports the pattern of settlement in Map A, including through changes to the CRPS, provisions in district plans, regional plans, the Regional Land Transport Strategy, Long Term Plans, other infrastructure plans, and any relevant strategic planning documents, whether prepared under the Local Government Act 2002, the Resource Management Act 1991 or the Land Transport Management Act 2003.

## **Principal reasons and explanation**

Map A shows existing urban areas and priority areas for development for Greater Christchurch. These areas are identified as being required to provide sufficient land zoned for urban purposes to enable recovery and rebuilding through to 2028. The Policy and Map A provide a clear, co-ordinated land use and infrastructure framework for the recovery of Greater Christchurch. Yearly monitoring of uptake and capacity will provide a continuing picture of future greenfield land requirements.

Land within the residential red zone is within the existing urban area shown on Map A until such time as the future use of the red-zoned land is determined by the Crown.

This is in accordance with the direction to prepare the Land Use Recovery Plan given by the Minister for Canterbury Earthquake Recovery.

To ensure that recovery resources are managed efficiently and sustainably, the provisions identify where certain types of development can take place, and where it cannot take place. They also recognise that specific activities are provided for outside of urban areas elsewhere in the CRPS, such as papakāinga housing and marae under Policy 5.3.4 and educational facilities where no other practicable options for locating the facility exist. It is anticipated that established urban activities located outside of the identified urban area will be able to continue to operate their activities, with any expansion considered on a case-by-case basis.

The identification of priority areas for development includes the following considerations:

- (1) whether there is infrastructure either in place or able to be efficiently provided to support urban activity;
- (2) the location of strategic infrastructure and significant natural resources;
- (3) that safe, convenient and sustainable access to community, social and commercial facilities can be provided;
- (4) that sufficient land is encompassed to provide efficiently and effectively for all residential and business needs for recovery and rebuilding;
- (5) the priority areas avoid noise sensitive activities occurring within the 50 dBA Ldn air noise contour surrounding Christchurch International Airport (with the exception of Kaiapoi as identified in Policy 6.3.5), so as not to compromise the efficient operation of Christchurch International Airport, taking into account the health, well-being and amenity of people;
- (6) they do not adversely affect the operation of the Port of Lyttelton;
- (7) they avoid the groundwater protection zone for Christchurch's drinking water, where urban land use increases the risk of contamination;
- (8) they do not lie between the primary and secondary stop banks south of the Waimakariri River which are designed to retain floodwaters in the event of flood breakout;
- (9) flooding, inundation and high hazard areas (as defined in Policy 11.3.1 and 11.3.2);
- (10) whether the areas are subject to other natural hazards including coastal hazards, coastal inundation and significant risk from tsunami;
- (11) whether the areas are subject to significant liquefaction potential where mitigation is less desirable over preference for sequencing more stable land;
- (12) they exclude land required to protect the landscape character of the Port Hills;
- (13) they exclude the rural land required to maintain the open space landscape character either between or surrounding the areas of urban activity within Greater Christchurch.

Within the existing urban area, Key Activity Centres are also indicated. These provide a focus for commercial activities and residential intensification. While post-earthquake business growth has increased in many of the Key Activity Centre areas, inappropriate non-centre growth has also occurred. Moving towards 2016, when the Order in Council which enables businesses to be run from residential locations expires, it will be important to refocus commercial activities to the Central City and Key Activity Centres, and where appropriate neighbourhood centres, so that the function of these centres as a focus for economic activity is reinforced.

### **Policy 6.3.2 – Development form and urban design**

**Business development, residential development (including rural residential development) and the establishment of public space is to give effect to the principles of good urban design below, and those of the NZ Urban Design Protocol 2005, through the design, assessment and development process:**

- (1) Tūrangawaewae – the sense of place and belonging – recognition and incorporation of the identity of the place, the context and the core elements that comprise the place. Through context and site analysis, the following elements should be used to reflect the appropriateness of the development to its location: landmarks and features, historic heritage, the character and quality of the existing built and natural environment, historic and cultural markers and local stories.**
- (2) Integration – recognition of the need for well-integrated places, infrastructure, movement routes and networks, spaces, land uses and the natural and built environment. These elements should be overlaid to provide an appropriate form and pattern of use and development.**
- (3) Connectivity – the provision of efficient and safe high quality, barrier free, multimodal connections within a development, to surrounding areas, and to local facilities and services, with emphasis at a local level placed on walking, cycling and public transport as more sustainable forms of transport.**
- (4) Safety – recognition and incorporation of Crime Prevention Through Urban Design (CPTED) principles in the layout and design of developments, networks and spaces to ensure safe, comfortable and attractive places.**
- (5) Choice and diversity – ensuring developments provide choice and diversity in their layout, built form, land use housing type and density, to adapt to the changing needs and circumstances of the population.**
- (6) Environmentally sustainable design – ensuring that the process of design and development minimises water and resource use, restores ecosystems, safeguards mauri and maximises passive solar gain.**
- (7) Creativity and innovation – supporting opportunities for exemplar approaches to infrastructure and urban form to lift the benchmark in the development of new urban areas in the Christchurch region.**

This policy implements the following objectives:

Objectives 6.2.1, 6.2.2, 6.2.3, 6.2.4, 6.2.5, 6.2.6

#### **Methods**

##### **Territorial Authorities:**

##### ***Will***

- (1) Include objectives, policies and rules in district plans to give effect to Policy 6.3.2, and determine thresholds when urban design provisions apply for development.

##### ***Should***

- (2) Develop urban design guidelines to assist developers with addressing the matters set out in Policy 6.3.2.

- (3) Consider the principles of good urban design as reflected in the New Zealand Urban Design Protocol (2005) in urban design processes.

### **Principal reasons and explanation**

Good urban design is critical to the rebuilding and recovery of Greater Christchurch. Urban design ranges in scale from the design and layout of the whole city, to the suburb, block, street and section design, or even to the architecture of the building and surrounding landscape. It becomes particularly important in the development of the street and block structure, at the interface between buildings/structures and spaces, and between public and private space. Urban design input can take place through the development of outline development plans, creation of development controls for zones, or at a finer-grained level through a resource consent process.

Good urban design can increase the functionality, amenity and efficiency of urban areas in Greater Christchurch. It will support the economic performance of Greater Christchurch, its attractiveness as a tourist destination, its ability to attract and retain new or returning residents and increase the quality of life of its existing residents. In particular, addressing the matters listed will:

- (1) assist with reducing crime;
- (2) reduce travel times, fuel usage and therefore greenhouse gas emissions, and dependence on private motor vehicles;
- (3) provide for a high standard of physical amenities;
- (4) minimise adverse effects on other areas such as flooding, traffic congestion and degraded water;
- (5) protect important features of the natural environment;
- (6) provide protection of historic heritage from inappropriate development, and enhance its values;
- (7) improve the mental and physical wellbeing of its residents; and
- (8) create efficient development patterns.

It is recognised that urban design is also about a process, where ideally collaboration takes place. Local authorities need to be clear about their expectations for development, to ensure efficient management through consent or plan change processes. As such, the development of clear, user friendly guides, developed in consultation with the development industry and professional institutes, for different types of development are a necessary means of achieving good design outcomes that will support the rebuilding and recovery of Greater Christchurch.

In addition, the NZ Urban Design Protocol (2005) provides a framework for good urban design and should be considered and referenced in any urban design process.

### **Policy 6.3.3 – Development in accordance with outline development plans**

**Development in greenfield areas, including rural residential development, is to occur in accordance with the provisions set out in an outline development plan or other rules for the area. Subdivision must not proceed ahead of the incorporation of an outline development plan in a district plan. Outline development plans and associated rules will:**

- (1) **Be prepared as:**
  - (a) **a single plan for the whole of the priority area; or**

- (b) where an integrated plan adopted by the Territorial Authority exists for the whole of the priority area and the outline development plan is consistent with the integrated plan, part of that integrated plan; or
- (c) as a single plan for the whole of a rural residential area; and
- (2) Be prepared in accordance with the matters set out in Policy 6.3.2;
- (3) Show proposed land uses including:
  - (a) Principal through roads, connections with surrounding road networks, relevant infrastructure services and areas for possible future development;
  - (b) Land required for community facilities or schools;
  - (c) Parks and other land for recreation;
  - (d) Land to be used for business activities;
  - (e) The distribution of different residential densities, in accordance with Policy 6.3.7;
  - (f) Land required for stormwater treatment, retention and drainage paths
  - (g) Land reserved or otherwise set aside from development for environmental, historic heritage, or landscape protection or enhancement;
  - (h) Land reserved or otherwise set aside from development for any other reason, and the reasons for its protection from development;
  - (i) Pedestrian walkways, cycleways, public transport routes both within and adjoining the area to be developed;
- (4) Demonstrate how Policy 6.3.7 will be achieved for residential areas within the area that is the subject of the outline development plan, including any staging;
- (5) Identify significant cultural, natural or historic heritage features and values, and show how they are to be protected and/or enhanced;
- (6) Document the infrastructure required, when it will be required and how it will be funded;
- (7) Set out the staging and co-ordination of subdivision and development between landowners;
- (8) Demonstrate how effective provision is made for a range of transport options including public transport options and integration between transport modes, including pedestrian, cycling, public transport, freight, and private motor vehicles;
- (9) Show how other potential adverse effects on and/or from nearby existing or designated strategic infrastructure (including requirements

for designations, or planned infrastructure) will be avoided, remedied or appropriately mitigated;

- (10) Show how other potential adverse effects on the environment, including the protection and enhancement of surface and groundwater quality, are to be avoided, remedied or mitigated;
- (11) Show how the adverse effects associated with natural hazards are to be avoided, remedied or mitigated as appropriate and in accordance with Chapter 11 and any relevant guidelines; and
- (12) Include any other information that is relevant to an understanding of the development and its proposed zoning.

This policy implements the following objectives:

Objectives 6.2.1, 6.2.2, 6.2.3, 6.2.4, 6.2.6

## **Methods**

### **The Regional Council:**

#### ***Will***

- (1) Establish a protocol and guidelines to assist all parties involved in the preparation of outline development plans to ensure Policy 6.3.3 is efficiently and effectively applied.

### **Territorial Authorities:**

#### ***Will***

- (2) Require an outline development plan to be developed and incorporated into district plans, prior to, or at the same time as, rezoning land for urban use in greenfield areas.
- (3) Ensure that any outline development plans and any urban intensification plans are completed in accordance with Policy 6.3.2 and Policy 6.3.3.

#### ***Should***

- (4) Ensure that financial provision is made for delivery of infrastructure to priority areas for development.
- (5) Ensure that sustainability outcomes are achieved through the outline development process, including solar orientation for subdivision layouts, efficient multimodal transport routes, and provision for low impact urban design and development.
- (6) Ensure that greenfields land is treated appropriately as part of a requirement for zoning, including the upgrade of land to be used for new urban expansion or infrastructure up to at least TC2 level or better.

## **Principal reasons and explanation**

The use of outline development plans for residential and business greenfield development is necessary for the recovery of Greater Christchurch. They will assist with the efficient use of resources when planning land uses, provide for sustainable urban development, and ensure adequate housing supply and choice to facilitate

earthquake recovery. Background information provided through the process provides the necessary background evaluation work before or at the same time as the land is rezoned.

Outline development plans provide a mechanism for integrating urban development with infrastructure, making the best use of existing infrastructure, and identifying and providing for the additional infrastructure required to meet the needs of incoming residents and businesses. They also provide the mechanism for integrating new development with existing urban areas, and of achieving the type and form of development necessary to accommodate urban growth in a sustainable way. Staging may be required to allow for infrastructure upgrades, enabling parts of a development to be delivered earlier.

In addition, these plans help to provide certainty for the community, developers, network utility providers and territorial authorities, and ensure that all constraints associated with the development of an area are investigated, addressed or protected at the time of initial zoning for urban purposes. By identifying opportunities for low impact urban design and development early on in the land development process, recovery will be enabled by building new developments in a better way.

#### **Policy 6.3.4 – Transport effectiveness**

**Ensure that an efficient and effective transport network that supports business and residential recovery is restored, protected and enhanced so that it maintains and improves movement of people and goods around Greater Christchurch by:**

- (1) Avoiding development that will overload strategic freight routes;**
- (2) providing patterns of development that optimise use of existing network capacity and ensuring that, where possible, new building projects support increased uptake of active and public transport, and provide opportunities for modal choice;**
- (3) providing opportunities for travel demand management;**
- (4) requiring integrated transport assessment for substantial developments; and**
- (5) improving road user safety.**

This policy implements the following objectives:

Objectives 6.2.1, 6.2.2, 6.2.3, 6.2.4, 6.2.5, 6.2.6

#### **Methods**

##### **Territorial Authorities:**

##### ***Will***

- (1) Include objectives and policies, and may include rules, to manage property access, streetscape and transport efficiency conflicts.
- (2) Include objectives and policies, and may include rules in district plans to ensure that, where possible, development provides for, and supports



increased uptake of active and public transport; and provides opportunities for modal choice, including walking and cycling.

- (3) Include trigger thresholds in district plans for development where an integrated transport assessment is required.
- (4) Identify strategic freight routes

**Local authorities:**

***Should***

- (5) Give consideration to any transport projects that may be needed to give effect to Policy 6.3.4 and include them in their Annual Plans, the Three Year Plan, Long Term Plans, the Regional Land Transport Programme or other infrastructure plans, as appropriate.
- (6) Provide options for travel demand management.

**Principal reasons and explanation**

Changing travel patterns since the earthquakes have placed significant stress on Christchurch's transport infrastructure. With pressure on to enable more land development in response to the earthquakes, there will inevitably be a tension on how to respond to transport needs with limited funds. The Councils and NZTA will look to prioritise transport improvements where they provide greatest return on the investment. This means that the usually expected levels of service for future development may be lower in the interim until the transport maintenance and improvement can catch up. It is recognised that efficient and effective movement of goods within Greater Christchurch is important for the rebuild of the city and outlying townships, and also important for future wellbeing and energy efficiency.

One way to achieve this is through provision for active forms of transport, such as cycling and walking. Other forms of travel demand management may also lead to efficient transport and encourage a compact urban form. The earthquake resulted in a significant reduction in public transport use. With new routes in place and travel patterns more settled, public transport use in Christchurch is increasing and is on track to return to pre-earthquake levels. A compact urban form and travel demand management will build upon the natural return to public transport use in Christchurch.

It is also important that Christchurch is able to protect its key transport infrastructure including the airport, rail networks, transport hubs and strategic routes from reverse sensitivity effects. These infrastructure services play a critical role in the functioning and economic wellbeing of the region.

The policy also requires development of integrated transport assessments for substantial developments. By focussing on large developments that have the potential to impact on strategic transport networks, territorial authorities will be able to fully consider all of the transport impacts together, and developers will be able to develop better responses to contribute to an efficient transport system.

An efficient and effective transport network which meets the changed needs of people and businesses, and enables accessible, sustainable, affordable and safe travel choices, is necessary for recovery.

### **Policy 6.3.5 – Integration of land use and infrastructure**

**Recovery of Greater Christchurch is to be assisted by the integration of land use development with infrastructure by:**

- (1) Identifying priority areas for development to enable reliable forward planning for infrastructure development and delivery;**
- (2) Ensuring that the nature, timing and sequencing of new development is co-ordinated with the development, funding, implementation and operation of transport and other infrastructure in order to;**
  - (a) optimise the efficient and affordable provision of both the development and the infrastructure;**
  - (b) maintain or enhance the operational effectiveness, viability and safety of existing and planned infrastructure;**
  - (c) protect investment in existing and planned infrastructure; and**
  - (d) ensure new development does not occur until provision for appropriate infrastructure is in place;**
- (3) Providing that the efficient and effective functioning of infrastructure, including transport corridors, is maintained, and the ability to maintain and upgrade that infrastructure is retained;**
- (4) Only providing for new development that does not affect the efficient operation, use, development, appropriate upgrading and safety of existing strategic infrastructure, including by avoiding noise sensitive activities within the 50dBA Ldn airport noise contour for Christchurch International Airport, unless the activity is within an existing residentially zoned urban area, priority area identified for Kaiapoi, or greenfield residential area identified in Map A; and**
- (5) Managing the effects of land use activities on infrastructure, including avoiding activities that have potential to limit the efficient and effective, provision, operation, maintenance or upgrade of strategic infrastructure and freight hubs.**

This policy implements the following objectives:

Objectives 6.2.1, 6.2.2, 6.2.3, 6.2.4, 6.2.5, 6.2.6

#### **Methods**

##### **Territorial Authorities:**

##### ***Will***

- (1) Include objectives, policies and rules in district plans that avoid noise sensitive activities within the 50dBA Ldn airport noise contour for Christchurch International Airport, unless the activity is within an existing urban area, priority area identified for Kaiapoi, or greenfield residential area identified in Map A.**
- (2) Include objectives, policies and rules in district plans to manage reverse sensitivity effects between strategic infrastructure and subdivision, use and development, including for residential and rural-residential activities.**

- (3) Adopt a consistent management approach for addressing the adverse effects of, and on, infrastructure. In-ground utilities should be within ground that is equivalent to TC2 or better, with design and construction in accordance with any relevant guidelines.

**Local authorities:**

***Should***

- (4) Give consideration to any infrastructure projects that may be needed to give effect to Policy 6.3.5 and include them in their Annual Plans, the Three Year Plan, Long Term Plans, the Regional Land Transport Programme or other infrastructure plans, as appropriate to enable the orderly and efficient development of priority areas.

**Principal reasons and explanation**

In order to achieve a coordinated and efficient recovery, development of urban areas must be integrated with the provision of infrastructure, including ensuring that existing strategic infrastructure can continue to operate efficiently and effectively. Access for freight movements to and from the major ports in Greater Christchurch must be maintained and enhanced, and not compromised by the location of new urban development.

Priority areas for development are generally clustered to the north, west and south-west of existing urban areas. These areas are all close to existing major infrastructure corridors which connect to the growth areas in the north and Waimakariri District, and to the south and on to Selwyn District. The growth areas have been assessed as having the best potential to accommodate residential and business growth through to 2028 whilst achieving a consolidated urban form and an efficient and orderly provision of infrastructure. It is important that timing and sequencing of development is aligned with funding and implementation of infrastructure.

Strategic infrastructure represents an important regional and sometimes national asset that should not be compromised by urban growth and intensification. Strategic infrastructure such as Christchurch International Airport, the Port of Lyttelton, the State Highway and strategic road networks and rail corridors is required to support Greater Christchurch's recovery through transporting such things as building materials, equipment and personnel. The locational requirements and existing investment in strategic infrastructure means that it is extremely inefficient for them to relocate, and effects of land use on their operation can significantly reduce efficiency and attractiveness as transport options. Care also needs to be taken in terms of safety for regionally significant infrastructure; while issues such as birdstrike have a low probability of causing adverse effects, the potential impacts can be significant. The operation of strategic infrastructure can affect the liveability of residential developments in their vicinity, despite the application of practicable mitigation measures to address effects, which in turn exerts pressure on the infrastructure to further mitigate their effects. It is better to instead select development options where such reverse sensitivity constraints do not exist.

The only exception to this is provided for at Kaiapoi, where offsets for other red-zoned residential areas affected by the earthquakes are provided for within the 50dBA noise contour. Specifically;

- A limited number of households within the 50 dBA Ldn air noise contour within Kaiapoi is provided as an offset for the displacement of noise sensitive

residential activities within the 50 dBA Ldn air noise contour (Kaiapoi Residential Red Zone) as a consequence of the 2010/2011 Canterbury earthquakes. Such development also provides for the contiguous and consolidated development of Kaiapoi.

- This situation is unique to Kaiapoi and distinguishable from all other urban areas within Greater Christchurch. Much of the Kaiapoi Residential Red Zone is already largely contained within the 50 dBA Ldn air noise contour.
- A secondary reason for accommodating residential development within the 50 dBA Ldn air noise contours in Kaiapoi is that the area under the 50 dBA Ldn air noise contour in Kaiapoi is surrounded by existing urban development or by urban limits, such that retaining non-residential zoning for this land would result in significant urban design and servicing issues for the surrounding urban development and a lack of cohesion for the separated neighbourhoods.
- Providing further household growth within the 50 dBA Ldn air noise contour in Kaiapoi does not create a precedent for allowing further household growth within the 50 dBA Ldn air noise contour at any other locations within Greater Christchurch.

#### **Policy 6.3.6 – Business land**

**To ensure that provision, recovery and rebuilding of business land in Greater Christchurch maximises business retention, attracts investment, and provides for healthy working environments; business activities are to be provided for in a manner which:**

- (1) Promotes the utilisation and redevelopment of existing business land, and provides sufficient additional greenfield business land through to 2028 as provided for in Map A;**
- (2) Recognises demand arising from the relocation of business and industrial activities as a result of earthquake-damaged land and buildings;**
- (3) Reinforces the role and attractiveness of the Central City, as the city’s primary commercial centre, and that of the Key Activity Centres;**
- (4) Recognises that new office and retailing is primarily to be directed to the Central City, Key Activity Centres and neighbourhood centres where these activities reflect and support the function and role of those centres; or in circumstances where locating out of centre, will not give rise to significant adverse distributional or urban form effects;**
- (5) Recognises that new greenfield business land in Christchurch City is primarily for industrial activities, and restricts these areas for office and retail use;**
- (6) Recognises that existing business zones provide for a range of business activities depending on the desired amenity of the receiving environment and the potential or otherwise for significant distributional or urban form effects from retail and office activity on other centres;**
- (7) Utilises existing infrastructure availability, capacity and quality;**

- (8) **Protects existing and future communities from reverse sensitivity issues;**
- (9) **Ensures close proximity to labour supply, major transport hubs and passenger transport networks;**
- (10) **Encourages self-sufficiency of employment and business activities within communities across Greater Christchurch;**
- (11) **Promotes, where appropriate, development of mixed-use opportunities, within commercial business areas, where reverse sensitivity issues can be appropriately managed; and**
- (12) **Incorporates good urban design principles appropriate to the context of the development.**

This policy implements the following objectives:

Objectives 6.2.1, 6.2.2, 6.2.3, 6.2.4, 6.2.5, 6.2.6

### **Methods**

#### **Territorial Authorities:**

##### ***Will***

- (1) Include objectives, policies and rules in district plans that implement Policy 6.3.6.
- (2) Identify trigger thresholds for office and retail activities in industrial areas where these activities are likely to give rise to distributional effects, particularly on larger commercial centres, or result in reverse sensitivity effects.

##### ***Should***

- (3) Consider appropriate administrative and financial arrangements to enable and encourage business land provision to occur.
- (4) Identify neighbourhood centres in district plans.

### **Principal reasons and explanation**

The provision of adequate land for future business activities is a key requirement for successful re-building and recovery, and for the economic wellbeing of Greater Christchurch. There was significant damage to industrial and other business land and buildings throughout Greater Christchurch, resulting in a shift of business both from the eastern side of the city to the west, and also from the Central City out into the suburbs. Ongoing insurance issues may continue to place pressure on further demands to relocate. In addition to this, an Order in Council that enables residential land to be used for business activities will expire in 2016, creating further demand for developed business land. Through the rebuilding process, office and retail development needs to focus on reinforcing the Central City and Key Activity Centres, as well as the network of neighbourhood centres, so that these areas can regenerate quickly and agglomeration efficiencies can be gained through locating similar types of businesses in one place.

Provision of new business land should be focused around existing infrastructure to minimise public costs and in particular to achieve integration with transport networks. Locating appropriate business land close to existing and future residential development helps to achieve a greater range of travel options as well as reducing energy usage. Greater self-sufficiency of employment within districts, suburbs and settlements is also desirable in terms of community development and social sustainability. It will be important that, as time passes, the use of industrial land for short-term accommodation for retail and offices is discouraged, and existing and recovering centres that cater for these uses are reinforced.

### **Policy 6.3.7 – Residential location, yield and intensification**

**In relation to residential development opportunities in Greater Christchurch:**

- (1) Subject to Policy 5.3.4, greenfield residential development shall occur in accordance with the priority areas as set out in Map A. These areas are sufficient for both growth and residential relocation through to 2028.**
- (2) Intensification in urban areas of Greater Christchurch is to be focused around the Central City, Key Activity Centres, neighbourhood commercial centres, public transport routes, mixed-use areas, and on suitable brownfield industrial land.**
- (3) Intensification developments and development in priority areas shall achieve at least the following residential net densities averaged over the whole of an ODP area:**
  - (a) 10 household units per hectare in greenfield areas in Selwyn and Waimakariri District;**
  - (b) 15 household units per hectare in greenfield areas in Christchurch City;**
  - (c) 50 household units per hectare for intensification development within the Central City;**
  - (d) 30 household units per hectare for intensification development elsewhere as identified in the Christchurch City Plan.**
- (4) Provision will be made in district plans for comprehensive development across multiple or amalgamated sites.**
- (5) Affordability is to be addressed by providing sufficient land to meet housing demand during the recovery period, enabling brownfield development and providing for a range of lot sizes, densities and appropriate development controls that support more intensive developments such as mixed use developments, apartments, townhouses and terraced housing.**

This policy implements the following objectives:

Objectives 6.2.1, 6.2.2, 6.2.3, 6.2.4, 6.2.5

### **Methods**

**Territorial authorities:**

***Will***

- (1) Include objectives, policies and methods in district plans to ensure that the specified densities in Policy 6.3.7 are achieved.
- (2) Identify areas in district plans that are suitable for urban intensification, including brownfields redevelopment.
- (3) Include objectives, policies and rules in district plans for comprehensive development across multiple or amalgamated sites in appropriate locations.

### **Should**

- (4) Consider incentives to encourage intensification and brownfields redevelopment.
- (5) In relation to Christchurch City, continue to promote medium to high density residential development, particularly within the Central City.
- (6) In relation to Christchurch City, investigate methods for promoting intensification, including selecting areas for specific Council initiatives, and developing Urban Intensification Plans where appropriate.
- (7) Co-ordinate the sequencing, provision and funding of infrastructure in Annual Plans, the Three Year Plan, Long Term Plans, the Regional Land Transport Programme or other infrastructure plans, as appropriate, to enable the orderly and efficient development of priority areas.

### **Principal reasons and explanation**

The earthquakes have resulted in some significant short-term impacts on the housing market, pushing up demand in the short term for temporary accommodation during the recovery and rebuilding period, and relocation of residents from red-zoned areas. This short-term demand is expected to slowly return to normal growth.

Accommodating the increased demand for households can be achieved in two ways, through greenfield expansion into priority areas, and through intensification within existing urban areas. To support a sustainable urban form, this intensification is ideally located around existing the Central City, Key Activity Centres and neighbourhood centres, and public transport routes, within mixed use areas where residential activities can support business activities, and on brownfield sites. Further work will be required to increase intensification, and work towards accommodating future growth within existing urban areas.

Certain areas in Christchurch City have been identified for more intensive residential use for many years. Other suitable areas may be identified, including the redevelopment of brownfields sites for residential or mixed-use activities. Providing for intensification in and around the Central City and Key Activity Centres will help ensure good access to commercial, community and recreational facilities and to public transport. Councils have the ability to encourage greater uptake of intensification in selected areas through investment such as the provision of land improvements to open space and the streetscape.

In order to efficiently utilise the identified priority areas to accommodate recovery and rebuild development, minimum densities are to be achieved. This will help to create a compact urban form that supports existing centres and can be served efficiently by infrastructure, including public transport. It will also help to ensure that housing supply and housing choice, including affordable housing options, meet demand and enable recovery.

### **Policy 6.3.8 - Regeneration of brownfield land**

**To encourage and provide for the recovery and regeneration of existing brownfield areas through new comprehensive residential, mixed-use or business developments, provided such activities will ensure the safe and efficient functioning of the transport network and will not have significant adverse distributional or urban form effects on the Central City, existing Key Activity Centres and neighbourhood centres, or give rise to significant reverse sensitivity effects.**

This policy implements the following objectives:

Objectives 6.2.1, 6.2.2, 6.2.3, 6.2.4, 6.2.5, 6.2.6

#### **Methods**

##### **Territorial authorities:**

##### ***Will***

- (1) Include objectives, policies and rules in district plans that enable the comprehensive redevelopment of appropriate brownfield sites.

##### ***Should***

- (2) Within 6 months of this chapter becoming operative, identify appropriate brownfield sites for redevelopment, including broad land uses.
- (3) Give consideration to appropriate administrative and financial arrangements to enable and encourage brownfield redevelopment to occur.

#### **Principal reasons and explanation**

Brownfield redevelopment will support the efficient reuse and recovery of underutilised or abandoned land. This will support the recovery of these areas and their wider neighbourhoods by bringing higher intensity and often more appropriate activities into these locations, and enhance the amenity of the area.

Redevelopment will also help to reduce the adverse effects of travel for work, business and recreation, limit the costs of new infrastructure, and avoid the adverse effects of development on sensitive landscapes, natural features and areas of high amenity. Significant adverse effects such as reverse sensitivity, distributional or urban form impacts on the Central City, existing Key Activity Centres and neighbourhood centres, and impacts on the transport network, need to be avoided or mitigated.

Such regeneration projects should occur in a comprehensive manner to ensure that good urban design and amenity outcomes are achieved. Councils have the ability to encourage redevelopment in selected areas through investment such as the provision of and improvements to open space and the streetscape.”



### **Policy 6.3.9 – Rural residential development**

**In Greater Christchurch, rural residential development further to areas already zoned in district plans as at 1<sup>st</sup> January 2013 can only be provided for by territorial authorities in accordance with adopted rural residential development strategy prepared in accordance with the Local Government Act 2002, subject to the following:**

- (1) In the case of Christchurch City, no further rural residential activity is to be provided for within the Christchurch City Plan area;**
- (2) The location must be outside the priority areas for development and existing urban areas;**
- (3) All subdivision and development must be located so that it can be economically provided with a reticulated sewer and water supply integrated with a publicly owned system, and appropriate stormwater treatment and disposal;**
- (4) Legal and physical access is provided to a sealed road, but not directly to a road defined in the relevant district plan as a Strategic or Arterial Road, or as a State highway under the Government Rooding Powers Act;**
- (5) The location and design of any proposed rural residential development shall:**
  - (a) avoid noise sensitive activities occurring within the 50 dBA Ldn air noise contour surrounding Christchurch International Airport so as not to compromise the future efficient operation of Christchurch International Airport or the health, well-being and amenity of people;**
  - (b) avoid the groundwater protection zone for Christchurch City's drinking water;**
  - (c) avoid land between the primary and secondary stop banks south of the Waimakariri River;**
  - (d) avoid land required to protect the landscape character of the Port Hills;**
  - (e) not compromise the operational capacity of the Burnham Military Camp, West Melton Military Training Area or Rangiora Airfield;**
  - (f) support existing or upgraded community infrastructure and provide for good access to emergency services;**
  - (g) avoid significant reverse sensitivity effects with adjacent rural activities, including quarrying and agricultural research farms, or strategic infrastructure;**
  - (h) avoid significant natural hazard areas including steep or unstable land;**
  - (i) avoid significant adverse ecological effects, and support the protection and enhancement of ecological values;**

- (j) support the protection and enhancement of ancestral land, water sites, wāhi tapu and wāhi taonga of Ngāi Tahu;
  - (k) where adjacent to or in close proximity to an existing urban or rural residential area, be able to be integrated into or consolidated with the existing settlement; and
  - (l) avoid adverse effects on existing surface water quality.
- (6) An outline development plan is prepared which sets out an integrated design for subdivision and land use, and provides for the long-term maintenance of rural residential character.
- (7) A rural residential development area shall not be regarded as in transition to full urban development.

This policy implements the following objectives:

Objectives 6.2.1, 6.2.2, 6.2.3, 6.2.4

## **Methods**

### **The Regional Council:**

#### ***Will***

- (1) Have regard to Policy 6.3.9 in relation to any consents relating to rural-residential activities in Greater Christchurch, and consider deferral under s91 where other consents are required from another local authority, so that the effects of a proposal can be considered together.

### **Territorial Authorities:**

#### ***Will***

- (2) Include objectives, policies and rules in district plans that provide for the matters set out in Policy 6.3.9.

#### ***Should***

- (3) Develop a rural residential strategy for the district to inform the extent of rural residential activity and outcomes sought for this form of development within the district.

## **Principal reasons and explanation**

An important aspect of residential capacity includes the contribution of rural residential development, which is provided for in Waimakariri and Selwyn Districts where it accords with a relevant rural residential strategy. Many of the rural western areas of Greater Christchurch remained undamaged during the earthquakes and are also located out of the area identified as being prone to liquefaction, making them more desirable locations to live.

At the same time, it is also important to manage the extent of rural residential activity due to the pressure it places on infrastructure, its impact on transport efficiency, and the maintenance of rural character and rural land use for production. In the case of Christchurch City, further rural-residential activity also has the potential to constrain

future urban expansion options, or otherwise be affected by noise contours for the airport, and so it is not provided for within the area covered by the Christchurch City Plan. Rural residential development can have significant effects disproportionate to the numbers of households living within this form of development, and more than limited provision would undermine the achievement of recovery.

Rural residential development is therefore provided for to a limited extent during the recovery period in recognition of the desirability of providing a range of choice in housing types for those needing to relocate, without compromising the overall intent of consolidation in the CRPS.

### **Policy 6.3.10 – Māori Reserves**

**Recognise and provide for the relationship of local Ngāi Tahu with their ancestral lands, waters, wāhi tapu and taonga by enabling Māori Reserves within the Greater Christchurch area to be developed and used for their intended purposes for which they were originally reserved, taking into account the following matters where relevant:**

- (a) flooding, inundation and other natural hazards;**
- (b) rural amenity and outlook;**
- (c) compact urban form;**
- (d) range of housing options;**
- (e) provision of appropriately sized local retail/commercial centres;**
- (f) any outline development plan; and**
- (g) the range of lot sizes and densities.**

This policy implements the following objectives:

Objectives 6.2.1, 6.2.2

### **Methods**

#### **Territorial authorities:**

#### ***Will***

- (1) Include objectives, policies and/or rules in district plans in relation to Māori Reserve Land in Greater Christchurch that recognise and provide for their intended purpose, and accord with Policies 6.3.2, 6.3.3 (except 6.3.3(1) and (4)) and 6.3.4.
- (2) Consult with Te Rūnanga o Ngāi Tahu and Papatipu Rūnanga to develop those plan provisions.
- (3) In relation to development at Māori Reserve 873, provide for development opportunities for Ngāi Tūāhuriri in accordance with Policy 6.3.10 by the inclusion of objectives, policies, rules and an Outline Development Plan within the District Plan.
- (4) In relation to Māori Reserve 873, include objectives, policies and/or rules, within the District Plan, that place appropriate controls on the size and scale of Tuahiwi.
- (5) Monitor and report on, at two yearly intervals, growth within Māori Reserve 873 to determine whether amendments to district plan objectives, policies and rules are required to either limit inappropriate growth and development or facilitate further growth and development.

## **Should**

- (6) Co-ordinate the sequencing, provision and funding of infrastructure in Long Term Plans, or other infrastructure plans, to enable the orderly and efficient development of Māori Reserves.

## **Principal reasons and explanation**

The earthquakes and the subsequent damage and red zoning of properties in Waimakariri District and Christchurch City has led to a number of Māori seeking opportunities to return to ancestral lands, including land at Māori Reserve 873 (Tuahiwi) and Māori Reserve 875 (Rāpaki). This policy recognises the original intent of the land purchase deeds of the 19<sup>th</sup> century to provide for the present and future needs of local Ngāi Tahu landowners and their descendants.

It is important that any development of Māori Reserves is enabled in a way that meets the needs of Māori and other residents, whilst protecting natural and physical resources through maintaining and enhancing the environmental qualities and rural amenity of the area.

Māori Reserves in Greater Christchurch have not been identified as a priority areas, nor as rural residential as development of this land is seen as something that will likely take a more dense form in certain areas and this could result in a more closely settled development pattern. However, it is considered important that any development is of a size and scale appropriate for to the surroundings and that a rural amenity and outlook is maintained. For these reasons it is considered important that an Outline Development Plan is prepared in consultation with the landowners within those reserves to guide and manage development.

## **Policy 6.3.11 Monitoring and Review**

**In relation to development in Greater Christchurch:**

- (1) **The Canterbury Regional Council, in conjunction with the territorial authorities, shall undertake adequate monitoring to demonstrate both in the short term and the long term that there is an available supply of residential and business land to meet the Objectives and Policies of this Chapter.**
- (2) **The Canterbury Regional Council, in conjunction with the territorial authorities, shall undertake monitoring of the supply, uptake and impacts of rural residential land use and development.**
- (3) **Prior to initiating a review of this chapter, for the purposes of information the Canterbury Regional Council may request the organisation or agency responsible for the operation of Christchurch International Airport to undertake a remodelling of the air noise contours relating to the airport.**
- (4) **The Canterbury Regional Council, following relevant territorial authority input, shall initiate a review of the extent and location of land for development if any of the following situations occur:**
  - (a) **A shortfall in available land is identified by monitoring under Policy 6.3.11; or**

- (b) It is identified that altered circumstances have arisen or will arise either in one or more parts of Greater Christchurch, in relation to the expected availability of sub-regional infrastructure, and a reconsideration of the extent, location and timing of land for development is necessary to achieve the objectives and policies of this chapter.
- (5) Any change resulting from a review of the extent, and location of land for development, any alteration to the Priority Areas, or provision of new Greenfield Areas, shall commence only under the following circumstances:
- (a) infrastructure is either in place or able to be economically and efficiently provided to support the urban activity;
  - (b) provision is in place or can be made for safe, convenient and sustainable access to community, social and commercial facilities;
  - (c) the objective of urban consolidation continues to be achieved;
  - (d) urban land use, including industrial and commercial activities, does not increase the risk of contamination of drinking water sources, including the groundwater recharge zone for Christchurch's drinking water;
  - (e) urban development does not lie between the primary and secondary stopbanks south of the Waimakariri River which are designed to retain floodwaters in the event of flood breakout;
  - (f) the landscape character of the Port Hills is protected;
  - (g) sufficient rural land is retained to maintain the open space landscape character either between or surrounding the areas of urban activity within Greater Christchurch; and
  - (h) the operational capacity of strategic infrastructure is not compromised.

This policy implements the following objectives:

Objectives 6.2.1, 6.2.2, 6.2.3, 6.2.4, 6.2.5, 6.2.6

## Methods

- (1) The monitoring for Policy 6.3.11 may include but is not limited to:
- any information published by or sought from Statistics New Zealand.
  - annual or less frequent surveys of business and residential land uptake, including Greenfield Development and redevelopment.
  - annual or less frequent surveys of the development capacity of zoned and serviced land.
  - obtaining and analysing a range of information to assist with the understanding and prediction of future needs, including information on market behaviour and social and economic trends.
- (2) The monitoring for Policy 6.3.11 shall include such matters as the councils consider relevant and appropriate.

- (3) The Canterbury Regional Council shall prepare a comprehensive monitoring report in relation to Policy 6.3.11 at least every three years, and make it publicly available.
- (4) Any remodelling in terms of Policy 6.3.11(3) shall:
  - involve an assessment of projected future airport business growth and operation, and shall take into account, but not be limited to aircraft movements, flight tracks, fleet mix and runway utilisation; and
  - be accompanied by the report of an independent panel of airport noise experts who have undertaken a peer review of the inputs, assumptions and outcomes of the remodelling; and
  - shall be provided to the Canterbury Regional Council in the form of a comprehensive report along with an executive summary or summary report.
- (5) The Canterbury Regional Council shall make the summary report of any remodelling under Method 4 publicly available as soon as practicable after receiving it.
- (6) Any amended growth pattern shall be given effect through the provisions of any relevant regional plan, changes to the Regional Policy Statement, district plans, the Regional Land Transport Strategy, the Regional Land Transport Programme, Annual Plans, Three Year Plans, Long Term Plans and any relevant strategic planning process, as appropriate.
- (7) Territorial authorities shall make appropriate arrangements to enable the achievement of any changes resulting from a review under Policy 6.3.11.

### **Principal reasons and explanation**

Relocation, population, household and business growth can be affected by a wide range of variables. The policy framework should be responsive to this variation in order to meet any changes in circumstances. Policy 6.3.11 is intended to ensure enough land is available and in the right locations to facilitate recovery through to 2028. Monitoring a range of statistics and trends is a key factor in this management. Anticipating the number of relocated or new households and the business activity to be accommodated, as well as the form that these are likely to take, indicates the land areas required for successful recovery.

Policy 6.3.11 also provides that the circumstances for altering the priority area provisions of this chapter are:

- (a) There is determined to be insufficient land within the Priority Areas over the recovery period
- (b) Altered circumstances have arisen in relation to anticipated timing of the infrastructure required to support the development planned by this chapter
- (c) There are changes to the relocation and growth management assumptions upon which the objectives and policies of this chapter are based.

## **ANTICIPATED ENVIRONMENTAL RESULTS**

- (1) Recovery and rebuilding is enabled within Greater Christchurch.
- (2) Priority areas and existing urban areas identified provide the location for all new urban development.
- (3) Significant natural resources are protected from inappropriate development.
- (4) People are protected from unacceptable risk from natural hazards.
- (5) Infrastructure, and urban and rural development, are developed in an integrated manner.
- (6) The use of existing infrastructure is optimized.
- (7) Development opportunities are provided for on Maori Reserves.
- (8) Growth is provided for through both greenfield and brownfield development opportunities.
- (9) Higher density living environments are provided.
- (10) Greenfield development is provided for at a rate that meets demand and enables the efficient provision and use of infrastructure.
- (11) Growth of rural towns within Greater Christchurch is sustainable and encourages self-sufficiency.
- (12) Rural residential development is appropriately managed.
- (13) Development incorporates good urban design.
- (14) Areas of special amenity, heritage value, or importance to Ngai Tahu are retained.
- (15) Residential development contains a range of densities.
- (16) Transport infrastructure appropriately manages network congestion, dependency of private vehicles is reduced, emissions and energy use from vehicles is reduced, and transport safety is enhanced.
- (17) The function and role of the Central City, the Key Activity and Neighbourhood Centres is maintained.
- (18) Sufficient business land is provided for, and different types of business activity take place in appropriate locations, adopting appropriate urban design qualities.

## TO BE INSERTED INTO THE GLOSSARY AND DEFINITIONS

<b><i>Area plan</i></b>	see definition of structure plan
<b><i>Brownfield</i></b>	means abandoned or underutilised business or industrial land, or land no longer required by a requiring authority for a designated purpose.
<b><i>Business or business activities</i></b>	means land or activities that include retail, office, industrial and other commercial and any ancillary activity.
<b><i>Central City</i></b>	means the area covered by the Christchurch Central Recovery Plan.
<b><i>Commercial activities</i></b>	means retail, office and other commercial service activities but does not include industrial activities.
<b><i>District development strategy</i></b>	see definition of structure plan
<b><i>Educational facilities</i></b>	means facilities used for primary, secondary or tertiary education.
<b><i>Electricity transmission network</i></b>	means the national grid as defined in the National Policy Statement on Electricity Transmission 2008.
<b><i>Greater Christchurch</i></b>	means the area shown on Map A.
<b><i>Greenfield areas</i></b>	means areas identified on Map A for greenfields development.
<b><i>Greenfield development</i></b>	means subdivision, use and/or development of land identified on Map A as Greenfield Areas.
<b><i>Historic heritage</i></b>	has the same meaning as in s2 of the Resource Management Act, and includes historic cultural and historic heritage landscapes.
<b><i>Industrial</i></b>	means the manufacturing, assembly, packaging, wholesaling or storage of products or the processing of raw materials and other ancillary activities.
<b><i>Intensification</i></b>	means an increase in the residential household yield within existing urban areas.
<b><i>Key Activity Centres</i></b>	<p>Key existing and proposed commercial/business centres identified as focal points for employment, community activities, and the transport network; and which are suitable for more intensive mixed-use development.</p> <p>The following centres shown on Map A are the</p>



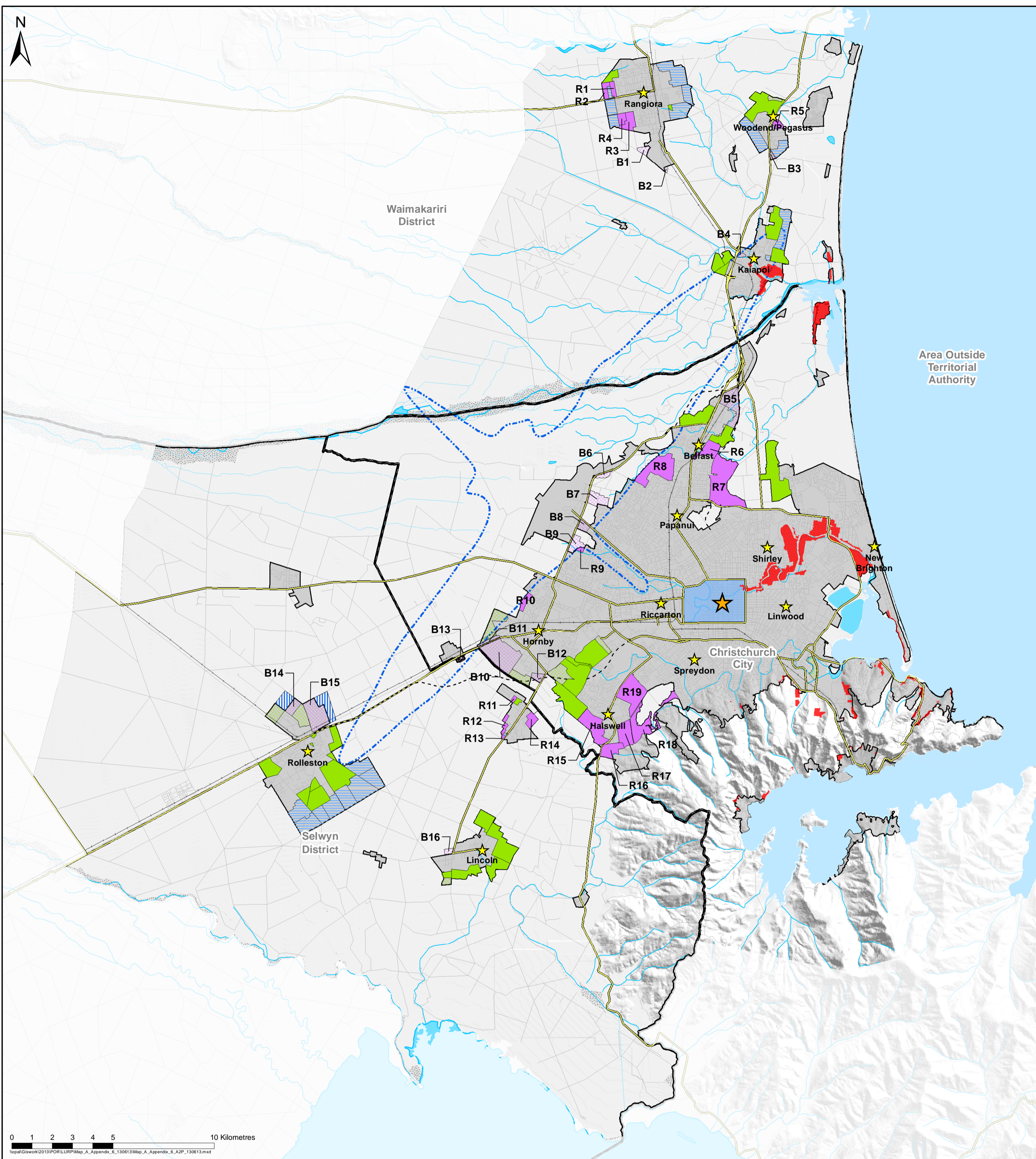
	<p>existing KACs within Greater Christchurch:</p> <ul style="list-style-type: none"> <li>• Papanui</li> <li>• Shirley</li> <li>• Linwood</li> <li>• New Brighton</li> <li>• Belfast</li> <li>• Riccarton</li> <li>• Halswell</li> <li>• Spreydon</li> <li>• Hornby</li> <li>• Kaiapoi</li> <li>• Rangiora</li> <li>• Woodend / Pegasus</li> <li>• Lincoln</li> <li>• Rolleston</li> </ul>
<b>Net density</b>	<p>is the number of lots or household units per hectare (whichever is the greater). The area (ha) includes land for:</p> <ul style="list-style-type: none"> <li>• Residential purposes, including all open space and on-site parking associated with residential development;</li> <li>• Local roads and roading corridors, including pedestrian and cycle ways, but excluding State Highways and major arterial roads;</li> <li>• Local (neighbourhood) reserves.</li> </ul> <p>The area (ha) excludes land that is:</p> <ul style="list-style-type: none"> <li>• Stormwater retention and treatment areas;</li> <li>• Geotechnically constrained (such as land subject to subsidence or inundation)</li> <li>• Set aside to protect significant ecological, cultural, historic heritage or landscape values;</li> <li>• Set aside for esplanade reserves or access strips that form part of a larger regional or sub-regional reserve network;</li> <li>• For local community services and retail facilities, or for schools, hospitals or other district, regional or sub-regional facilities.</li> </ul>
<b>Noise sensitive activities</b>	<p>means</p> <ul style="list-style-type: none"> <li>• Residential activities other than those in conjunction with rural activities that comply with the rules in the relevant district plan as at 23 August 2008;</li> <li>• Education activities including pre-school places or premises, but not including flight training, trade training or other industry related training facilities located within the Special Purpose (Airport) Zone in the Christchurch District Plan or on other land used or available for business activities;</li> </ul>

	<ul style="list-style-type: none"> <li>• Travellers' accommodation except that which is designed, constructed and operated to a standard that mitigates the effects of noise on occupants;</li> <li>• Hospitals, healthcare facilities and any elderly persons housing or complex.</li> </ul>
<b><i>Outline development plan</i></b>	means a plan prepared for the development of a Greenfield Area or Rural Residential Development in the manner outlined in Policies 6.3.3 and 6.3.8. It shall include maps, plans, and other descriptive and illustrative material as necessary to convey the information referred to in Policies 6.3.3 and 6.3.8.
<b><i>Priority areas</i></b>	means areas identified as being priority for greenfield development as identified on Map A.
<b><i>Rural activities</i></b>	means activities of a size, function, intensity or character typical of those in rural areas and includes: <ul style="list-style-type: none"> <li>• Rural land use activities such as agriculture, aquaculture, horticulture and forestry</li> <li>• Businesses that support rural land use activities</li> <li>• Large – footprint parks, reserves, conservation parks and recreation facilities</li> <li>• Residential activity on lots of 4 ha or more</li> <li>• Quarrying and associated activities</li> <li>• Strategic Infrastructure outside of the existing urban area and priority areas for development. .</li> </ul>
<b><i>Rural residential activities</i></b>	means residential units outside the identified priority areas at an average density of between 1 and 2 households per hectare.
<b><i>Rural residential strategy</i></b>	means a strategy or plan developed for the purpose of identifying a territorial authority's approach to management of rural residential development in its district, using the special consultative procedure under the Local Government Act 2002.
<b><i>Strategic infrastructure</i></b>	means those necessary facilities, services and installations which are of greater than local importance, and can include infrastructure that is nationally significant. The following are examples of strategic infrastructure: <ul style="list-style-type: none"> <li>• Strategic transport networks</li> <li>• Christchurch International Airport</li> <li>• Rangiora Airfield</li> <li>• Port of Lyttelton</li> <li>• Bulk fuel supply infrastructure including terminals, wharf lines and pipelines</li> <li>• Defence facilities including Burnham Military</li> </ul>

	<p>Camp and West Melton Military Training Area</p> <ul style="list-style-type: none"> <li>• Strategic telecommunications facilities</li> <li>• The electricity transmission Network</li> <li>• Other strategic network utilities</li> </ul>
<b><i>Strategic transport networks</i></b>	means transport networks and operations of national or regional significance. These include the strategic road network including State Highway and major arterial roads as defined in district plans and the rail network, along with the region's core public passenger transport operations and significant regional transport hubs such as Christchurch International Airport and the Port of Lyttelton.
<b><i>Structure plan; or area plan; or district development strategy:</i></b>	means a comprehensive development plan for a whole or part of a territorial authority administrative area that has been adopted by the territorial authority, under the Local Government Act 2002, which clearly shows the relationship between a proposed land use pattern and all infrastructure requirements.
<b><i>Urban activities</i></b>	Means activities of a size, function, intensity or character typical of those in urban areas and includes: <ul style="list-style-type: none"> <li>• residential units (except rural residential activities) at a density of at more than one household unit per 4 ha of site area;</li> <li>• Business activities, including industrial and commercial activities, except those that fall within the definition of rural activities;</li> <li>• Sports fields and recreation facilities that service the urban population (but excluding activities that require a rural location);</li> <li>• Any other land use that is to be located within the existing urban area or new priority areas for development.</li> </ul>
<b><i>Urban form effects</i></b>	means an effect on urban form and structure, including anticipated location and networks of activities, facilities and infrastructure.
<b><i>Urban intensification plan</i></b>	means a plan prepared for the redevelopment of an area that has been selected by a territorial authority for specific Council initiatives to promote intensification. As a minimum such plans shall identify: <ul style="list-style-type: none"> <li>• The development capacity of the area proposed for intensification.</li> <li>• The capacity of the existing infrastructure and proposed new infrastructure.</li> <li>• The effect on areas with historic heritage values and special amenity.</li> </ul>

- Opportunities for giving effect to Policy 6.3.2.
- How the residential density targets contained in Policy 6.3.7 will be met.
- A range of transport options, including pedestrian, cycling, passenger transport, motor vehicles.





**Legend**

- Key Activity Centres**
- ★ Key Activity Centre
  - ★ Christchurch Central City
- Priority Areas - Business**
- Zoned
  - Not Yet Zoned
  - B## Business Identifier Number
- Priority Areas - Residential**
- Zoned
  - Not Yet Zoned
  - R## Residential Identifier Number
- Existing Urban Area**
- Boundary of infrastructure supported priority, existing urban and greenfield areas.
  - Existing Urban Area - Pre 2011

- Territorial Authority Boundary
  - CERA Red Zoned Land - 31/10/2012
  - Christchurch Central Recovery Plan Area
  - 50dba Airport Noise Contour
- Greenfield Areas - post Land Use Recovery Plan**
- Residential
  - Business

**Locality**



**Scale:**  
1:125,000  
(original sheet size A2)

**Publication Date:**  
13/06/2013

**Coordinate System:**  
NZGD 2000 New Zealand Transverse Mercator

**Disclaimer:**  
This map is a static output of depicted layers and is for reference only. Data layers that appear on this map may or may not be accurate, current or otherwise reliable.

**Map Document:**  
Map\_A\_Appendix\_6\_A2P\_130613.mxd

**Environment Canterbury**  
Kaitiaki Take Kōwhiri



## **Appendix 3: Amendments to the Christchurch City District Plan**

The proposed changes to the Christchurch City Plan set out in this Appendix cover the following:

- Identifying and locating a priority area for Greenfield development in the Upper Styx area (R8);
- Introducing an ODP in full into the City Plan for the whole of the Upper Styx (R8) area, and a master plan for part of the Upper Styx (Highsted) area to guide future urban development. The “Highsted” area is also being rezoned to Living G;
- Amendments to planning maps showing the location of the revised 50dBA airport noise Contour; and
- Amendments to the wording of the policy managing activities within the 50dBA airport noise contour.

*Note: also included in this Appendix (at Attachment 1) are enlargements of all priority areas on Map A for the Christchurch City Council Territorial Authority area. These have been included to enable greater legibility of the Priority Area boundaries. The Land Use Recovery Plan directs the Christchurch City Council to amend their District Plan planning maps to identify these areas.*

## **The Need for Change for Recovery Purposes**

### **(1) Airport Noise Contours**

These amendments shift the current locations of the airport noise contours on the City Planning Maps to the remodelled locations for the contours approved by the Environment Court in 2008. These revised contours have already been included in the Waimakariri and Selwyn District Plans, so the CCC airport noise contours will now align with those in adjoining districts.

The revised (outermost) 50dBA Ldn contour is shown in the Land Use Recovery Plan (Figure 6) and is referred to in several policies in Chapter 6 of the Proposed Regional Policy Statement. Appendix 6 Map A, identifies the location and extent of urban development that will support recovery. Greenfield Priority areas for development are identified only where developing such areas will avoid new noise-sensitive activities occurring within the revised 50 dBA Ldn air noise contour. This will support the efficient operation and development of Christchurch International Airport, while also limiting potential impacts on health, well-being and residential quality of life arising from the 24-hour operation of the Christchurch International Airport.

### **(2) Upper Styx R8 Area – Future Urban Development Area and Outline Development Plan**

These amendments place a Future Urban Development Area (FUDA) notation on land in the Upper Styx area identified as R8 in Map A of Chapter 6 of the RPS. This gives a clear signal that the land is expected to be urbanised, predominantly for residential purposes. The land can accommodate over 1700 households.

The amendments also introduce an Outline Development Plan (ODP) for the area into the City Plan to guide future development for the area, as and when it is rezoned for urban purposes. Outline Development Plans aim to ensure that land use change is supported by the provision of infrastructure and community services, that required housing densities are achieved overall, and that the principles of good urban design and planning are woven into new growth areas. These principles include connectivity in the design and location of networks such as transport linkages, stormwater systems and open space. Supporting policies and rules protect the implementation of the ODP while land is still zoned for rural purposes.

### **(3) Upper Styx R8 Area – Rezoning of Land from Rural 3 to Living G (Highsted)**

These amendments rezone four parcels of land within the Outline Development Plan for the R8 area, to Living G zoning, providing for approximately 350 households and the core of an extensive stormwater management system. They enable urban development and supporting infrastructure through policies and rules and a Master Plan for Highsted, showing the broad pattern of land use and roading, in compliance with the Outline Development Plan provisions.

The package of amendments provides for a mixture of housing densities in accordance with Chapter 6 of the RPS, across three broad bands (150m<sup>2</sup>–300m<sup>2</sup>, 300m<sup>2</sup>–450m<sup>2</sup> and 450m<sup>2</sup>–1000m<sup>2</sup>). The amendments also provide for 2000m<sup>2</sup> of business land for a local commercial centre close to Tulett Park, and green links across the site to the Styx Mill Conservation Reserve.

Much of the land is equivalent to TC2 or TC3, but will be subject to a requirement to demonstrate that through specific engineering investigation, and remediation measures, all sections can be delivered at TC2 or better. This will include ensuring that buildings and structures near waterways and stormwater facilities are appropriately designed to mitigate the risk of lateral spread.

The balance of the land within the ODP area is expected to be rezoned through the CCC District Plan review, once further detailed planning and geotechnical work has been undertaken.

## CITY PLAN AMENDMENTS

Note: For the purposes of this plan change, any text proposed to be added by the plan change is shown as **bold underlined** and text to be deleted as ~~**bold strikethrough**~~.

Amend the City Plan as follows:

### *1. Volume 3, Planning Maps*

- a) Delete Planning Maps 17A, 17B and 24A.*
- b) Add new Planning Maps 17A, 17B and 24A as attached.*

### *2. Volume 2, Policy 6.3A.1 Urban Boundary*

*Renumber existing Policy 6.3.A1 as (a) to that Policy, and add a new (b) as follows:*

- (b) To ensure that the area identified as a “Future Urban Development Area”- Upper Styx (R8) in the Planning Maps facilitates urban development (primarily residential) in the 2012-2041 period**

### *3. Volume 2, Objectives and Policies*

*Add new policy as follows:*

**Policy 6.3A.21 Outline Development Plan for Future Urban Development Area – Upper Styx( R8).**

- (a) To ensure that development of land for urban activities within the Future Urban Development Area (R8) only occurs in general accordance with the Outline Development Plan for the area as shown in Appendix 7A and 7B to the Rural Zones and in accordance with the supporting rules, which reflect the following principles:**
- i. To achieve a mix of residential densities which increases the minimum net residential density to 15 households per hectare in the R8 area as a whole**
  - ii. To locate higher density housing adjacent to public transport routes , a local or mixed use business centre ,Tulett Park and/or neighbourhood parks**
  - iii. To achieve good quality urban design outcomes in all residential development, especially in the higher density residential development areas**
  - iv. To recognise Tangata Whenua’s traditional and contemporary relationship with land and water.**
  - v. To acknowledge the variable geotechnical characteristics of the land, to ensure that residential allotments can be provided as Technical Category 1 and Technical Category 2, and to recognise and mitigate the**



- risk of lateral spread in the location of buildings and structures and the design of stormwater facilities
- vi. To ensure stormwater management operates in an environmentally sustainable manner in accordance with the Styx River /Purakaunui Area Stormwater Management Plan , August 2012 ,including an element of retrofitting for the wider Bishopdale area.
  - vii. To ensure that development complements and does not adversely affect the amenity ,recreational and ecological values of the Styx Mill Conservation Reserve and the Styx River.
  - viii. To ensure that one neighbourhood retail shopping centre of a maximum 2,000m2 gross floor area ,for the convenience needs of the area is provided adjacent to Tulett Park , public transport routes and high density housing,with a high standard of amenity and visual character when viewed from the street.
  - ix. Accessible and linked public open space including an extension to the Tulett Park sportsfields, distributed neighbourhood parks and walkway/cycleways that are integrated with stormwater facilities.
  - x. A movement network that is well connected both internally and externally and that includes safe and efficient provision for public transport, and vehicle, pedestrian and cycle movements.
- (b) To ensure that while rurally zoned, that development of the area identified for Future Urban Development Area -Upper Styx(R8) is not precluded or compromised by the establishment of incompatible activities, or by subdivision below the minimum rural lot size.

#### *4. Volume 2, Objectives and Policies*

*Add new policy as follows:*

#### Policy 6.3A.22 Master Plan(s) – Future Urban Development Area – Upper Styx - R8

(a) Development and future Plan Changes for urban activities within the Future Urban Development Area-Upper Styx(R8) shall include a Master Plan and associated rules (s) which are in general accordance with the Outline Development Plan –Upper Styx shown in Appendix 7A and 7B to Part 4 Rural Zones and with the principles set out in Policy 6.3A.21; and shall show/include:

- i. The distribution of different residential densities within the development or Plan Change site(s), and evidence, of how the mix of residential densities generally achieves a minimum net density of 15 households per hectare as shown in the Outline Development Plan –Upper Styx.
- ii. The location, size and detention/treatment functions of all stormwater facilities (detention basins, wetlands, swales and pipework) in terms of the “Blue Network” of the Outline Development Plan – Upper Styx and also to meet the terms of the discharge consent application CRC131249 which implements the Stormwater Management Plan for the Styx River /Puruakanui Area, dated August 2012.

- iii. The location and nature of the provision for transportation including walking , cycling and public transport in terms of the “Movement Network” of the Outline Development Plan – Upper Styx (R8) including Appendix 7B
- iv. The location and nature of the provision of public reserves and other public open spaces for active sports use and neighbourhood reserves in terms of the “Green Network” of the Outline Development Plan – Upper Styx(R8).
- v. Identification and proposed protection/enhancement of natural and heritage features
- vi. Provisions and methods implementing urban design best practice.
- vii. Details of how the area will connect to existing sewerage, and water services.
- viii. Where ground treatment and building foundation design is required for all Technical Category 2 and Technical Category 3 lands as identified by geotechnical investigations, and consequently the nature and location of proposed set backs from the blue network for buildings and structures.
- ix. Provisions ensuring that new allotments for residential and commercial use are provided in either Technical Category 1 or Technical Category 2 condition.
- x. Provision of a neighbourhood retail business centre to a maximum gross floor area of 2,000m<sup>2</sup>

#### 5. Volume 3 Part 1, Definitions

*Add the following definitions:*

Net density in the Future Urban Development Area – Upper Styx (R8);

means the number of lots or household units per hectare (whichever is the greater).

The area (ha) includes land for:

- Residential purposes, including all open space and on-site parking associated with residential development;
- Local roads and roading corridors, including pedestrian and cycle ways, but excluding State Highways and major arterial roads; and
- Local (neighbourhood) reserves.

The area (ha) excludes land that is:

- Stormwater retention and treatment areas;
- Geotechnically constrained (such as land subject to subsidence or inundation);
- Set aside to protect significant ecological, cultural, heritage or landscape values;
- Set aside for esplanade reserves or access strips that form part of a larger regional or sub-regional reserve network; and
- For local community services and retail facilities, or for schools, hospitals or other district, regional or sub-regional facilities.

**Urban activities in the Future Urban Development Area – Upper Styx (R8) and Rural 3 – Zone Outline Development Plan – Upper Styx (R8);**

**means**

- i. Residential units (except rural residential activities) at a density of more than one household unit per 4 ha of site area;**
- ii. Business activities, including industrial and commercial activities;**
- iii. Sports fields and recreation facilities; and**
- iv. Any other land use within the urban limits.**

***6. Volume 3, Part 2, Living Zones***

*Add new rules as follows:*

**14.0 Rules – Upper Styx (R8) and Living G (Highsted) Zone**

**14.2 Development Standards – Upper Styx (R8)**

**14.2.1 Outline Development Plan – Upper Styx(R8)**

**Any use or development of land for urban activities , shall be in general accordance with the Outline Development Plan in Appendix 7A and 7B to the Rural Zones, and in accordance with a relevant Master Plan.**

**14.2.2 Alternative Locations of Housing Density**

**Where housing densities proposed are not in general accordance with the locations shown on the Outline Development Plan–Upper Styx (R8) and Master Plan, the developer may , as an alternative provide such housing at the nominated densities in other locations within the R8 area so as to comply with the overall density of 15 houses per hectare ,provided there is written legal agreement from the relevant landowners and such relocations ensure that the location of higher density housing complies with the principles of Policy 6.3A.21 (a).**

*Add new Critical Standard as follows:*

**14.5 Critical Standards – Upper Styx (R8)**

**14.5.1 Residential Site Density - Outline Development Plan – Upper Styx (R8) area:**

**(a) The following densities shall be achieved for residential units within the ODP – Upper Styx (R8) area:**

**Density A Minimum net site area 150m<sup>2</sup>. maximum net site area 300m<sup>2</sup>**

**average net site area 225m<sup>2</sup> (+-5%)**

**Density B Minimum net site area 300m<sup>2</sup>. maximum net site area 450m<sup>2</sup>**

**average net site area 375 m<sup>2</sup> (+-5%)**

**Density C Minimum net site area 450m<sup>2</sup>. maximum net site area 1000m<sup>2</sup>**

**average net site area 625 m<sup>2</sup> (+-5% )**

**(b) Rule 14.5.1 (a) shall not apply to residual lots. Residual lots are those lots which are not subject to immediate development but are held over for future urban development. Future development of the lots can only occur if it is in accordance with the densities set in Rule 14.5.1 (a).**

*7. Volume 3, Part 2, Living Zones*

**15.0 Assessment Matters for Resource Consents**

**15.2 Living 1, 1F, H, RS, RV, TMB, 2, 3, 4A, 4B, 4C and G Zones**

*Add a new assessment matter as follows:*

**15.2.68 Non-Compliance with Outline Development Plan – Upper Styx (R8)**

**For activities not in general accordance with the Outline Development Plan –Upper Styx ( R8) contained in Part 4. Appendix 7A and 7B, or not in accordance with the relevant Master Plan :**

- (a) The extent to which comprehensive, integrated development would continue to be achieved across the area eg with respect to roading, sewer, water, stormwater and reserves infrastructure;**
- (b) The ability of the proposal to integrate with the surrounding context;**
- (c) The nature and degree of any adverse effects caused by proposals not in accordance with the Outline Development Plan Upper Styx– R8 or the relevant Master Plan**
- (d) The relationship of proposals to any other existing development within the block;**
- (e) The extent to which the proposal does or does not meet Policy 6.3A.21 and 22;**
- (f) Any environmental benefits created by development not being in general accordance with the Outline Development Plan –Upper Styx (R8) or the relevant Master Plan , eg the ability to avoid geotechnically constrained land or the ability to better enhance the natural or heritage features of the area.**

*8. Volume 3 Part 4, Rural Zones*

*Add new Critical Standard*

**2.5 Critical Standards**

**2.5.16 Outline Development Plan – Upper Styx - Interim Activities**

**New buildings or structures shall not be located within 20 metres of:**

- the elements of the proposed stormwater network;**

- **the indicative proposed roads and pedestrian/cycle accessways;**
- **the indicative local commercial centre; and/or**
- **the proposed extension to Tulett Park**

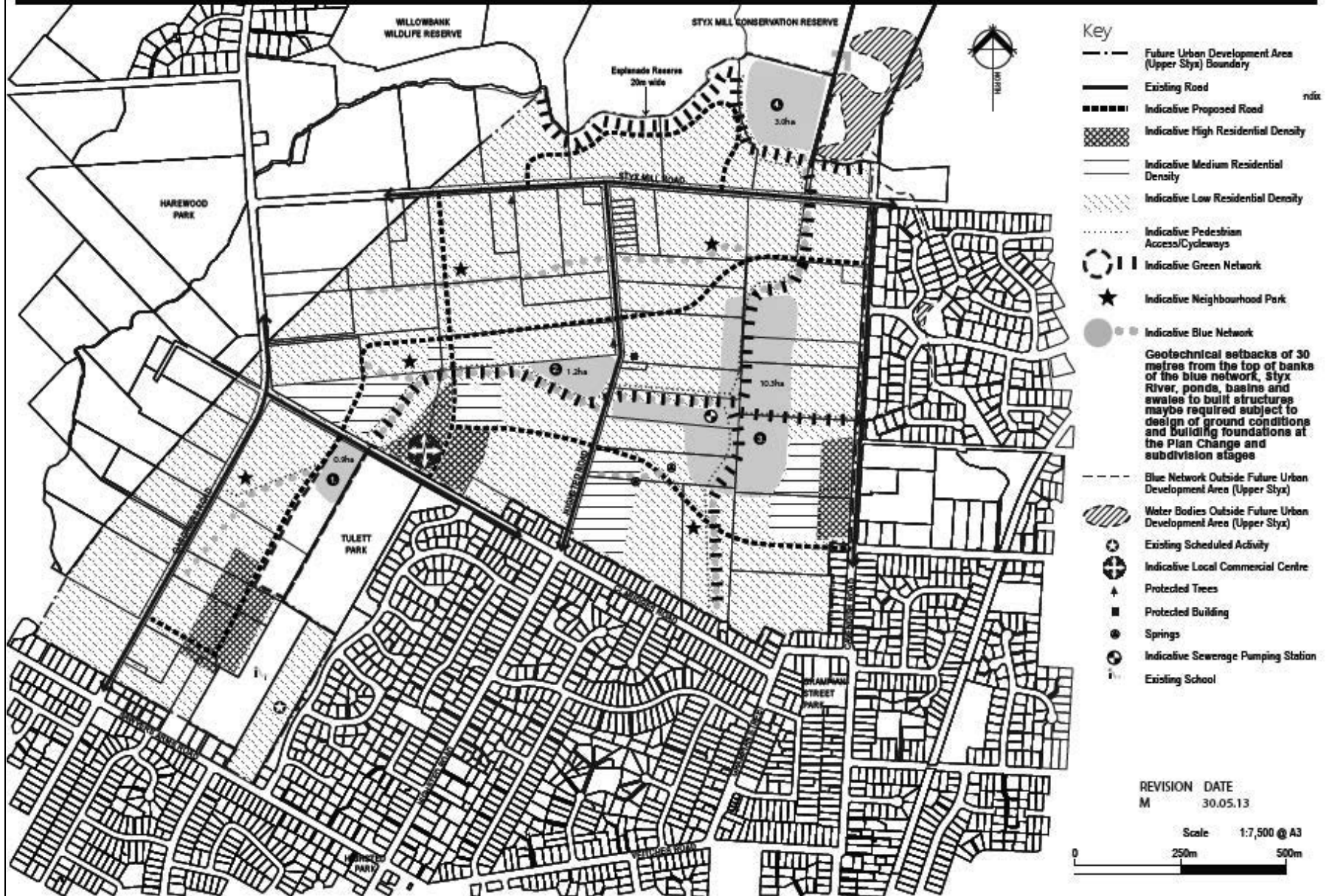
**as shown on the Outline Development Plan for the Upper Styx Area,**

*9. Volume 3 Part 4 Rural 1-7, Appendices*

*Add Outline Development Plan – Upper Styx as Appendix 7A.*

*Add Movement Networks- Upper Styx as Appendix 7B*

# APPENDIX 7A - OUTLINE DEVELOPMENT PLAN - UPPER STYX

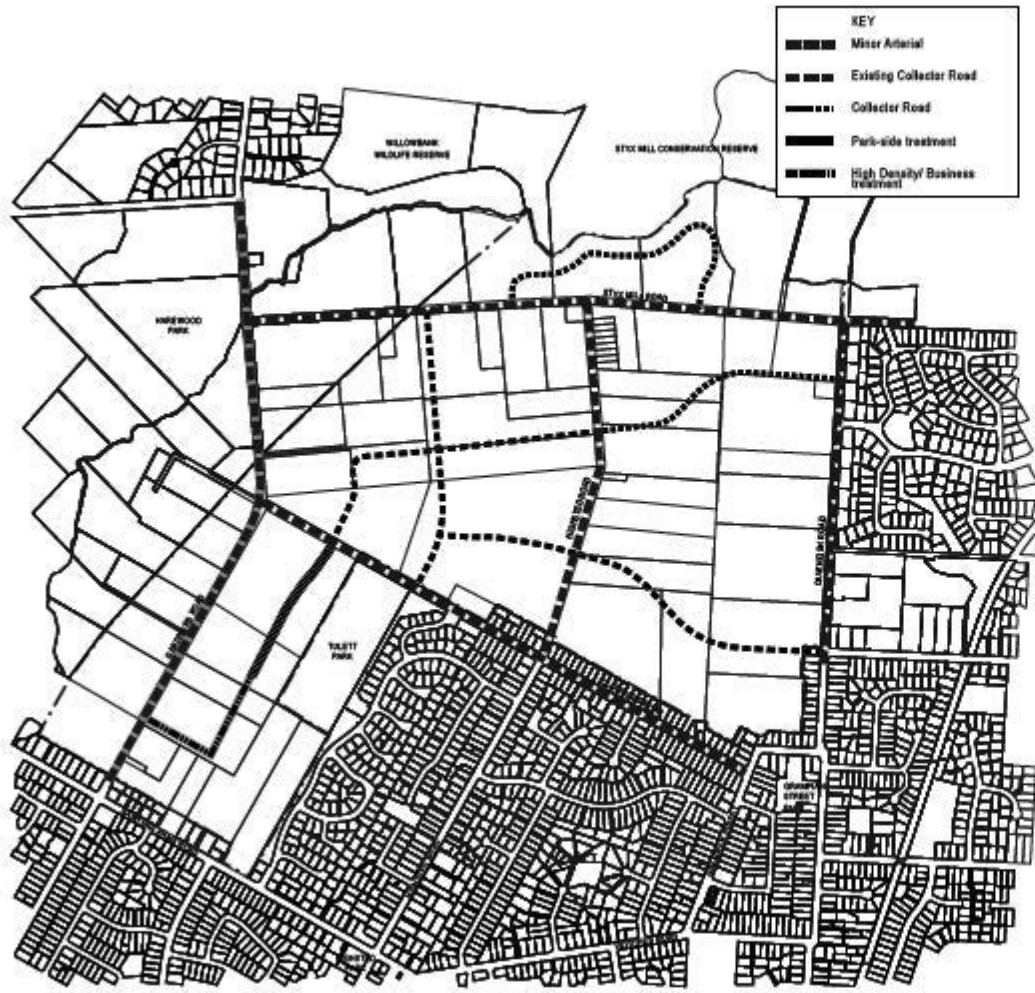


- Key**
- Future Urban Development Area (Upper Styx) Boundary
  - Existing Road
  - Indicative Proposed Road
  - ▨ Indicative High Residential Density
  - ▧ Indicative Medium Residential Density
  - ▩ Indicative Low Residential Density
  - ⋯ Indicative Pedestrian Access/Cycleways
  - Indicative Green Network
  - ★ Indicative Neighbourhood Park
  - Indicative Blue Network
  - Geotechnical setbacks of 30 metres from the top of banks of the blue network, Styx River, ponds, basins and swales to built structures may be required subject to design of ground conditions and building foundations at the Plan Change and subdivision stages
  - Blue Network Outside Future Urban Development Area (Upper Styx)
  - ▨ Water Bodies Outside Future Urban Development Area (Upper Styx)
  - ⊙ Existing Scheduled Activity
  - ⊕ Indicative Local Commercial Centre
  - ▲ Protected Trees
  - Protected Building
  - Springs
  - ⊙ Indicative Sewerage Pumping Station
  - ⌚ Existing School

REVISION DATE  
M 30.05.13

Scale 1:7,500 @ A3  
0 250m 500m

# APPENDIX 7B - MOVEMENT NETWORKS



KEY	
	Minor Arterial
	Existing Collector Road
	Collector Road
	Park-side treatment
	High Density Business treatment



Figure 1: Internal proposed Collector Road - Roadway elements



Figure 2: Highlighted Road existing Collector Road within site - Roadway elements



Figure 3: Park-side treatment - Roadway elements



Figure 4: Collector Road - High Density Business Area - Roadway elements

REVISION DATE  
M 30.05.13

*10. Part 14 Subdivision Rule 4.2 Development standards - Allotment sizes and dimensions*

*Add the following:*

**4.2.5 – Subdivision in Future Urban Development Area – Upper Styx (R8)**

**4.2.5.1 Any complying subdivision in the Future Urban Development Area (R8) while it is zoned Rural 3 or Cultural 3 shall be a restricted discretionary subdivision activity.**

**4.2.5.2 Any subdivision into allotments less than 4 hectares in the Future Development Area – Upper Styx ( R8) while it is zoned Rural 3 or Cultural 3 is a non-complying activity in accordance with Rule 4.3.1.**

*11. Part 14, Clause 4.4 Assessment matters*

*Add a new subclause to Part 14, Clause.4.4 as follows:*

**4.4 Assessment matters for resource consents**

...

**(h) whether the size and dimensions of the proposed allotments and roading would compromise or preclude the integrated development of the Upper Styx Future Urban Development Area including that shown on an Outline Development Plan, both internally and with adjoining urban zonings, and in particular the ability to provide for effective and efficient layouts within the whole Future Urban Development Area for:**

- i. Land use (including density of development);**
- ii. Movement networks (roading, cycleways and pedestrian routes);**
- iii. Blue networks (surface water management);**
- iv. Green networks (parks, open spaces and any other green areas);**
- v. Other network infrastructure; and**
- vi. Provision of convenience shops.**



*12. Part 14 Subdivision*

*Add the following new Development Standards:*

**Rule 32.0 Subdivision in the Outline Development Plan – Upper Styx (R8) area and Living G (Highested zone).**

**32.1 Development Standards – Subdivision in the Outline Development Plan – Upper Styx (R8) Area**

**32.1.1 Any subdivision for urban activities shall be in general accordance with the Outline Development Plan in Appendix 7A and 7B to the Rural Zones, and in accordance with a relevant Master Plan.**

**32.3 Critical Standards – Outline Development Plan Upper Styx (R8) Area**

*Add new Critical Standard:*

**32.3.1 - Residential Allotment Size and Site Density -Outline Development Plan – Upper Styx(R8) area:**

**(a) The following residential allotment sizes and site densities shall be achieved within the ODP – Upper Styx (R8) area:**

**Density A Minimum net site area 150m<sup>2</sup>, maximum net site area 300m<sup>2</sup>  
average net site area 225m<sup>2</sup> (+-5%)**

**Density B Minimum net site area 300m<sup>2</sup>, maximum net site area 450m<sup>2</sup>  
average net site area 375 m<sup>2</sup> (+-5%)**

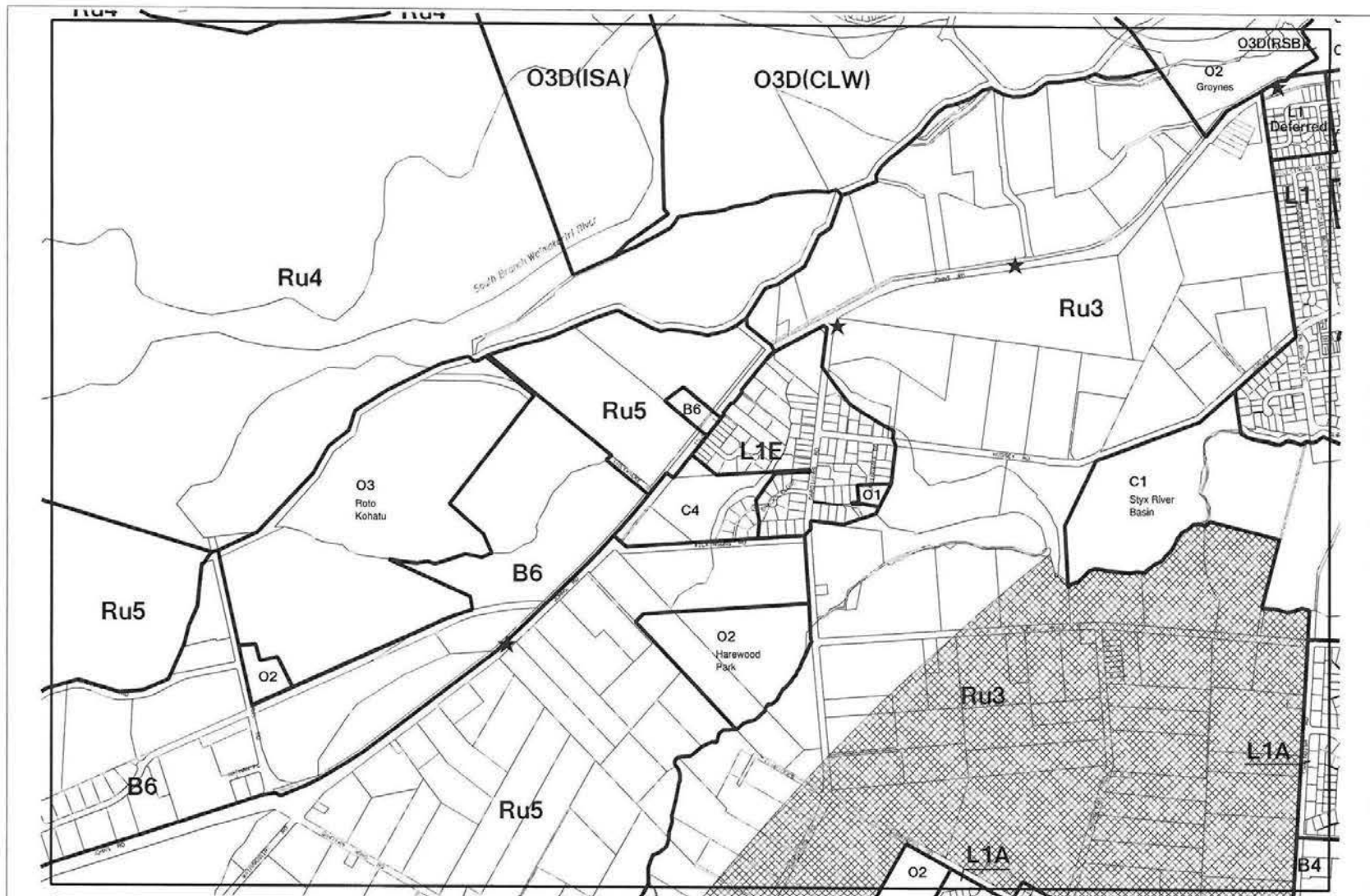
**Density C Minimum net site area 450m<sup>2</sup>, maximum net site area 1000m<sup>2</sup>  
average net site area 625m<sup>2</sup> (+-5%).**

**(b) Rule 32.3.1 (a) shall not apply to residual lots. Residual lots are those lots which are not subject to immediate development but are held over for future urban development.. Future development of the lots can only occur if it is in accordance with the densities set in Rule 32.3.1(a).**

**13. Part 14, Subdivision, Appendix 1 – Esplanade Reserve and Strip Schedule**

**Include the following new entry in the table:**

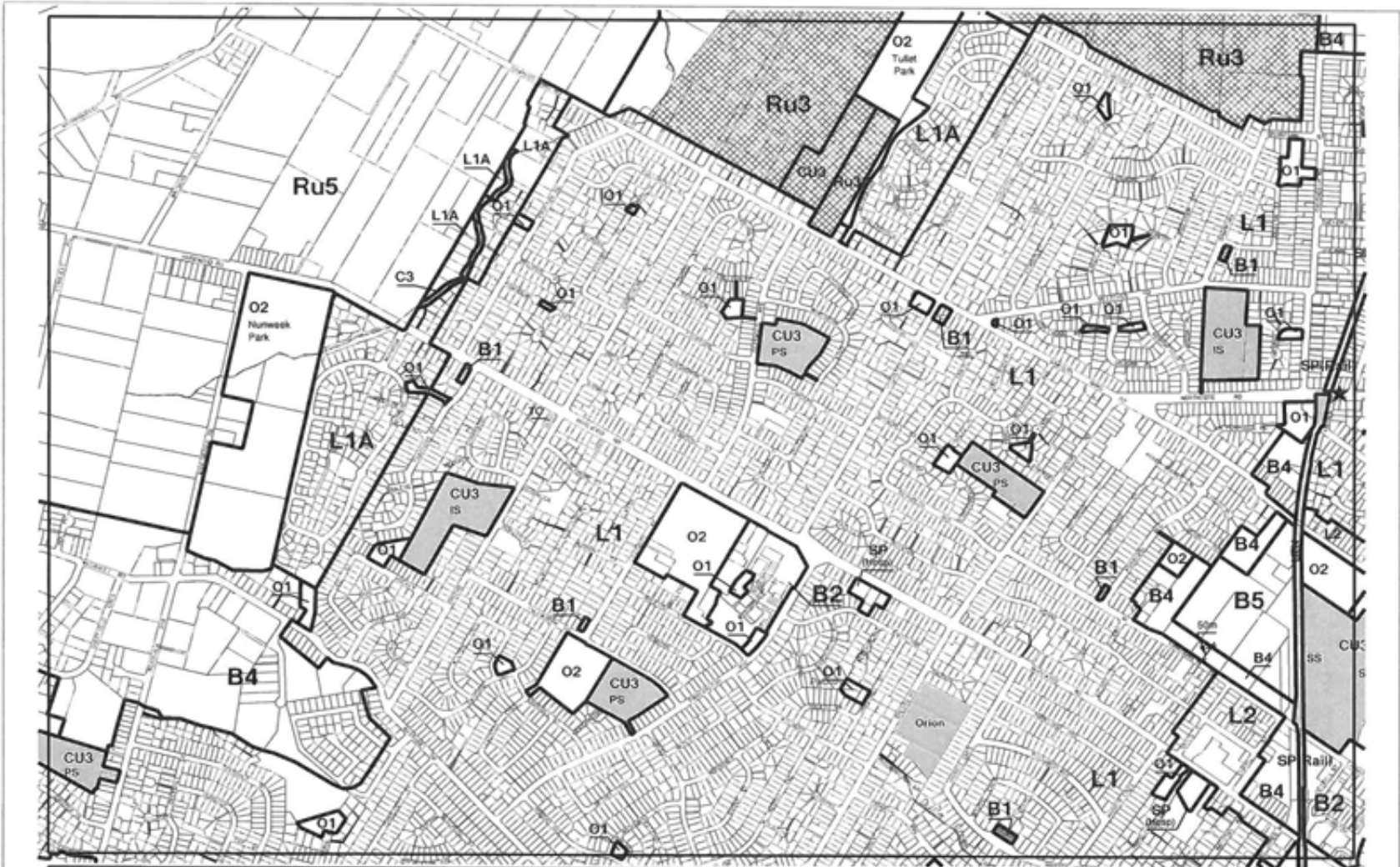
<b>Water Body</b>	<b>Reserve or Strip</b>	<b>Location</b>	<b>Column A</b> (Development standard) (metres)
<b><u>Styx River (middle section)</u></b>	<b><u>Reserve</u></b>	<b><u>True right back, commencing at the west boundary of the Styx River Basin Reserve, thence downstream to the point where the Styx River heads north into the Reserve.</u></b>	<b><u>20</u></b>



9	<b>City of Christchurch</b> City Planning Maps	<b>Key</b>	
16 <b>17A</b> 18		--- City Boundary	CMZ Coastal Marine Area
24		--- Zone Boundary	M.H.W.S. Mean High Water Springs
	--- 160m Contour	★ Major Road Works	Future Urban Development Area (Residential)
	--- Designated Land	☆ Local Road to be Widened	
	--- Non-Operative Area		

Scale 1:15,000 Date 04/09/2012

All spiritual facilities (see Part 1) existing as at 24/6/95 and located in living or rural zones are deemed to be Class 2 scheduled activities (see Part 9).



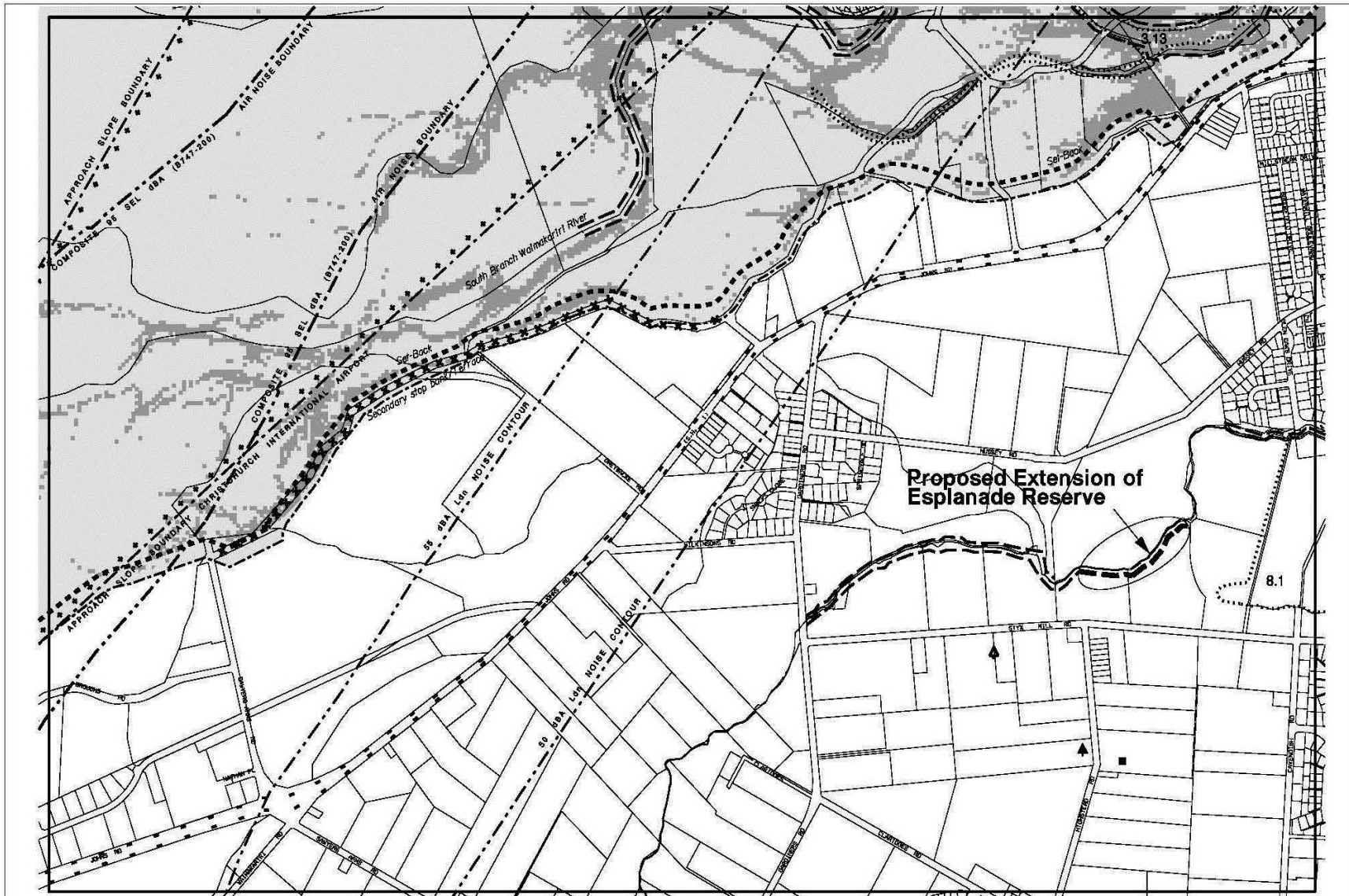
<table border="1"> <tr><td>17</td></tr> <tr><td>23 24A 25</td></tr> <tr><td>31</td></tr> </table>	17	23 24A 25	31	<p><b>City of Christchurch</b> City Planning Maps</p> <p>Scale 1:15,000 Date 04/09/2012</p>	<p><b>Key</b></p> <ul style="list-style-type: none"> <li> City Boundary</li> <li> Zone Boundary</li> <li> 160m Contour</li> <li> Designated Land</li> <li> Non-Operative Area</li> </ul>	<ul style="list-style-type: none"> <li> Coastal Marine Area</li> <li> Mean High Water Springs</li> <li> Major Road Works</li> <li> Local Road to be Widened</li> </ul>	<ul style="list-style-type: none"> <li> Future Urban Development Area (Residential)</li> </ul>
17							
23 24A 25							
31							

All spiritual facilities (see Part 1) existing as at 24/6/95 and located in living or rural zones are deemed to be Class 2 scheduled activities (see Part 9).

Business & Information Services  
Corporate Support  
Christchurch City Council

**Proposed Plan Change 71 - Upper Styx Future Urban Development Area and Outline Development Plan.**

pc\_71-24a.dgn  
Date 04/09/2012



**Key .... cont'd**

- |  |   |   |  |
|--|---|---|--|
| <ul style="list-style-type: none"> <li>--- City Boundary</li> <li>--- Summit Road Protection Act</li> <li>--- Airport/Airfield Approach Slope Boundaries</li> <li>--- Airport/Airfield Noise Control Boundaries</li> <li>--- Pylons</li> <li>..... Hazard line 1</li> <li>--- Esplanade Reserve or strip required</li> <li>● Scheduled Activity</li> <li>● M.H.W.S. Mean High Water Springs</li> </ul> | <ul style="list-style-type: none"> <li>✕ ✕ ✕ Road to be stopped</li> <li>--- Limited Access Roads</li> <li>--- Indicative Roads</li> <li>--- Protected Object, Place or Building</li> <li>▲ Protected Tree</li> <li>▲ Group of Protected Trees</li> <li>□ Non-Operative Area</li> </ul> | <ul style="list-style-type: none"> <li>□ Waimakariri River stopbank floodplain (pc32 non-operative)</li> <li>□ High hazard area (pc32 non-operative)</li> <li>□ Area exempted from Waimakariri River Stopbank Floodplain flood protection provisions (pc32 non-operative)</li> <li>□ Flood Ponding Area</li> <li>□ Flood Management Area</li> </ul> | <ul style="list-style-type: none"> <li>□ Special Amenity Area</li> <li>□ Community Footprint</li> <li>□ Ecological Heritage "A" Sites</li> <li>□ Comprehensive Housing Improvement Area</li> </ul> |
|--|---|---|--|

**City of Christchurch**  
**City Planning Maps**  
 Scale 1:15,000 Date 25/08/2012  
 0 100 200 300 400m NORTH

9
16 <b>17B</b> 18
24

Business & Information Services  
 Corporate Support  
 Christchurch City Council

**Proposed Plan Change 71 - Upper Styx Future Urban Development Area and Outline Development Plan.**

pc\_71-17b.dgn  
 Date 11/12/2012

## **Highsted – Rezoning to Living G**

Amend the City Plan as follows:

### **Volume 2, Section 10, Subdivision and Development**

Add new policy as follows:

#### **10.3.10 Policy: Sustainable subdivision design (Highsted)**

**(a) To ensure subdivision and development of land occurs in a comprehensive and integrated manner with sustainable and energy efficient outcomes in general accordance with the pattern of development shown on the Highsted Master Plan in Appendix 3Z, Part 2, Volume 3.**

**(b) To achieve a mix of residential densities that gives effect to the minimum net residential densities shown on the Outline Development Plan - Upper Styx (R8).**

**(c) To ensure integration of the Green, Blue and Movement Networks takes place within the zone and to adjoining areas.**

**(d) To avoid subdivision for development until such time as sites are able to be serviced by stormwater facilities in accordance with discharge consent application CRC131249 and the Styx River/Puruakanui Area Stormwater Management Plan.**

**(e) To avoid subdivision for development until risks from site contamination are removed or appropriately mitigated.**

**(f) To avoid subdivision for development unless sites are provided as Technical Category 1 and Technical Category 2 and buildings and structures are not exposed to an unacceptable risk of damage from lateral spread.**

### **Volume 2, Section 11, Living**

Add new Policy as follows:

#### **11.7.17 Highsted**

**To ensure Highsted:**

**(a) provides a diverse range of housing and household choice with energy efficient outcomes.**

**(b) fosters a strong sense of community identity and all residential areas are developed in accordance with the principles of good urban design. In particular, development of high density residential areas (Density A) shall;**

- (i) consider the amenity of residents, neighbours and the wider community;**
- (ii) ensure that residential units are designed to enable their engagement with the street ensuring community safety, social interaction, and visual interest;**
- (iii) avoid excessive bulk or repetition and is of a domestic appearance, human scale, visually interesting, and not dominated by car parking and garaging;**
- (iv) be softened by trees and other landscaping on-site;**
- (v) provide accessible and integrated utility areas and accessible, usable and attractive outdoor living spaces that achieve levels of privacy, access to sunlight and insulation from traffic noise.**

**(c) delivers sustainable outcomes in general accordance with the Highsted Master Plan that enables:**

- (i) the creation of high quality open spaces, including the enhancement of district sport park facilities through an extension of Tulett Park, neighbourhood parks, and pedestrian and cycle links.**
- (ii) the integrated management of stormwater and waterways in accordance with discharge consent application CRC131249 and the Styx River/Puruakanui Area Stormwater Management Plan, and where appropriate, the realignment and naturalisation of waterways, the retention of features of heritage value relating to drainage, and recognition of tangata whenua values through the enhancement of water quality and the use of indigenous vegetation.**
- (iii) A well-connected, safe and efficient movement network that establishes an integrated road hierarchy and provides for shared pedestrian and cycle pathways.**

**(d) provides a small scale neighbourhood retail centre with a maximum gross floor area of 2,000m<sup>2</sup> located adjacent to high density housing, public transport routes and Tulett Park that provides a high standard of amenity and visual character when viewed from the street.**

Add the following new rules to Volume 3: Part 2 Living Zones:

**14. RULES – [Upper Styx (R8) and] Living G (Highsted) Zone**

**14.1 Categories of activities**

**14.1.1 Residential activities**

**(a) Any residential activity which complies with:**

- **all of the development standards under Clause 14.2 and 14.3; and**
- **all of the community standards under Clause 14.4; and**
- **all of the critical standards under Clause 14.5 and 14.6**  
**and is not a prohibited activity, shall be a permitted activity.**

**(b) Any residential activity which complies with all of the critical standards under Clause 14.5 and 14.6, but does not comply with any one or more of the development standards in Clause 14.2 and 14.3 shall be a restricted discretionary activity with the exercise of the Council's discretion limited to the matter(s) subject to that standard.**

**(c) Any residential activity that does not comply with any one or more of the community standards in Clause 14.4 shall be a discretionary activity.**

**(d) Any residential activity which does not comply with any one or more of the critical standards in Clause 14.5 and 14.6 shall be a non-complying activity.**

**(e) Clarification of categories of activities:**

**The standards may also specify that an activity is discretionary or controlled (where there is a non-compliance with development standards) with the exercise of the Council's discretion limited to the matter(s) subject to that standard.**

**14.1.2 Other activities**

**(a) Any other activity which complies with:**

- **all of the development standards under Clause 14.2 and 14.3; and**
- **all of the community standards under Clause 14.4; and**
- **all of the critical standards under Clause 14.5 and 14.6**  
**and is not a prohibited activity, shall be a permitted activity.**

**(b) Any other activity which complies with all of the community standards and critical standards, but does not comply with any one or more of the development standards in Clause 14.2 and 14.3 shall be a restricted discretionary activity with the exercise of the Council's discretion limited to the matter(s) subject to that standard.**



**(c) Any activity other than a prohibited activity, which complies all of the critical standards, but does not comply with any one or more of the community standards in Clause 14.4 shall be a discretionary activity.**

**(d) Any other activity other than a prohibited activity, which does not comply with any one or more of the critical standards in Clause 14.5 and 14.6, shall be a non-complying activity.**

**14.3 Development Standards – Living G (Highsted) - All Residential Areas**

**(a) Any application arising from the following clauses will not require the written consent of other persons and shall be non-notified:**

- **14.3.4 Street scene – residential and other activities.**
- **14.3.8 Outdoor living space – residential activities.**
- **14.3.11 Service and storage spaces for Density A Residential Sites.**
- **14.3.12 Fencing on sites adjoining green and blue networks – residential and other activities.**
- **14.3.15 Ground floor habitable room and dwelling orientation to the street.**
- **14.3.16 Urban design and amenity for Density A residential sites – residential and other activities.**

**(b) The Development Standards do not apply to the land identified as Business 1 Commercial Area on the Master Plan (Highsted)(Appendix 3Z). For this area the Business 1 Zone rules shall apply.**

**14.3.1 Open space – residential and other activities**

**The maximum percentage of:**

**(a) The maximum percentage of the net area of the site covered by buildings shall be:**

	<b><u>Residential activities with garage provided</u></b>	<b><u>Other activities and, residential activities without garage provided</u></b>
<b><u>Density A residential areas</u></b>	<b><u>40%</u></b>	<b><u>40% less 18m<sup>2</sup></u></b>
<b><u>Density B residential area</u></b>	<b><u>40%</u></b>	<b><u>40% less 18m<sup>2</sup></u></b>

<b><u>Density C residential area</u></b>	<b><u>35%</u></b>	<b><u>35% less 18m<sup>2</sup></u></b>
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- (b) **The percentage of the site covered by paved impermeable surfaces (excluding garage and dwelling) shall be no more than 25%.**

**Refer also to critical standards for open space – Clause 14.6.1**

#### **14.3.2 Building height – residential and other activities**

**The maximum height of any buildings shall be:**

- **Density A residential areas: 11m**
- **Density B and C residential area: 8m**

**Except that where there is an internal boundary to a site that is within a different density area as specified in Clause 14.5.1 [Note: Clause 14.5.1 is introduced by Plan Change 71] the more restrictive maximum height shall apply to both sites other than where that part of the building that exceeds 8m in height is more than 8 metres from the boundary.**

#### **14.3.3 Sunlight and outlook for neighbours – residential and other activities**

- a) **Buildings shall not project beyond a building envelope constructed by recession planes from points 2.3m above ground level on internal boundaries, as shown in Part 2, Appendix 1, as follows:**

**Density A Residential areas where they adjoin Density B and Density C areas only - Part 2, Appendix 1, Diagram D**

**Density B Residential areas – Part 2, Appendix 1, Diagram D**

**Density C Residential area - Part 2, Appendix 1, Diagram A**

**except that:**

- (i) **Where an internal boundary of a site immediately adjoins an access or part of an access the recession planes shall be constructed from points 2.3m above the far side of the access.**
- (ii) **Where buildings on adjoining sites have a common wall along an internal boundary or a zero building setback boundary wall the recession planes shall not apply along that part of the boundary covered**

by such a wall.

(iii) Other than where (ii) applies, where there is an internal boundary between sites that are in different density areas as specified in Clause 14.5.1 [Note: Clause 14.5.1 is introduced by Plan Change 71] the more restrictive recession plane shall apply to both sites.

b) The level of internal boundaries shall be measured from filled ground level except where the site on the other side of the internal boundary is at a lower level, then that lower level shall be adopted.

#### 14.3.4 Street scene – residential and other activities

(a) Minimum building setback from road boundaries shall be as follows:

Density A residential area 2m

Density B residential area 2m

Density C residential area 3m

except that:

- (i) Where a garage has a vehicle door generally facing a road or shared access, the minimum setback of the garage door shall be 5.5m from the road boundary or shared access; and
- (ii) In Density A areas, garages, carports and other accessory buildings (excluding basement carparking and swimming pools) shall be located no further forward than the front façade of any ground level habitable room of a residential unit.

(b) Any fence located on the road boundary or in the minimum building setback from road boundaries shall have a maximum height of 1m, except that where a fence or other screening structure is over 1m in height then the whole of that structure shall be at least 50% visually transparent. No fencing or other screening structure shall exceed a height of 2m.

(c) Garage doors and carport entrance ways shall not comprise more than 50% of any ground floor elevation viewed from any road boundary and shall not be more than 6m wide.

#### 14.3.5 Separation from neighbours – residential and other activities

The minimum building setback from internal boundaries shall be 1.8m, except that:

- (a) In Density B and C areas, accessory buildings may be located within 1.8m of internal boundaries where the total length of walls or parts of accessory buildings facing, and located within 1.8m of each internal boundary, does not exceed 10.1m in length.
- (b) In Density A areas, terraced or semi-detached residential units, including units with attached garages, may be built to the internal side boundaries (i.e. boundaries other than the road and rear boundaries).  
except that:
- (i) Where the end facade of a row of terraced residential units face internal boundaries no part of the building shall be located within the applicable setbacks for those internal boundaries;
  - (ii) There shall be no windows on the wall at the internal boundary permitted in any such building unless windows are non-opening and glazed w with opaque glass.
- (c) Where an internal boundary of a site immediately adjoins an access or part of an access, the minimum building setback (except accessory buildings) from that internal boundary shall be 1m. For Density A areas, no setback is required where a building adjoins an access lot or access strip on the same site provided any windows on the ground storey facade facing the access and in 1m of the access lot or access strip are non-opening.
- (d) For all residential activities any part of any balcony or any window of a living area at first floor level or above shall not be located within 4m of any internal boundary provided that:
- (i) this shall not apply to a window at an angle of 90° or greater to the boundary; and
  - (ii) first floor level shall not include any part of a window or balcony within 1.2m of ground level (such as above a garage which is partly below ground level. For an explanation see the diagram in relation to Clause 2.2.6 in this part of the City Plan).
- (e) For all residential activities, where a window of a ground floor living area of a residential unit faces an internal boundary, the window shall be set back a minimum of 3m from the internal boundary provided that:
- (i) where an internal boundary of a site immediately adjoins an access or part of an access, the setback shall be 1m measured from the internal boundary; and

**(ii) this shall not apply to a window at an angle of 90° or greater to the boundary. For an explanation see diagram in relation to Clause 2.2.6 in this part of the City Plan.**

**(f) No setback is required for basements, provided that any part of a basement located in 1.8m of an internal boundary is wholly below ground level.**

**14.3.6 Continuous building length – ridgelines and parapets – residential and other activities**

**No length of any ridgeline/s and/or horizontal parapet/s of a building, or buildings separated by a length of less than 3.6m (from ridgelines and/or parapets to ridgeline and/or parapet), combined with the length of any distance/s between the ridgeline/s and/or horizontal parapet/s shall exceed 20m without providing either a horizontal step of at least 2m, or a vertical step of at least 1m. The minimum length of all steps shall be 6m**

**except that:**

**(i) This rule shall not apply to any part of a ridgeline and/or horizontal parapet which is more than 10m from every internal boundary and more than 6m from every road boundary;**

**(ii) Where a step occurs within 6m of the end of the ridgeline and/or horizontal parapet at the end of the building, the length of that step need equal only the remaining length of the ridgeline and or horizontal parapet.**

**Refer to Appendix 1A and the definitions of step, length and ridgeline for further clarification of this rule.**

**This rule does not apply to any development in the Density A Residential area subject to Rule 14.3.16.**

**14.3.7 Continuous building length – exterior walls – residential and other activities**

**(a) Subject to (b) below, steps shall be provided along the length of exterior walls in accordance with the following table:**

<b><u>Length of exterior wall</u></b>	<b><u>Minimum number of steps</u></b>
<b><u>≤20m</u></b>	<b><u>0</u></b>
<b><u>&gt;20m ≤ 24m</u></b>	<b><u>1</u></b>

<u>&gt;24m ≤ 28m</u>	<u>2</u>
<u>&gt;28m ≤ 32m</u>	<u>3</u>
<u>&gt;32m</u>	<u>4 + 1 for every additional 10m of length over 32m</u>

(b) Where steps are required by (a) above:

- (i) One step shall have a minimum depth of 2m. Any steps required thereafter shall have a minimum depth of 1m.
- (ii) One step shall have a minimum length of 2m. Any steps required thereafter shall have a minimum length of 4m.
- (iii) No length of any exterior wall shall exceed 20m without a step of the required dimension having commenced.
- (iv) The required steps shall be provided at all levels of the exterior wall, except that:
  - This rule shall not apply to any part of an exterior wall which is more than 10m from every internal boundary and more than 6m from every road boundary.
  - Where no part of a building exceeds 5.5m in height, this rule shall not apply to any exterior wall of less than 28m in length.

Refer to Appendix 1A and the definitions of step, length and ridgeline for further clarification of this rule.

This rule does not apply to any development in the Density A Residential area subject to Rule 14.3.16.

#### 14.3.8 Outdoor living space – residential activities

(a) Each residential unit with a room or garage on the ground floor shall be provided with an outdoor living space in a contiguous area, contained in the net area of the site with a minimum area and dimension as follows:

	<u>Minimum area</u>	<u>Minimum Dimension</u>
<u>Density A</u>	<u>30m<sup>2</sup></u>	<u>4m</u>
<u>Density B</u>	<u>50m<sup>2</sup></u>	<u>4m</u>
<u>Density C</u>	<u>70m<sup>2</sup></u>	<u>4m</u>

(b) The required minimum area shall be readily accessible from a living area of each unit. At least half of the required minimum area shall be able to receive sunshine on the shortest day of the year.

**Note for clarification: This rule applies only to structures on the same site. Outdoor living spaces in the Density A area are also subject to assessment criteria under Rule 14.3.16.**

**(c) The required minimum area shall not be occupied by any building, access or parking space, other than:**

- **An outdoor swimming pool; or**
- **An accessory building of less than 8m<sup>2</sup>; or**
- **Any building or parts of a building without walls (other than a balustrade) on at least a quarter of its perimeter, and which occupies no more than 30% of the area of the outdoor living space.**

#### **14.3.9 Family flats – residential activities in Density B and C only**

**(a) Family flats shall have a maximum gross floor area, excluding terraces, garages, sundecks and verandahs, of 65m<sup>2</sup>.**

**(b) Once the building is no longer being used as a family flat and where the family flat does not comply with all the standards for a residential unit:**

- (i) the family flat shall be relocated from the site; or**
- (ii) the family flat shall have the kitchen removed so that the flat is no longer a self-contained residential unit.**

#### **14.3.10 Screening from neighbours – other activities**

**Parking and outdoor storage areas shall be screened from adjoining sites by landscaping, wall(s), fence(s) or a combination thereof to a height of at least 1.5m along the length of the parking or storage area. Where the screening is to be provided by way of landscaping, the 1.5m minimum height standard is to be achieved at the time of planting.**

#### **14.3.11 Service and storage spaces for Density A Residential Area – residential activities**

**(a) Each residential site in Density A residential area shall be provided with:**

- (i) outdoor service, rubbish, and recycling space of 5m<sup>2</sup> with a minimum dimension of 1.5m; and**
- (ii) a single, indoor storage space of 4m<sup>2</sup> with a minimum dimension of 1m. Except that if a communal outdoor service, rubbish, and recycling space with a minimum area of 10m<sup>2</sup> is provided in the site, the outdoor service, rubbish and recycling space may reduce to 3m<sup>2</sup> for each unit.**

- (b) Each outdoor service, rubbish, and recycling space shall not be located between the road boundary and any habitable room and shall be screened to a height of 1.5 metres from adjoining sites, conservation or open space zones, roads, and adjoining outdoor living spaces.

**14.3.12 Fencing on sites adjoining the Green and Blue Network – residential and other activities**

**Fencing of sites that adjoin the green and blue network shown on the Highest Master Plan shall have a maximum height of 1m, except that where a fence is over 1m in height, then the whole of that fence shall be at least 50% visually transparent. No fencing shall exceed a height of 2m.**

**14.3.13 Restrictions on outdoor activities - other activities**

**All manufacturing, altering, repairing, dismantling or processing of any materials, goods or articles shall be carried out inside a building.**

**14.3.14 Other activities – Noise from pre-schools**

**Pre-schools shall be a restricted discretionary activity with the exercise of the Council's discretion limited to consideration of the effects of noise from the location of outdoor activities and facilities associated with this activity.**

**14.3.15 Ground floor habitable room and orientation to the street - residential activities**

- (a) **Each residential building shall have a habitable room located at the ground level which is internally accessible to the rest of the unit.**
- (b) **The ground floor habitable rooms shall provide a total window area of at least 3m<sup>2</sup> that overlook the setback, if any, from the road boundary.**

**14.3.16 Urban design and amenity for Density A Residential Area - residential and other activities.**

- (a) **Except where new buildings have been included as part of subdivision consent granted pursuant to Part 14, Volume 3, Rule 32.2.1, the erection of new buildings, including accessory buildings, shall be a restricted discretionary activity with the exercise of the Council's discretion limited to matters of urban design and amenity of the site and development thereon as set out in**



**Assessment Matters 15.2.70.**

**(b) Alterations or additions to existing buildings, including accessory buildings, shall be a restricted discretionary activity with the exercise of the Council's discretion limited to the urban design and amenity of the site and development thereon as set out in Assessment Matters 15.2.70.**

**14.3.17 Retailing - other activities**

**Retail activities involving the sale of goods grown or produced on the site shall be a discretionary activity with the exercise of the Council's discretion limited to the impact on the surrounding living environment.**

**(Refer also to the critical standards for retailing – Clause 14.6.4 which means that the sale of goods other than those grown or produced on the site is a non-complying activity).**

**14.3.18 Geotechnical Setbacks from Stormwater Facilities**

**The erection of buildings or structures shall be a restricted discretionary activity within the geotechnical setbacks specified below. These setbacks and the point of origin of their measurement are not the same as waterway setbacks required under Clause 9.5.2.4 of the City Plan.**

**The Council's discretion will be limited to the assessment matters specified in Clause 15.2.71 below.**

	<b><u>Setback Distance</u></b>	<b><u>Point of origin of measurement</u></b>
<b><u>Waterway corridors, stormwater basins/facilities</u></b>	<b><u>30m</u></b>	<b><u>Crest of slope</u></b>

**14.4 Community Standards – Living G (Highsted) zone**

**The community standards do not apply to the land identified as being Business 1 Commercial Area on the Master Plan (Highsted)(Appendix 3Z). For those areas, the Business 1 Zone rules shall apply.**

**14.4.1 Scale of activity – other activities**

**(a) The maximum gross floor area of buildings plus the area of any outdoor storage, used for activities other than residential activities, shall be 40m<sup>2</sup> or 30% of the gross floor area of all buildings on the site, whichever is the larger, except where an activity is an educational, spiritual, day-care, or health facility.**

**(b) No more than one full-time equivalent person, who resides permanently elsewhere than on the site may be employed in undertaking any activity on the site except where the activity is an educational, spiritual, day-care, or health facility.**

**14.4.2 Site size – other activities**

**The maximum net area of any site for activities other than residential activities shall be 1100m<sup>2</sup> except:**

**(a) Where the activity occupies not more than 40m<sup>2</sup> of floor space and at least one person engaged in the activity resides permanently on the site, the maximum net area of any site for activities other than residential activities shall be 1375m<sup>2</sup>.**

**14.4.3 Hours of operation - other activities**

**(a) The maximum total number of hours the site shall be open to visitors, clients or deliveries for any activity other than a residential activity shall be 50 hours per week.**

**(b) Hours of operation, including all related visitors, clients and deliveries to the site, shall be limited to between the hours:**

**0700 – 2300 Monday to Friday, and**

**0800 – 2300 Saturday, Sunday and public holidays**

**except that:**

**(i) where the activity occupies not more than 40m<sup>2</sup> of floor space, and**

**(ii) where each person engaged in the activity outside the above hours resides permanently on the site, and**

**(iii) there are no visitors, clients or deliveries to or from the site outside the above hours.**

**Refer also to city rules – (Part 11, Clause 1 – Noise)**

**14.4.4 Traffic generation – other activities**

**(a) The maximum number of vehicle trips per site shall be:**

**(i) Sites where access is shared with at least one other site:**

**Heavy vehicles 2 per week**

**Other vehicles 16 per day**

**(ii) Sites with frontage to local roads, other than (i) above:**

**Heavy vehicles 2 per week**

**Other vehicles 32 per day**

**(iii) All other sites:**

**Heavy vehicles 4 per week**

**Other vehicles 50 per day**

**except that for educational, spiritual, day-care and health facilities the maximum number of vehicle trips per site shall be:**

**Collector and arterial roads: 100 per day**

**(b) Vehicles, other than heavy vehicles, associated with any residential activity on the site shall be included in determining the number of vehicle trips to and from any site. Vehicles parking on the street or on any other site, in order that their occupants can visit the site, shall also be included in determining the number of vehicle trips to and from any site.**

**14.4.5 Building size and separation – Density B and C Residential Areas residential and other activities**

**(a) The maximum gross floor area of any single residential unit shall be 550m<sup>2</sup>.**

**(b) Where buildings located on the same site each have a gross floor area of greater than 100m<sup>2</sup>, they shall be separated from each other by not less than 3.6 metres.**

**14.4.6 Residential coherence – other activities**

**At least one person engaged in the activity shall reside permanently on the site, except where the activity is an educational, spiritual, day-care or health facility and is located:**

- (a) On a front site with frontage to a collector or arterial road.
- (b) Any residential activity on an adjoining front site or front site separated by an access with the frontage to the same road is left with at least one residential neighbour. For the purpose of this clause, the residential neighbour shall be on an adjoining front site or front site separated by an access and have frontage to the same road.
- (c) The residential block is not left with more than two non-residential activities in that block (for an explanation of this Clause, see the diagram in Part 2, Clause 2.3.7).

**14.6 Critical standards – Living G (Highsted) zone**

**14.6.1 Open space – residential and other activities**

- (a) The maximum percentage of the net area of the site covered by buildings shall be:

	<u>Residential activities with garage provided</u>	<u>Other activities and, Residential activities without garage provided</u>
<u>Density A</u>	<u>50%</u>	<u>50% less 18m<sup>2</sup></u>
<u>Density B</u>	<u>45%</u>	<u>45% less 18m<sup>2</sup></u>
<u>Density C</u>	<u>40%</u>	<u>40% less 18m<sup>2</sup></u>

- (i) For elderly persons housing complexes the percentage coverage by buildings shall be calculated over the net area of the site of the entire complex, rather than over the net area of the site of any part of the complex.
- (b) In Density B and C residential areas the maximum percentage of the site covered by paved impermeable surface (excluding garage and dwelling) shall be 30%.
- (c) In Density A residential areas the maximum percentage of the site covered by paved impermeable surface and buildings shall be 80%.

**Refer also to the development standards for open space – Clause 14.3.1**

#### **14.6.2 Boarding of animals – other activity**

**Boarding of animals on a site shall be limited to a maximum of four animals in the care of a registered veterinarian for medical or surgical purposes only.**

#### **14.6.3 Dismantling or repair of motor vehicles – other activities**

**There shall be no dismantling or repair of motor vehicles, including the storage of such vehicles.**

#### **14.6.4 Retailing – other activities**

**Retail activities shall be limited to the sale of grown or produced on the site.**

**(Refer also to the development standards for retailing – Clause 14.3.17 which means that the sale of goods grown or produced on the site is a discretionary activity in respect of that standard.)**

### **15.0 Assessment matters for resource consents**

#### **15.2 Living 1, 1F, H, RS, RV, TMB, 2, 3, 4A, 4B, 4C, and G Zones**

##### **15.2.1 Site density and open space**

Delete clause (l) and (m) and replace with the following:

- (l) In the Living G (Yaldhurst) (Awatea) (Wigram) (Prestons) (Halswell West) (East Belfast) and (Highsted) Zones, the extent to which the site density and 'green network' provisions indicated in the Outline Development Plan and for Highsted the Master Plan and supporting principles for these zones are given effect to.**
- (m) In the Living G (Yaldhurst) Zone, and the Living G (Awatea) Zone, and the Living G (Wigram) Zone, and the Living G (Prestons) Zones, and the Living G (East Belfast) Zone, and the Living G (Halswell West) Zones, and the Living G (Highsted) Zone the extent to which high density (A) or (B) (Yaldhurst) areas, and Density A areas and residential activities in the Urban Village (Prestons), Density A areas (Awatea), and Density A and B areas (Wigram), and Density A areas (East Belfast), and Density A and B areas (Halswell West), and Density A areas (Highsted) are located adjacent to areas of green space so as to provide for compensating open space amenity.**

##### **15.2.2 Building height, and outlook for neighbours**

Delete clause (l) and replace with the following:

- (l) **In the Living G (Yaldhurst) Zone and the Living G (East Belfast) Zone and the Living G (Awatea) Zone, and the Living G (Wigram) Zone and the Living G (Prestons) Zone and the Living G (Halswell West) Zones, and the Living G (Highsted) Zone the extent to which the character of the living areas surrounding high density (A) or (B) (Yaldhurst) and the High Density A (Awatea), and the Density ATC and A (Wigram) and Density A areas and residential activities in the Urban Village (Prestons) and Density A and B (Halswell West), and Density A areas (Highsted) Zone development remains reasonably open rather than being dominated by buildings.**

### 15.2.3 Street scene

(a) All Street scene resource consents.

Add new clause (xvi) as follows:

- (xvi) **In the Living G (Highsted) Zone:**
- **The extent to which species are included in Appendix 2B – Plant Species for Living G (Highsted) Zone.**
  - **The extent to which a reduction in visual transparency may be more visually appropriate or suited to the character of the site or area.**
  - **The extent to which a reduction in visual transparency or use of non-transparent materials may be appropriate to provide levels of privacy or security.**
  - **The extent to which the front fence is varied in terms of incorporating steps, changes in height, variety in materials, incorporates landscaping, and avoids presenting a blank, solid facade to the street.**
  - **The extent to which any reduction in public surveillance over public open space areas may lead to or promote unsafe or insecure environments.**

### 15.2.5 Separation from neighbours

Delete clause (n) and replace with the following:

- (n) **In the Living G (Yaldhurst) Zone and the Living G (East Belfast) Zone and the Living G (Awatea) Zone, and the Living G (Wigram) Zone and the Living G (Prestons) Zone and the Living G (Halswell West) Zone, and the Living G (Highsted) Zone the extent to which buildings designed to achieve High Densities (A) or (B) (Yaldhurst) and High Density A (Awatea) and Density**

**ATC, A or B (Wigram) and Density A areas and residential activities in the Urban Village (Prestons) and High Density A and B (Halswell West) and High Density A (Highsted) may dictate that setbacks are either unnecessary and/or may be dispensed with. Provided that this clause shall not apply in the case of the 9.5m setback from Halswell Junction Road required under Rule 8.2.5 and Rule 11.4.3.**

#### **15.2.14 Outdoor living space**

Delete clauses (f) and (g) and replace with the following:

**(f) In the Living G (Yaldhurst) (East Belfast) (Awatea) (Wigram) (Prestons) (Halswell West) and (Highsted) Zones, the extent to which any outdoor living space or fenced court area intrudes in front of any residential unit to the detriment of the street scene.**

**(g) In the Living G (Yaldhurst) (East Belfast) (Awatea) (Wigram) (Prestons) (Halswell West) and (Highsted) Zones, the ability of any outdoor living space or fenced court area to receive a minimum of 2 hrs continuous sunlight, measured on the winter solstice between the hours of 10.00 am and 2.00 pm. When assessing the adequacy of sunlight access, regard should be had to the bulk and height of any building that could be constructed as of right on any adjoining site.**

#### **15.2.25 Residential coherence**

Delete clause (b) and replace with the following:

**(b) In the Living 1, H, RS, RV, 2, 3 and G (Yaldhurst) (Awatea) (Wigram) (Prestons) (Halswell West) and (Highsted) Zones, the extent to which the activity provides a local function by meeting the needs of residents principally within the surrounding residential environment.**

Add the following clauses:

#### **15.2.69 Fencing on sites adjoining the Green and Blue Networks – Living G (Highsted) Zone**

**(a) The extent to which a reduction in visual transparency may be more visually appropriate or suited to the character of the site or area.**

- (b) The extent to which a reduction in visual transparency or use of non-transparent materials may be appropriate to provide levels of privacy or security.**
- (c) The extent to any reduction in public surveillance over open space areas may lead to or promote unsafe or insecure environments.**

#### **15.2.70 Urban Design and amenity – Density A residential areas within the Living G (Highsted) Zone**

##### **General matters**

**General matters set out expectations for design of new residential development.**

**Applicants are also encouraged to provide written and graphic evidence of their design rationale to accompany site specific proposals.**

**Any proposal shall be assessed against the extent to which the development achieves the following principles:**

- (i) Site and context**
  - (a) Developments should consider local environmental conditions including but not limited to the position of the sun and prevailing winds.**
  - (b) Developments are encouraged to support any prominent vistas and view shafts.**
  - (c) Developments are encouraged to provide a high degree of passive surveillance.**
  - (d) The extent to which high density sites are located to provide convenient access to public transport routes, a local commercial centre, Tulett Park and/or neighbourhood parks.**
  - (e) The extent to which the number of households will contribute to an overall minimum yield of 15 households a hectare in the Outline Development Plan area as a whole.**
- (ii) Relationship with street, lanes and public open spaces**
  - (a) The appropriateness of the depth and consistency of setback from the road boundary taking into account the setback from the road boundary of buildings on adjacent sites.**
  - (b) Buildings should be oriented toward the street and positioned close to the road boundary.**
  - (c) Developments should place active areas of buildings, such as habitable rooms and entrances, along the street and public open spaces, particularly at ground level.**



- (d) Buildings should have pedestrian entrances that are identifiable, well-articulated and directly accessible from the street or, in the case of rear units, shared access ways.
  - (e) Facades of buildings facing the street should have a high degree of glazing that is evenly distributed.
  - (f) Fences and landscaping along the road boundary or adjacent to public open spaces should not obstruct ground level views.
  - (g) Fences should complement the building design.
  - (h) Building design and location provide opportunities for passive surveillance particularly in relation to shared access ways and lanes.
  - (f) To minimise the extent of vehicle crossings and/or garages that face the street and to maximise building frontage and on-street parking, regard should be had to:
    - Avoiding extensive use of rear lanes
    - The use of shared access.
- (iii) Corner sites
- (a) Buildings on corner sites should orientate toward all adjacent streets and public open spaces and emphasise these corners.
  - (b) Pedestrian entrances are encouraged to be located along main pedestrian routes.
- (iv) Building form and articulation
- (a) Buildings should be of a domestic scale.
  - (b) Buildings in series should avoid excessive repetition of building forms.
  - (c) Buildings should avoid façades and elevations whose length or bulk is visually excessive or blank.
  - (d) Roofs should be designed to limit continuous ridgelines and minimise the visual bulk of a building.
  - (e) The separation of buildings on sites is encouraged to reduce perceived building bulk.
  - (f) Architectural features and a variety of materials and colours are encouraged to provide human scale and visual interest.
  - (g) The extent to which the orientation, size and shape of buildings establishes connectivity with prominent vistas and view shafts.
  - (h) The extent to which the overall design and orientation encourages the minimisation of energy use.

(v) **Landscaping and site amenity**

- (a) **Car parking, garages, side boundaries, shared access ways, lanes and service areas should be softened by landscaping.**
- (b) **Lighting, planting, fences and other structures on sites and shared accessways, lanes and service areas should be designed to maximise safety of occupants and visitors.**
- (c) **For lanes and shared accessways, fences and gates at the rear of properties should provide varying degrees of solidity and transparency, while maximising occupants' safety and providing opportunities for passive surveillance.**
- (d) **Landscape design is encouraged to use endemic or locally appropriate plants, including those that minimise water and maintenance requirements, promotes biodiversity and healthy ecosystems, such as those plants in the species list at Appendix 2B to Part 14.**
- (e) **The extent to which crime prevention through environmental design (CPTED) principles have been included in the design, orientation, size and shape of buildings, the provision of hard and soft landscaping, and the location of street lighting.**

(vi) **Outdoor Living Spaces**

- (a) **Outdoor living spaces should be located on sites in a way that will optimise useable space and provide a pleasant outlook for unit occupants.**
- (b) **Private outdoor living spaces, including balconies and terraces, should link directly to main living areas in the residential unit.**

(vii) **Service Areas and Utilities**

- (a) **Service areas should be positioned in a development to minimise adverse visual, noise or odour amenity effects and to enable practical use.**
- (b) **Rubbish storage areas, letter boxes, utility boxes and other service facilities required to be accessible from the street should be visually integrated into the development frontage.**
- (c) **Building services such as external access ways and mechanical, electrical and communications equipment should be integrated in the building to minimise their visual impact, particularly from streets or public open spaces.**
- (d) **Storage space should be easily accessible to residents.**

(viii) **Residential Amenity**

- (a) **The location, orientation and internal design of residential units should balance outlook and sunlight with the privacy of internal occupants and**

**neighbouring residential units.**

- (b) **Windows and balconies on upper levels should be orientated and screened to limit direct overlooking of adjacent dwellings, their outdoor living space and the private outdoor living space of other units in the same development.**
- (c) **Developments are encouraged to provide a variety of unit types and sizes to accommodate a range of households.**

**(ix) Treatment of Lanes**

- (a) **Define the identity of, and entrance to, a right of way through both landscaping and built form elements such as location of residential dwellings close to the street or where they are visible from the street.**
- (b) **Provision of shared vehicle and pedestrian access with no defined footpath.**
- (c) **Variation in a right of way clearway through design by tightening, extending and terminating views in a lane.**
- (d) **Provision of permanent passive surveillance for all parts of the lane.**
- (e) **Establish a consistent character for a right of way with complementary architectural features on the right of way and adjacent buildings.**

**Volume 3: Part 2 Living zones**

Add new **Appendix 3Z – Master Plan (Highsted) and Appendix 3ZA – Movement Network (Highsted).** See attached Appendices.

**Volume 3: Part 3, Business Zones**

**Section 3.4 Development standards - Business 1 and 2 Zones:**

Delete clause 3.4.8 and replace with the following:

**3.4.8 Residential activities - Outline Development Plan (Awatea)(Appendix 3T, Part 2) and (Halswell West)(Appendix 3W, Part 2) and Master Plan (Highsted)(Appendix 3Z)**

**In the Business 1 Commercial Area, as identified on Outline Development Plan (Awatea)(Appendix 3S, Part 2) and (Halswell West)(Appendix 3W, Part 2) and Master Plan (Highsted)(Appendix 3Z, Part 2) residential activity shall not be located on the ground floor of any building.**

Delete the title of clause 3.4.9 and replace with the following:

**3.4.9 Urban design and amenity for development in the Business 1 Commercial Area - Outline Development Plan (Awatea)(Appendix 3T Part 2), (Halswell West)(Appendix 3W, Part 2) and Master Plan (Highsted)(Appendix 3Z, Part 2)**

**Section 3.5 Community standards – Business 1 and 2, and 2P (Fendalton Mall) Zones**

Add new rule as follows:

**3.5.8 Special provision – Highsted Business 1 Commercial Area**

**Land and/or buildings shall be made available to accommodate a Business 1 Commercial Area of 2,000m<sup>2</sup> gross floor area in the location shown on the Highsted Master Plan and Outline Development Plan – Upper Styx (R8).**

**Part 3 Business Zones: Section 6 Assessment matters for resource consents**

Delete the title of 6.3.16 and replace with the following:

**6.3.16 Design and amenity for development in the Business 1 and Community Footprint Area - Outline Development Plan (Halswell West)(Appendix 3W and 3W(a), Part 2) and in the Business 1 Commercial Area on Master Plan (Highsted)(Appendix 3Z)**

Delete the title of 6.3.17 and replace with the following:

**6.3.17 Residential Activities - Outline Development Plan (Halswell West) and Master Plan (Highsted)**

**Volume 3: Part 13, Transport**

**2.2.1 Parking Space Numbers**

**Table 1b. Minimum parking required in all other zones**

Add new entry for Living G (Highsted ) zone below "**Residential activities** generally": -

<b><u>Living G</u></b> <b><u>(Highsted)</u></b> <b><u>Zone:</u></b> <b><u>residential</u></b> <b><u>area only.</u></b>	<b><u>For a site of</u></b> <b><u>400m<sup>2</sup> or less:</u></b> <b><u>1 car-parking</u></b> <b><u>space ,</u></b> <b><u>otherwise 2</u></b> <b><u>spaces/unit</u></b>	<b><u>N/A</u></b>	<b><u>Nil</u></b>	
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## **Volume 3: Part 14, Subdivision**

Add the following new clauses

### **Rule 5.2.4 New Roads in 5.2 Development standards – Property Access**

#### **5.2.4 New roads**

**(b) where the road is within the area of land to which the Master Plan (Highsted) in Appendix 3Z, Part 2, Volume 3 applies, then the cross-sections detailed in Appendix 3ZA shall apply in place of the requirements of Appendix 5 should there be any conflict.**

### **32.0 Subdivision in the [Outline Development Plan – Upper Styx (R8) area and] Living G (Highsted) zone**

#### **32.2 Development Standards – Subdivision in the Living G (Highsted) zone**

##### **32.2.1 Density A Residential Area. Comprehensive subdivision and land use development**

**Any subdivision shall be accompanied by comprehensive building and allotment design detailing the nature, character, scale and form of development associated with proposed allotments and shall be a restricted discretionary activity with the Councils' discretion limited to the design and layout of the subdivision and associated land-use development.**

**For the purpose of this rule, associated land use development means:**

- **Buildings, including accessory buildings;**
- **Driveways and entrances;**
- **Fences and gates;**
- **Landscaping;**
- **Pedestrian paths and entrances;**
- **Shared access ways and lanes;**
- **Outdoor living areas;**
- **On-site car parking;**
- **Lighting**
- **Service areas;**
- **Utilities.**

**32.2.2 The Business 1 zone subdivision rules shall apply to subdivision in the area shown as Business 1 Commercial Area on the Master Plan (Highsted) (Appendix 3Z, Part 2).**

**32.4 Critical Standards – Living G (Highsted) zone**

**32.4.1 Special Provision - Allotment with existing building**

**The net area provisions in 14.5.1 shall not apply, where a larger allotment is to be created that includes the existing building on Pt Lot RS1600 (266 Highsted Road) and a subdivision concept plan is able to demonstrate that a feasible future subdivision (inclusive of all legal access) that gives effect to 14.5.1, can take place on that larger allotment.**

**32.4.2 Allotment sizes - Residential activity**

**No allotment, vacant at the time of subdivision, shall be created such that it is unable to accommodate a rectangle of the dimensions specified below:**

**Density C     16m x 16m**

**Density B     10m x 10m**

**Density A     6m x 8m**

**Notwithstanding the above, minimum allotment dimensions shall not apply to allotments created for access, utilities, roads, open space and/or reserves.**

**32.4.3 Stormwater**

**Any subdivision shall control stormwater in accordance with the Styx River/Purakaunui Area Stormwater Management Plan.**

**32.4.4 Site Contamination**

**Any subdivision of land for residential purposes shall include an assessment which identifies and confirms the nature of any contamination of the land as well as specifying what remediation actions are required to ensure that the land is suitable for residential occupation.**

#### **32.4.5 Geotechnical Assessment and Remediation**

**Any subdivision of land for residential purposes shall include site specific deep investigations of ground conditions to at least the densities set out in Ministry of Business Innovation and Employment guidelines on the investigation and assessment of subdivisions (September 2012).**

#### **32.5 Information to be supplied with subdivision consent**

- (a) **Information that illustrates how the proposed subdivision accords with the Highsted Master Plan (Appendix 3Z, Part 2);**
- (b) **Information to show the staging of development including the location of structural elements identified in the Master Plan (Highsted) and takes account of the need to prevent clogging of soakage basins with sediments associated with construction.**
- (c) **Location of high, medium and low density sites.**
- (d) **Measures to be taken to reduce dust emissions (if any).**
- (e) **Appropriate remedial and/or site management measures to make land suitable for residential purposes in the event soil contamination is identified.**
- (f) **Appropriate geotechnical assessment to meet MBIE guidelines, and a description of methods which will be adopted to address the risk of liquefaction.**
- (g) **For comprehensive subdivision and associated land use development in Density A residential areas, building design information detailing how buildings are to satisfy the assessment matters detailed in Level 2 of 32.6 Design and Layout.**

#### **32.6 Assessment matters for subdivision - Living G (Highsted) Zone**

##### **General matters**

**General matters set out expectations for design of new residential development.**

**Applicants are also encouraged to provide written and graphic evidence of their design rationale to accompany site specific proposals.**

**Any proposal shall be assessed against the extent to which the development achieves the following principles:**

##### **Level 1 – All Density areas**

##### **Design and Layout**



- (a) The extent to which subdivision can accommodate the land use and built form and layout anticipated for the land in the Highsted Master Plan and the Living G (Highsted) policies at Sections 10 and 11, Volume 2 of the City Plan.
- (b) The extent to which the orientation, size and shape of allotments enables appropriate housing forms to establish that are reflective of the density area in which they are located.
- (c) The extent to which the orientation, size and shape of allotments establishes connectivity with prominent vistas and view shafts.
- (d) The extent to which crime prevention through environmental design (CPTED) principles have been included in the design, orientation, size and shape of allotments, the provision of hard and soft landscaping, and the location of street lighting.
- (e) The extent to which the overall design, orientation, size and shape of allotments encourages the minimisation of energy use.
- (f) The ability to minimise the impact of access and garages on the streetscape through subdivision design. In higher density areas, to minimise the extent of vehicle crossings and/or garages that face the street and to maximise building frontage and on-street parking, regard should be had to
- (g) Avoiding extensive use of rear lots
- (h) The use of rear lanes (via access lots, rights of way or legal road) particularly for narrow terrace housing lots.
- (i) The use of shared access (via access lots or rights of way).
- (j) The application of appropriate lot dimensions and sizes to prevent the creation of long monotonous facades.
- (k) Avoiding the widespread use of cul-de-sacs. Any cul-de-sacs should be short and relatively straight.
- (l) Street blocks should maximize the permeability of the movement network within the Master Plan (Highsted), particularly in relation to pedestrian movements.
- (m) The ability of subdivision to add to the diversity of housing types as appropriate for the Density Area.
- (n) The extent to which the average allotment size in the subdivision application will contribute to an overall minimum yield of 15 households a hectare in the Outline Development Plan area as a whole.

### Connectivity

- (a) The extent to which the proposal will be in general accordance with the movement network within the Master Plan (Highsted)(Appendix 3ZA, Part

2).

- (b) The extent to which both the green network and pedestrian/cycle connections within the Master Plan share space and inter-relate.

#### **Roading**

- (a) The extent to which the roading layout supports a functional hierarchy of streets.
- (b) The extent to which the roading layout achieves a well-connected and highly permeable movement network.
- (c) The extent to which the roading layout integrates in a practical and functional manner with the adjoining existing road network and the road network on proposed or concurrent subdivision consent applications.
- (d) The extent to which the proposed cross sections contribute toward achieving an environment that is compatible with each street's function.
- (e) The extent to which space is provided for cyclists and cycling is encouraged by the cross section design.
- (f) The account taken of pedestrian movement and continuity of walking facilities within the development.
- (g) The extent to which the roading layout supports walking to bus stops and/or key community facilities.
- (h) The opportunities for tree and amenity planting provided and the extent to which amenity features have been incorporated into each street.
- (i) The appropriateness of the legal road width to accommodate the proposed cross-section design.
- (j) The ability of the legal road width to accommodate a future change in the function of the street (where applicable).
- (k) The ability of the street to accommodate public transport services and the provision of bus stops, where appropriate.
- (l) The extent to which new roads make adequate provision for vehicle movements, car parking and property access.
- (m) The extent to which stormwater management features are incorporated into the road stormwater treatment design, where appropriate.

#### **Public open space**

- (a) The extent to which the proposal will be in general accordance with the green network within the Master Plan (Highsted)(Appendix 3Z, Part 2).
- (b) The extent to which the proposal supports the Open Space Strategy (2010)

in particular through the provision of integrated, well distributed and sized neighbourhood park(s), recreational park(s), amenity strips and landscaping areas.

- (c) Landscape design is encouraged to use endemic or locally appropriate plants, including those that minimise water and maintenance requirements, promotes biodiversity and healthy ecosystems, such as those plants in the species list at Appendix 2B to Part 14.
- (d) In the riparian parks landscaping areas shown on the green network within the Master Plan all planting should be native species selected from the species list included in Appendix 2B, Part 14 - Plant Species for Living G (Highsted) Zone.
- (e) In the recreation reserve landscaping area shown on the green network within the Master Plan all planting should be from the species list included in Appendix 2B, Part 14 - Plant Species for Living G (Highsted) Zone and comprise at least 60% native species.
- (f) Landscaping along the road boundary or adjacent to public open spaces should not obstruct ground level views and should not contain species that are able to reach a size at maturity, that would require the plant's removal to maintain amenity on the site.
- (g) Landscaping along the road boundary or adjacent to public open spaces should not be planted where it will interfere with the operation, repair or maintenance of underground infrastructure.

#### Street Trees

- (a) The extent to which trees are proposed to be accommodated within the legal road reserve.
- (b) The provision of trees intended to provide a high level of visual amenity.
- (c) The provision of trees which recognise the context and scale of the area in which they are located and the significance of the road in the roading hierarchy.
- (d) The provision of appropriate indigenous trees from Part 14, Appendix 2B - Plan species for Living G (Highsted) Zone to achieve at least 50% indigenous street trees within the zone.

#### Stormwater

- (a) Measures adopted so as to ensure the protection of groundwater quality including treatment of discharges from roads and sealed car parking areas.
- (b) The extent to which alternative treatments may be available.

- (c) The contribution made by the stormwater facilities layout and design to the visual amenity of the immediate area.
- (d) The ability to capture and treat stormwater on site.
- (e) The extent to which the proposal will be in general accordance with the blue network within the Master Plan.
- (f) The extent to which the proposal will be in general accordance with the Styx River/Puruakanui Area Stormwater Management Plan.
- (g) The extent to which the proposal incorporates stormwater management infrastructure to provide appropriately for the control of the stormwater during the period of construction.

#### Street Scene

- (a) The extent to which lot design and orientation will allow buildings to address the street.
- (b) The extent to which consideration has been given to the potential use of back lanes for vehicle access in subdivision layout.

#### Mix of Densities

- (a) The extent to which the application clearly identifies and meets the density ranges specified in the Outline Development Plan – Upper Styx and the Master Plan.

#### Sanitary Sewer

- (a) The extent to which the subdivision will necessitate the construction of more than one wastewater pumping station.

#### Unanticipated Discovery of Archaeological Sites

- (a) The extent to which the requirement for an unanticipated discovery of archaeological sites protocol should be included in any subdivision consent for site works associated with the consent.
- (b) The need to notify developers of their obligations under the Historic Places Act 1993.

#### Tangata Whenua cultural values

- (a) The extent to which Tangata Whenua values have been taken into account in the design and construction methodology for any proposed subdivision.
- (b) The extent to which the application provides for other Tangata Whenua

related objectives, policies, rules and methods within the City Plan and other relevant iwi management plans.

#### Level 2 - Density A areas

For comprehensive subdivision and associated land use development in Density A residential areas, the extent to which building and site development will achieve the following:

- (i) Site and context
  - (a) Developments should consider local environmental conditions including but not limited to the position of the sun and prevailing winds.
  - (b) Developments are encouraged to support any prominent vistas and view shafts.
  - (c) Developments are encouraged to provide a high degree of passive surveillance.
  - (d) The extent to which high density sites are located to provide convenient access to public transport routes, a local commercial centre, Tulett Park and/or neighbourhood parks.
  
- (ii) Relationship with street, lanes and public open spaces
  - (a) The appropriateness of the depth and consistency of setback from the road boundary taking into account the set back from the road boundary of buildings on adjacent sites.
  - (b) Buildings should be oriented toward the street and positioned close to the road boundary.
  - (c) Developments should place active areas of buildings, such as habitable rooms and entrances, along the street and public open spaces, particularly at ground level.
  - (d) Buildings should have pedestrian entrances that are identifiable, well-articulated and directly accessible from the street or, in the case of rear units, shared access ways.
  - (e) Facades of buildings facing the street should have a high degree of glazing that is evenly distributed.
  - (f) Fences and landscaping along the road boundary or adjacent to public open spaces should not obstruct ground level views.
  - (g) Fences should complement the building design.
  - (h) Building design and location provide opportunities for passive surveillance particularly in relation to shared access ways and lanes.

(iii) **Corner sites**

- (a) **Buildings on corner sites should orientate toward all adjacent streets and public open spaces and emphasise these corners.**
- (b) **Pedestrian entrances are encouraged to be located along main pedestrian routes.**

(iv) **Building form and articulation**

- (a) **Buildings should be of a domestic scale.**
- (b) **Buildings in series should avoid excessive repetition of building forms.**
- (c) **Buildings should avoid façades and elevations whose length or bulk is visually excessive or blank.**
- (d) **Roofs should be designed to limit continuous ridgelines and minimise the visual bulk of a building.**
- (e) **The separation of buildings on sites is encouraged to reduce perceived building bulk.**
- (f) **Architectural features and a variety of materials and colours are encouraged to provide human scale and visual interest.**

(v) **Landscaping and site amenity**

- (a) **Car parking, garages, side boundaries, shared access ways, lanes and service areas should be softened by landscaping.**
- (b) **Lighting, planting, fences and other structures on sites and shared accessways, lanes and service areas should be designed to maximise safety of occupants and visitors.**
- (c) **For lanes and shared accessways, fences and gates at the rear of properties should provide varying degrees of solidity and transparency, while maximising occupants' safety and providing opportunities for passive surveillance.**

(vi) **Outdoor Living Spaces**

- (c) **Outdoor living spaces should be located on sites in a way that will optimise useable space and provide a pleasant outlook for unit occupants.**
- (d) **Private outdoor living spaces, including balconies and terraces, should link directly to main living areas in the residential unit.**

(vii) **Service Areas and Utilities**

- (a) **Service areas should be positioned in a development to minimise adverse visual, noise or odour amenity effects and to enable practical use.**

- (b) Rubbish storage areas, letter boxes, utility boxes and other service facilities required to be accessible from the street should be visually integrated into the development frontage.
- (c) Building services such as external access ways and mechanical, electrical and communications equipment should be integrated in the building to minimise their visual impact, particularly from streets or public open spaces.
- (d) Storage space should be easily accessible to residents.

(viii) **Residential Amenity**

- (a) The location, orientation and internal design of residential units should balance outlook and sunlight with the privacy of internal occupants and neighbouring residential units.
- (b) Windows and balconies on upper levels should be orientated and screened to limit direct overlooking of adjacent dwellings, their outdoor living space and the private outdoor living space of other units in the same development.
- (c) Developments are encouraged to provide a variety of unit types and sizes to accommodate a range of households.

(ix) **Treatment of Right of Ways**

- (a) Define the identity of, and entrance to, a right of way through both landscaping and built form elements such as location of residential dwellings close to the street or where they are visible from the street.
- (b) Provision of shared vehicle and pedestrian access with no defined footpath.
- (c) Variation in a right of way clearway through design by tightening, extending and terminating views in a lane.
- (d) Provision of permanent passive surveillance for all parts of the right of way.
- (e) Establish a consistent character for a right of way with complementary architectural features on the right of way and adjacent buildings.

Add a new Appendix as follows:

Volume 3 Section 14

**Appendix 2B - Plant species for the Living G (Highsted) Zone**

**LARGE NATIVE TREES**

<u><i>Elaeocarpus dentatus</i></u>	<u>hinau</u>
<u><i>Podocarpus totara</i></u>	<u>totara</u>

Prumnopitys taxifolia

matai, black pine

### **LARGE EXOTIC TREES (Streets and Parks)**

Acer campestre

field maple

Alnus cordata

Italian alder

Fagus sylvatica

'Purpurea' copper beech

Liriodendron tulipifera

tulip tree

Platanus orientalis

"Autumn Glory" plane tree

Quercus coccinea

scarlet oak

Quercus rubra

red oak

Tilia cordata

small-leaved lime

Ulmus carpinifolia

smooth-leaved elm

Ulmus glabra

wych elm

### **TREES & TALL SHRUBS**

Coprosma lucida

shining karamu

Coprosma robusta

Karamu

Cordyline australis

ti kouka, cabbage tree

Griselinia littoralis

broadleaf, kapuka

Hoheria angustifolia

narrow-leaved lacebark

Kunzea ericoides

kanuka

Leptospermum scoparium

manuka

Olearia paniculata

akiraho, golden akeake

Pittosporum eugenioides

tarata, lemonwood

Pittosporum tenuifolium

Kohuhu, black matipo

Plagianthus regius

manatu, lowland ribbonwood

Pseudopanax crassifolius

lancewood, horoeka

Sophora microphylla

South Island kowhai



## **SHRUBS**

<u><i>Coprosma propinqua</i></u>	<u>mikimiki (mingimingi)</u>
<u><i>Coprosma virescens</i></u>	<u>pale green coprosma</u>
<u><i>Coprosma crassifolia</i></u>	<u>thick-leaved mikimiki</u>
<u><i>Coprosma rubra</i></u>	<u>red-stemmed coprosma</u>
<u><i>Coprosma virescens</i></u>	<u>pale green coprosma</u>
<u><i>Cyathodes juniperina</i></u>	<u>prickly mingimingi</u>
<u><i>Helichrysum lanceolatum</i></u>	<u>ninia</u>
<u><i>Leucopogon fasciculatus</i></u>	<u>mingimingi</u>
<u><i>Muehlenbeckia astonii</i></u>	<u>wiggywig, mingimingi</u>
<u><i>Muehlenbeckia complexa</i></u>	<u>pohuehue</u>
<u><i>Teucrium parvifolium</i></u>	<u>NZ shrub verbena</u>

## **GROUNDCOVERS etc.**

<u><i>Acaena novae-zelandiae</i></u>	<u>bidibidi, piripiri</u>
<u><i>Anemanthele lessoniana</i></u>	<u>bamboo grass, wind grass</u>
<u><i>Blechnum minus</i></u>	<u>Swamp kiokio</u>
<u><i>Carex lambertiana</i></u>	<u>sedge</u>
<u><i>Carex solandri</i></u>	<u>sedge</u>
<u><i>Cortaderia richardii</i></u>	<u>toetoe grass</u>
<u><i>Cyperus ustulatus</i></u>	<u>umbrella sedge, upoko-tangata</u>
<u><i>Deschampsia caespitosa</i></u>	<u>tufted hair grass</u>
<u><i>Dichondra repens</i></u>	<u>dichondra</u>
<u><i>Juncus gregiflorus</i></u>	<u>wiwi</u>
<u><i>Juncus pallidus</i></u>	<u>giant rush</u>
<u><i>Leucopogon fraseri</i></u>	<u>Patototara, a dwarf heath</u>
<u><i>Microlaena polynoda</i></u>	<u>Rice grass, native bamboo</u>
<u><i>Microlaena stipoides</i></u>	<u>meadow rice grass</u>
<u><i>Phormium tenax</i></u>	<u>harakeke, NZ flax</u>

Phymatosorus pustulatus

hounds tongue fern, maratata

Pratia angulate

Panakeneke, creeping pratia

Pteridium esculentum

bracken fern, rahurahu

#### **ADDITIONAL PLANTS FOR SHELTERED SITES: TREES & SHRUBS**

Alectryon excelsus

titoki

Aristolelia serrata

makomako, wineberry

Coprosma areolata

thin-leaved coprosma

Coprosma linariifolia

mikimiki, yellow-wood

Coprosma rhamnoides

red-fruited karamu

Coprosma rubra

red-stemmed coprosma

Cyathodes juniperina

prickly mingimingi

Fuchsia excorticata

Kotukutuku, tree fuchsia

Melicope simplex

poataniwha

Melicytus ramiflorus

mahoe, whiteywood

Myoporum laetum

tahuo

Myrsine australis

mapou, red matipo

Pennantia corymbosa

kaikomako

Pittosporum eugenioides

tarata, lemonwood

Pseudopanax anomalus

shrub pseudopanax

Pseudopanax arboreus

five finger, whauwhaupaku

Streblus heterophyllus

turepo, small-leaved milk tree

#### **GROUNDCOVERS**

Asplenium flabellifolium

necklace fern

Astelia fragrans

bush flax, kakaha

Blechnum penna-marina

kiokio, small hardfern

Daniella nigra

turutu, blue berry

Hypolepsis ambigua

Libertia ixioides

Microlaena avenacea

Microlaena polynoda

Microlanea stipoides

Pellaea rotundifolia

Uncinia uncinata

Polystichum richardii

Polystichum vestitum

Uncinia uncinata

NZ iris, mikoikoi

bush rice grass

bamboo grass

a rice grass

button fern

watau, dense forest sedge

shield fern, pikopiko

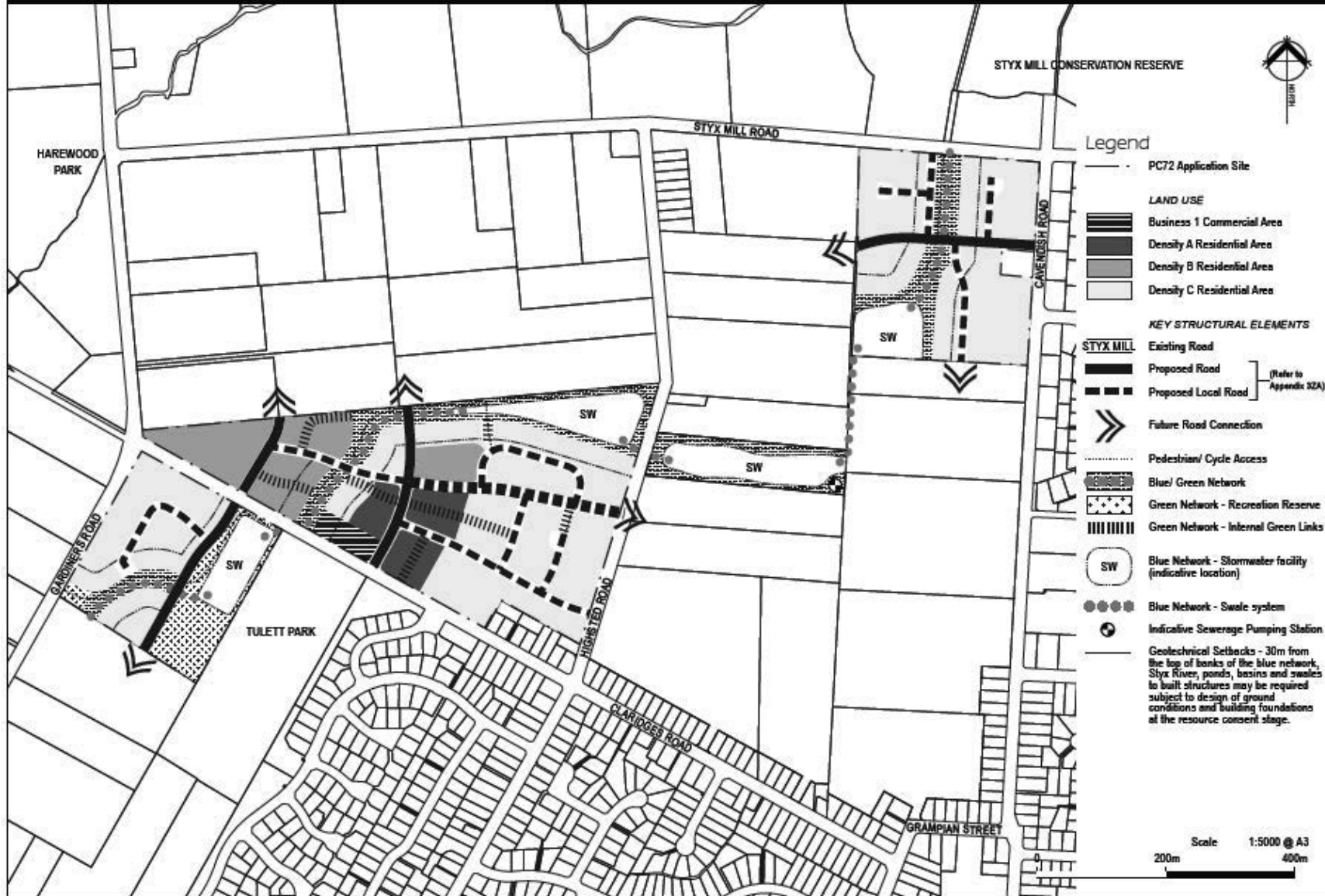
Shield fern, punui, prickly shield fern

watau

### **Volume 3: Planning Maps**

Delete existing **Planning Maps 17A and 24A** and replace with the attached.

# Appendix 3Z - Living G (Highsted) Zone - Master Plan



## Legend

- PC72 Application Site
- LAND USE**
  - Business 1 Commercial Area
  - Density A Residential Area
  - Density B Residential Area
  - Density C Residential Area
- KEY STRUCTURAL ELEMENTS**
- STYX MILL**
  - Existing Road
  - Proposed Road (Refer to Appendix 3ZA)
  - Proposed Local Road
  - Future Road Connection
  - Pedestrian/ Cycle Access
  - Blue/ Green Network
  - Green Network - Recreation Reserve
  - Green Network - Internal Green Links
- Blue Network - Stormwater facility (indicative location)
- Blue Network - Swale system
- Indicative Sewerage Pumping Station
- Geotechnical Setbacks - 30m from the top of banks of the blue network, Styx River, ponds, basins and swales to built structures may be required subject to design of ground conditions and building foundations at the resource consent stage.

Scale 1:5000 @ A3  
200m 400m

# Appendix 3ZA - Living G (Highsted) Zone - Movement Network



Figure 1: Proposed Local Street >250 vpd

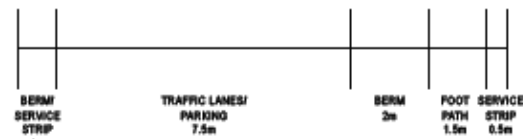
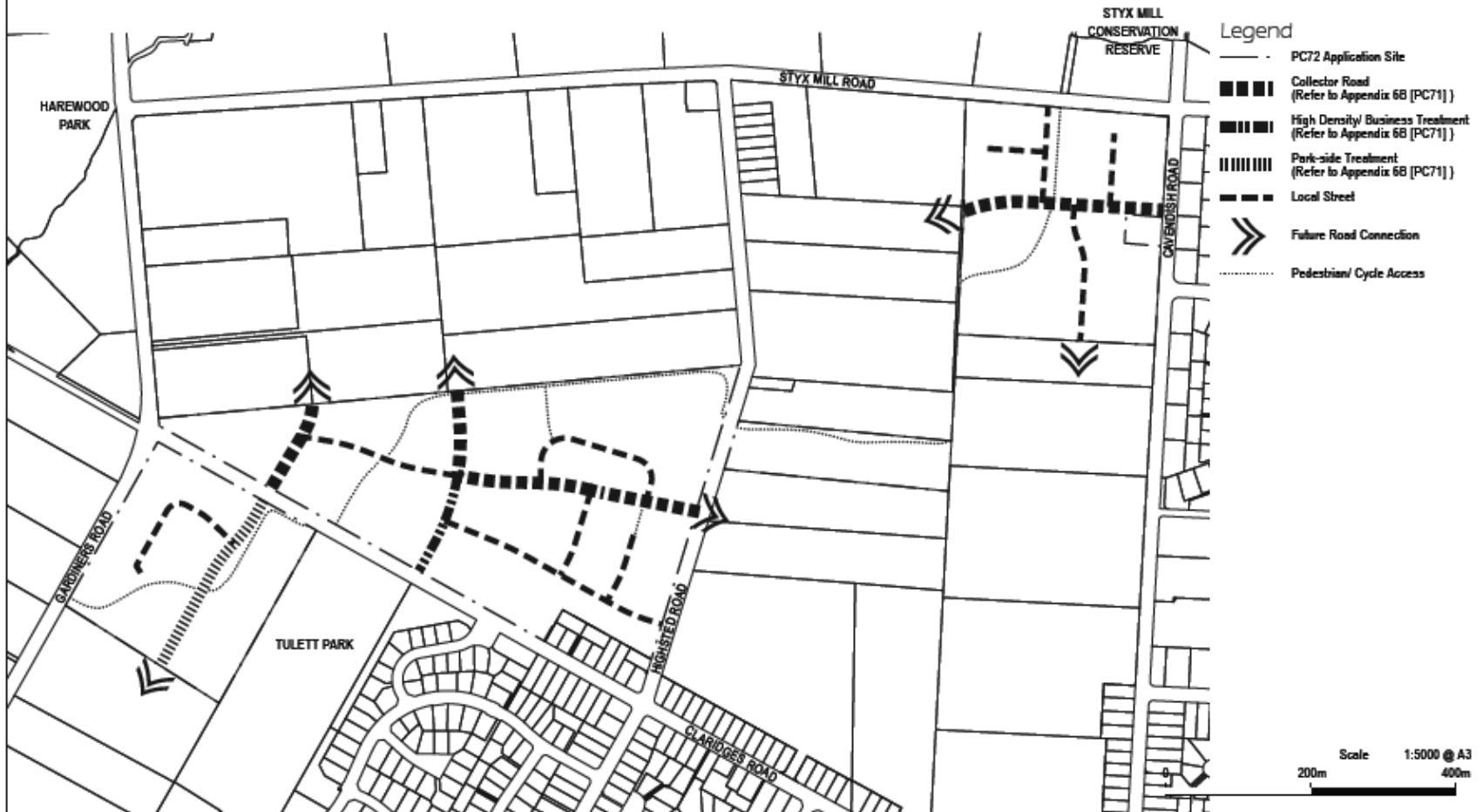
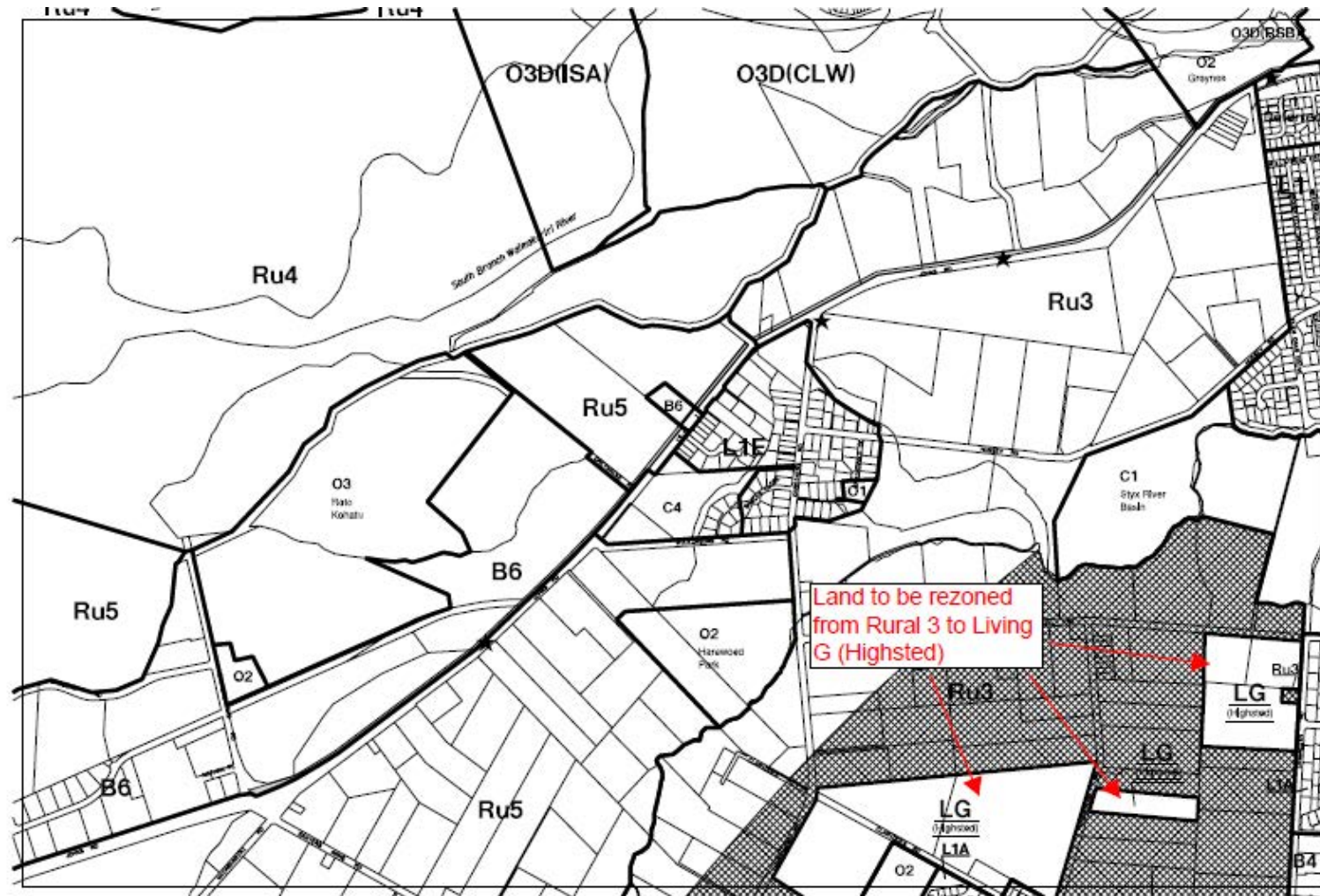


Figure 2: Proposed Local Street <250 vpd (Cul-de-sac)





Land to be rezoned  
from Rural 3 to Living  
G (Highsted)

<table border="1"> <tr><td>9</td></tr> <tr><td>16 17A 18</td></tr> <tr><td>24</td></tr> </table>	9	16 17A 18	24	<p><b>City of Christchurch</b> City Planning Maps</p>	<p><b>Key</b></p> <ul style="list-style-type: none"> <li> City Boundary</li> <li> Zone Boundary</li> <li> 100m Contour</li> <li> Designated Land</li> <li> Non-Operative Area</li> </ul>	<ul style="list-style-type: none"> <li> Coastal Marine Area</li> <li> Mean High Water Springs</li> <li> Major Road Works</li> <li> Local Road to be Widened</li> </ul>	<ul style="list-style-type: none"> <li> Future Urban Development Area (Residential)</li> </ul>
9							
16 17A 18							
24							

Scale 1:15,000 Date 04/08/2012

All spiritual facilities (see Part 1) existing as at 24/6/95 and located in living or rural zones are deemed to be Class 2 scheduled activities (see Part 9).

## Airport Noise Contours

Amend the City Plan as follows:

### 1. Volume 3, Planning Maps

- (a) Delete Planning Maps 3B, 6B-10B, 14B -18B, 22B-24B, 29B -31B, 35B-38B, 42B- 43B, and 50B; and
- (b) Add new Planning Maps 3B, 6B-10B, 14B -18B, 22B-24B, 29B -31B, 35B-38B, 42B- 43B, and 50B.

### 2. Volume 2, Policy 6.3A.7 Airport operations

(a) Delete Policy 6.3A.7 Airport operations and add new Policy 6.3A.7 Airport operations to read as follows:

**To avoid noise-sensitive activities within the 50 dBA Ldn noise contour around Christchurch International Airport except:**

- **those permitted in conjunction with rural activities in the rural zones, and**
- **activities within the existing Living zones as defined in the city plan; and**
- **activities in the Open Space 3D (Clearwater) zone.**

### 3. Volume 3, Part 1 Definitions

(b) Add new definition to read as follows:

**"Noise-sensitive activities" mean:**

- **Residential activities;**
- **Education activities including pre-school places or premises, but not including flight training, trade training or other industry related training facilities within the Special Purpose (Airport) Zone or on other land zoned or used for business activities;**
- **Travellers accommodation except that which is designed, constructed and operated to a standard to mitigate the effects of aircraft noise on occupants;**
- **Hospitals, healthcare facilities and any elderly person housing or complex.**

**This definition only applies to airport noise contours.**

### 4. Volume 3, Part 4 Rural Zones, 3.4 Critical Standards: 3.4.2 Aircraft noise exposure

(c) Delete Clause 3.4.2 (c) – this rule no longer applies because the entire RuQ zone referred to in the clause below is outside the amended 65 dBA Ldn/95 SEL composite line, as follows:

### 3.4.2 Aircraft noise exposure

(a) ...

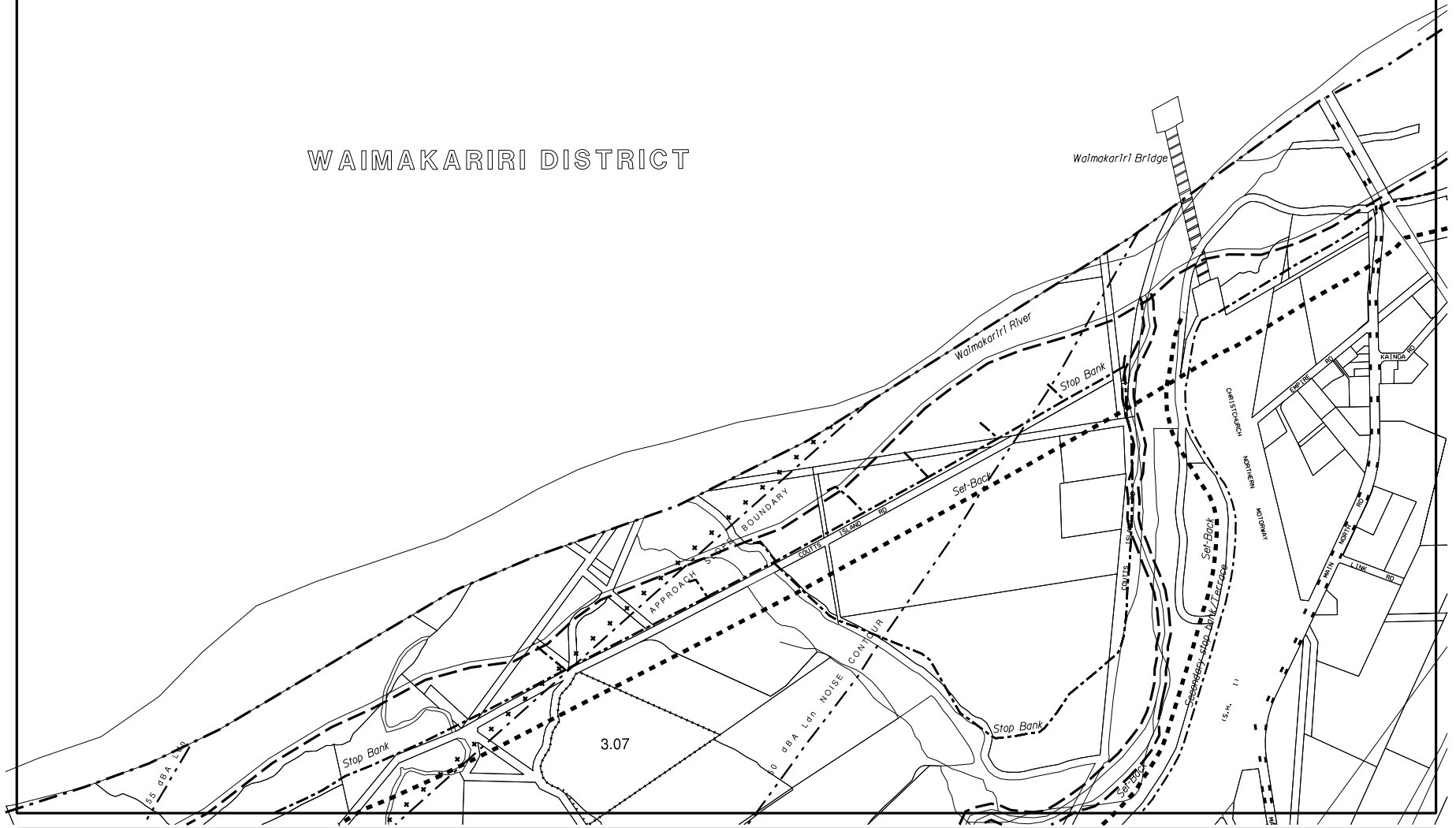
(b) ...

~~(c) Within the Pound Road/Hasketts Road area of the Quarry zone, construction of residential units, education facilities including pre-school places or premises, travellers accommodation, hospitals, healthcare facilities, elderly persons housing or complexes (excluding in all cases accessory buildings, outdoor storage or car parking) within the 65 dBA Ldn/95 SEL air noise boundary as shown on the Planning Maps, shall be a prohibited activity, and no resource consent shall be granted.~~



## **Airport Noise Contour Planning Maps (Volume 3)**

# WAIMAKARIRI DISTRICT



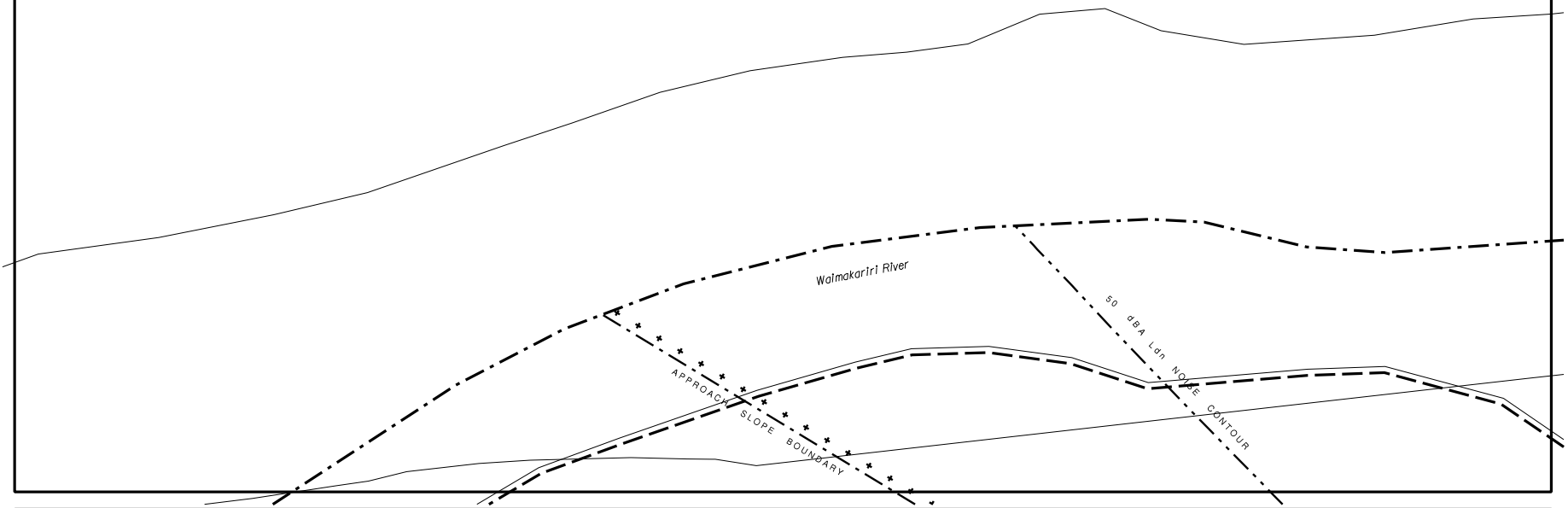
## Key .... cont'd

- |  |   |   |   |
|--|---|---|---|
| <ul style="list-style-type: none"> <li>--- City Boundary</li> <li>--- Summit Road Protection Act</li> <li>--- Airport/Airfield Approach Slope Boundaries</li> <li>--- Airport/Airfield Noise Control Boundaries</li> <li>--- Pylons</li> <li>..... Hazard line 1</li> <li>--- Esplanade Reserve or strip required</li> <li>● Scheduled Activity</li> <li>M.H.W.S. Mean High Water Springs</li> </ul> | <ul style="list-style-type: none"> <li>xxx Road to be stopped</li> <li>== Limited Access Roads</li> <li>- - - Indicative Roads</li> <li>■ Protected Object, Place or Building</li> <li>▲ Protected Tree</li> <li>▲▲ Group of Protected Trees</li> <li>▨ Non-Operative Area</li> </ul> | <ul style="list-style-type: none"> <li>▨ Waimakariri River stopbank floodplain (pc32 non-operative)</li> <li>▨ High hazard area (pc32 non-operative)</li> <li>▨ Area exempted from Waimakariri River Stopbank Floodplain flood protection provisions (pc32 non-operative)</li> <li>▨ Flood Ponding Area</li> <li>▨ Flood Management Area</li> </ul> | <ul style="list-style-type: none"> <li>SAm# Special Amenity Area</li> <li>Community Footprint</li> <li># Ecological Heritage "A" Sites</li> <li>Comprehensive Housing Improvement Area</li> </ul> |
|--|---|---|---|

**City of Christchurch**  
**City Planning Maps**  
 Scale 1:15,000

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-	<b>3B</b>	4
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# WAIMAKARIRI DISTRICT



## Key .... cont'd

- |  |   |   |   |
|--|---|---|---|
| <ul style="list-style-type: none"> <li>--- City Boundary</li> <li>--- Summit Road Protection Act</li> <li>-+ Airport/Airfield Approach Slope Boundaries</li> <li>-+ Airport/Airfield Noise Control Boundaries</li> <li>--- Pylons</li> <li>..... Hazard line 1</li> <li>--- Esplanade Reserve or strip required</li> <li>--- Scheduled Activity</li> <li>M.H.W.S. Mean High Water Springs</li> </ul> | <ul style="list-style-type: none"> <li>x x x Road to be stopped</li> <li>= = = Limited Access Roads</li> <li>- - - Indicative Roads</li> <li>■ Protected Object, Place or Building</li> <li>▲ Protected Tree</li> <li>▲ Group of Protected Trees</li> <li>▨ Non-Operative Area</li> </ul> | <ul style="list-style-type: none"> <li>▨ Waimakariri River stopbank floodplain (pc32 non-operative)</li> <li>▨ High hazard area (pc32 non-operative)</li> <li>▨ Area exempted from Waimakariri River Stopbank Floodplain flood protection provisions (pc32 non-operative)</li> <li>▨ Flood Ponding Area</li> <li>▨ Flood Management Area</li> </ul> | <ul style="list-style-type: none"> <li>SAM# Special Amenity Area</li> <li>Community Footprint</li> <li># Ecological Heritage "A" Sites</li> <li>Comprehensive Housing Improvement Area</li> </ul> |
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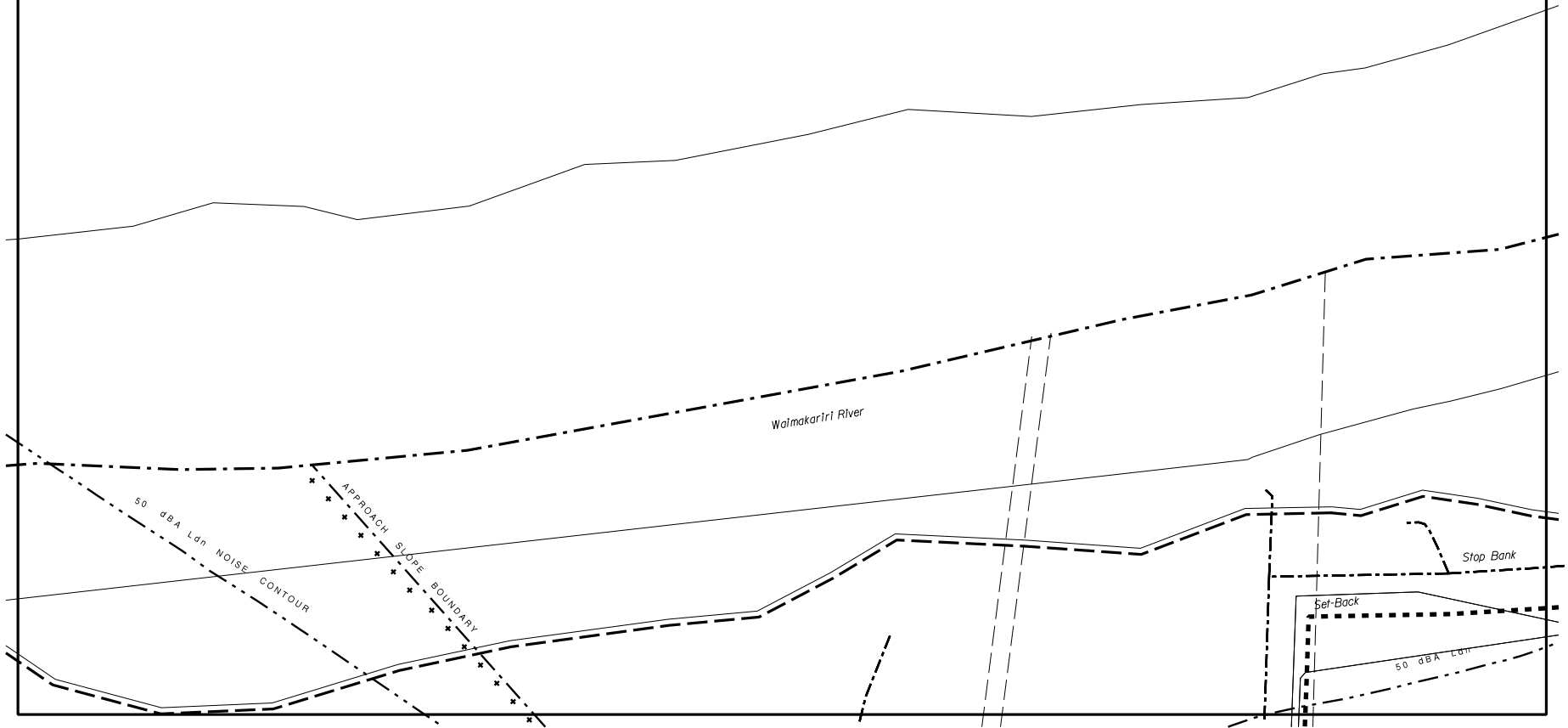
**City of Christchurch**  
City Planning Maps

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14	

# WAIMAKARIRI DISTRICT



## Key .... cont'd

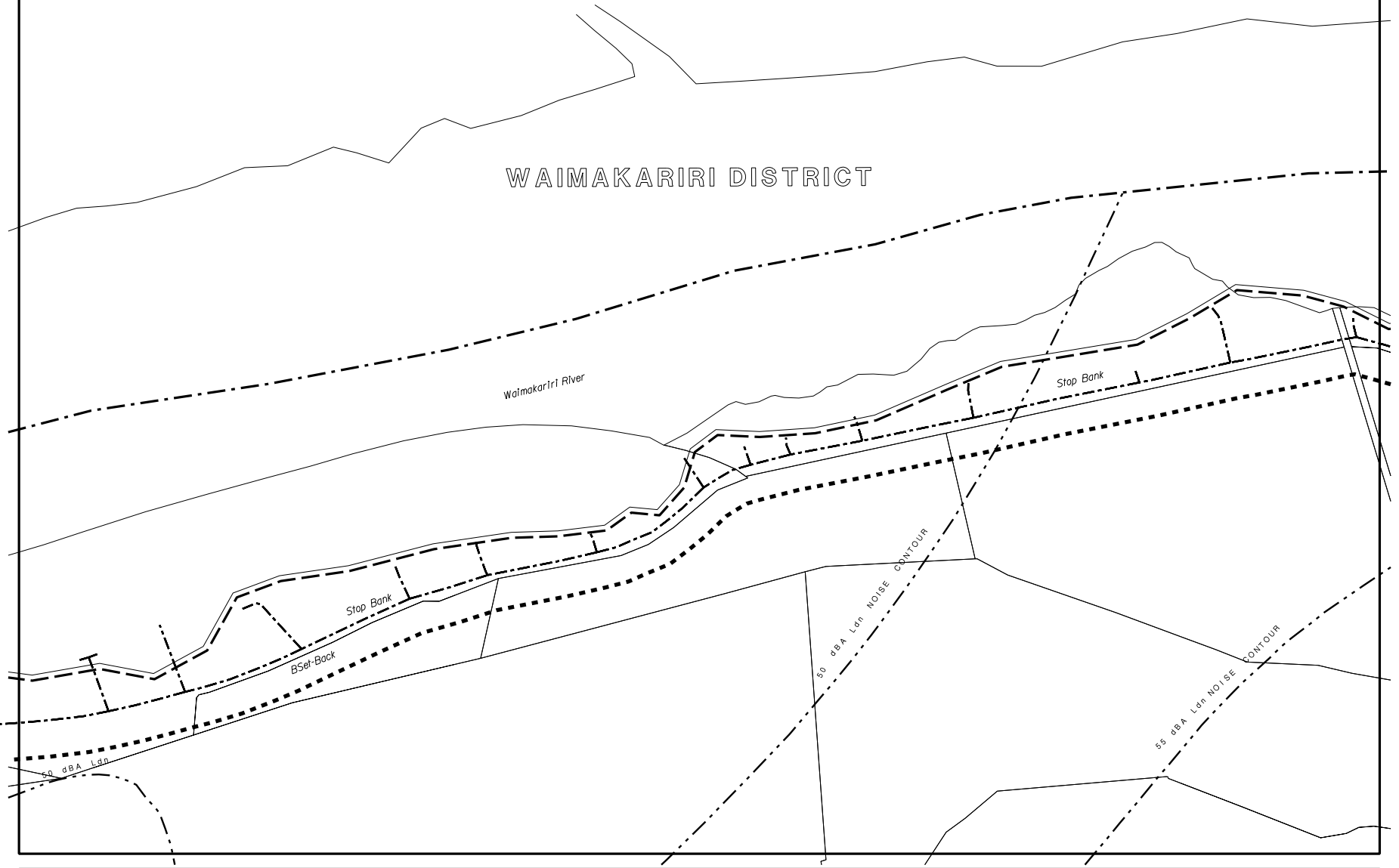
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**City of Christchurch**  
**City Planning Maps**

Scale 1:15,000

6	7B	8
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# WAIMAKARIRI DISTRICT



## Key .... cont'd

- |  |                                     |   |  |
|--|-------------------------------------|---|--|
| City Boundary                              | Road to be stopped                  | Waimakariri River stopbank floodplain (pc32 non-operative)  | Special Amenity Area                   |
| Summit Road Protection Act                 | Limited Access Roads                | High hazard area (pc32 non-operative)   | Community Footprint                    |
| Airport/Airfield Approach Slope Boundaries | Indicative Roads                    | Area exempted from Waimakariri River Stopbank Floodplain flood protection provisions (pc32 non-operative) | Ecological Heritage "A" Sites          |
| Airport/Airfield Noise Control Boundaries  | Protected Object, Place or Building | Flood Ponding Area  | Comprehensive Housing Improvement Area |
| Pylons                                     | Protected Tree                      | Flood Management Area   |  |
| Hazard line 1                              | Group of Protected Trees            |   |  |
| Esplanade Reserve or strip required        | Non-Operative Area                  |   |  |
| Scheduled Activity                         |                                     |   |  |
| M.H.W.S. Mean High Water Springs           |                                     |   |  |

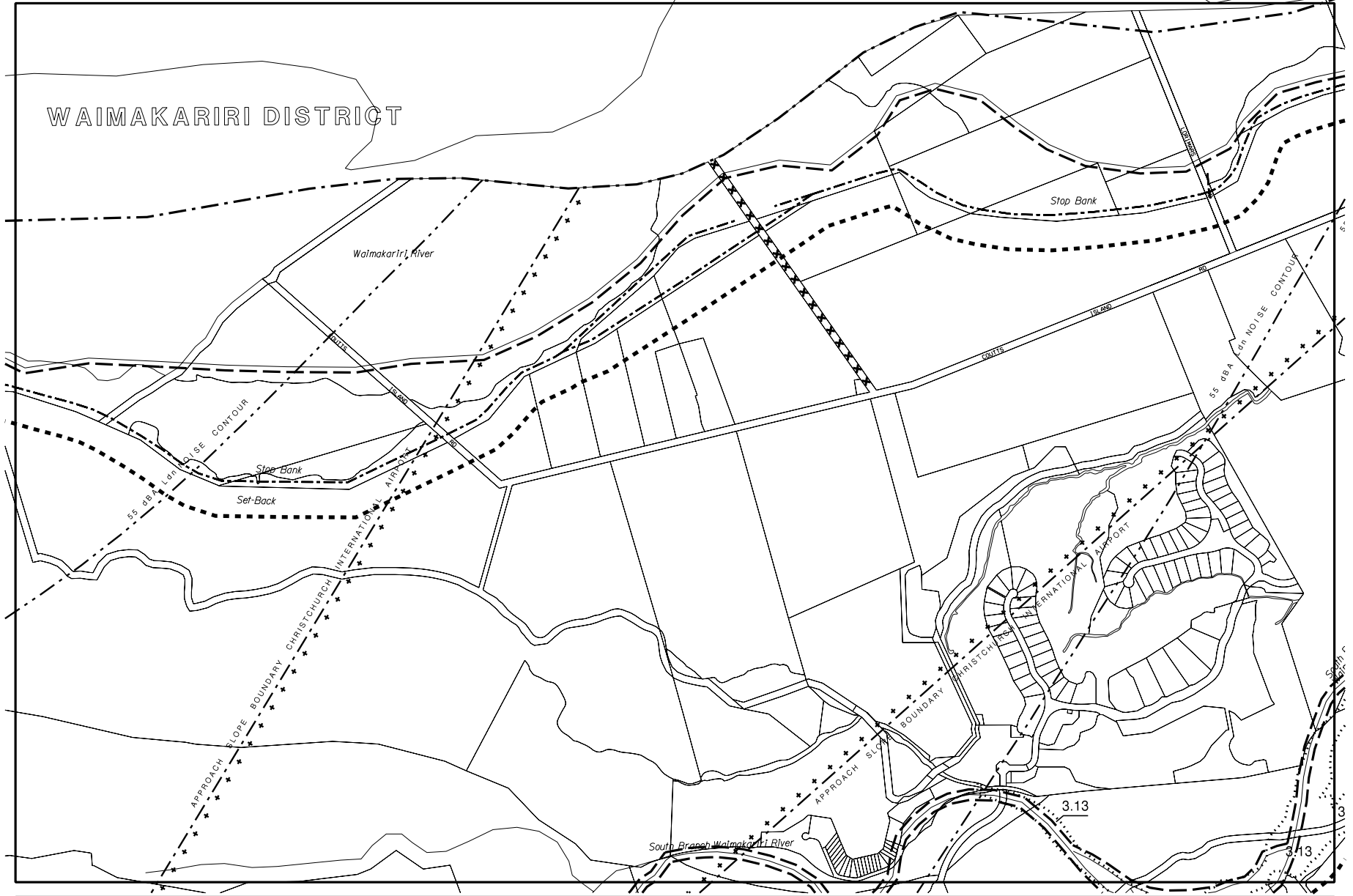
**City of Christchurch**  
**City Planning Maps**

Scale 1:15,000

NORTH

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7	<b>8B</b>	9
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# WAIMAKARIRI DISTRICT



## Key .... cont'd

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|--|---|---|--|
| <ul style="list-style-type: none"> <li>--- City Boundary</li> <li>--- Summit Road Protection Act</li> <li>--- Airport/Airfield Approach Slope Boundaries</li> <li>--- Airport/Airfield Noise Control Boundaries</li> <li>--- Pylons</li> <li>..... Hazard line 1</li> <li>--- Esplanade Reserve or strip required</li> <li>● Scheduled Activity</li> <li>M.H.W.S. Mean High Water Springs</li> </ul> | <ul style="list-style-type: none"> <li>xx Road to be stopped</li> <li>xx Limited Access Roads</li> <li>--- Indicative Roads</li> <li>■ Protected Object, Place or Building</li> <li>▲ Protected Tree</li> <li>▲ Group of Protected Trees</li> <li>□ Non-Operative Area</li> </ul> | <ul style="list-style-type: none"> <li>□ Waimakariri River stopbank floodplain (pc32 non-operative)</li> <li>□ High hazard area (pc32 non-operative)</li> <li>□ Area exempted from Waimakariri River Stopbank Floodplain flood protection provisions (pc32 non-operative)</li> <li>□ Flood Ponding Area</li> <li>□ Flood Management Area</li> </ul> | <ul style="list-style-type: none"> <li>SA# Special Amenity Area</li> <li>Community Footprint</li> <li># Ecological Heritage "A" Sites</li> <li>Comprehensive Housing Improvement Area</li> </ul> |
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**City of Christchurch**

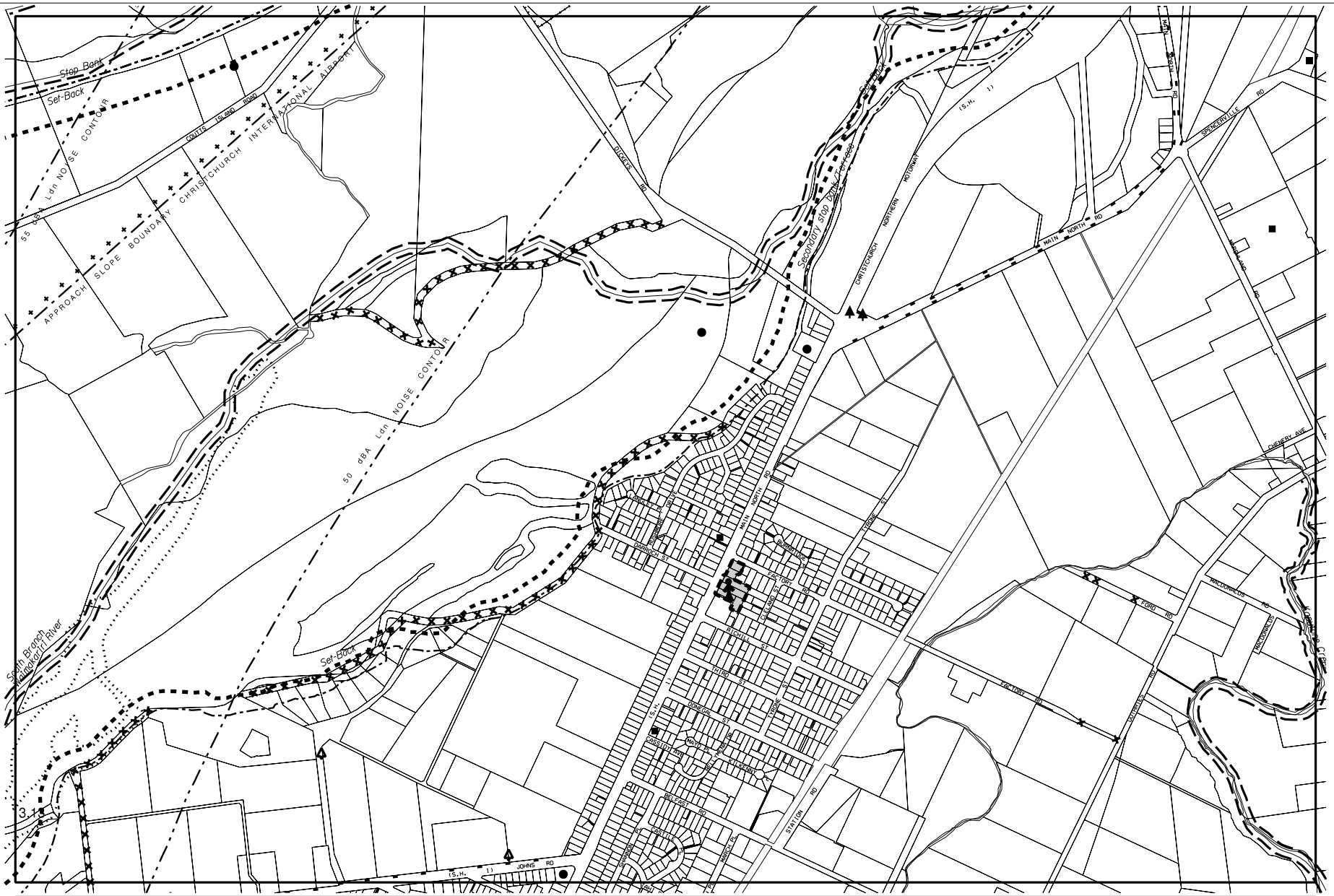
**City Planning Maps**

Scale 1:15,000

NORTH

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## Proposed Plan Change 74 - Airport Noise Contour



**Key .... cont'd**

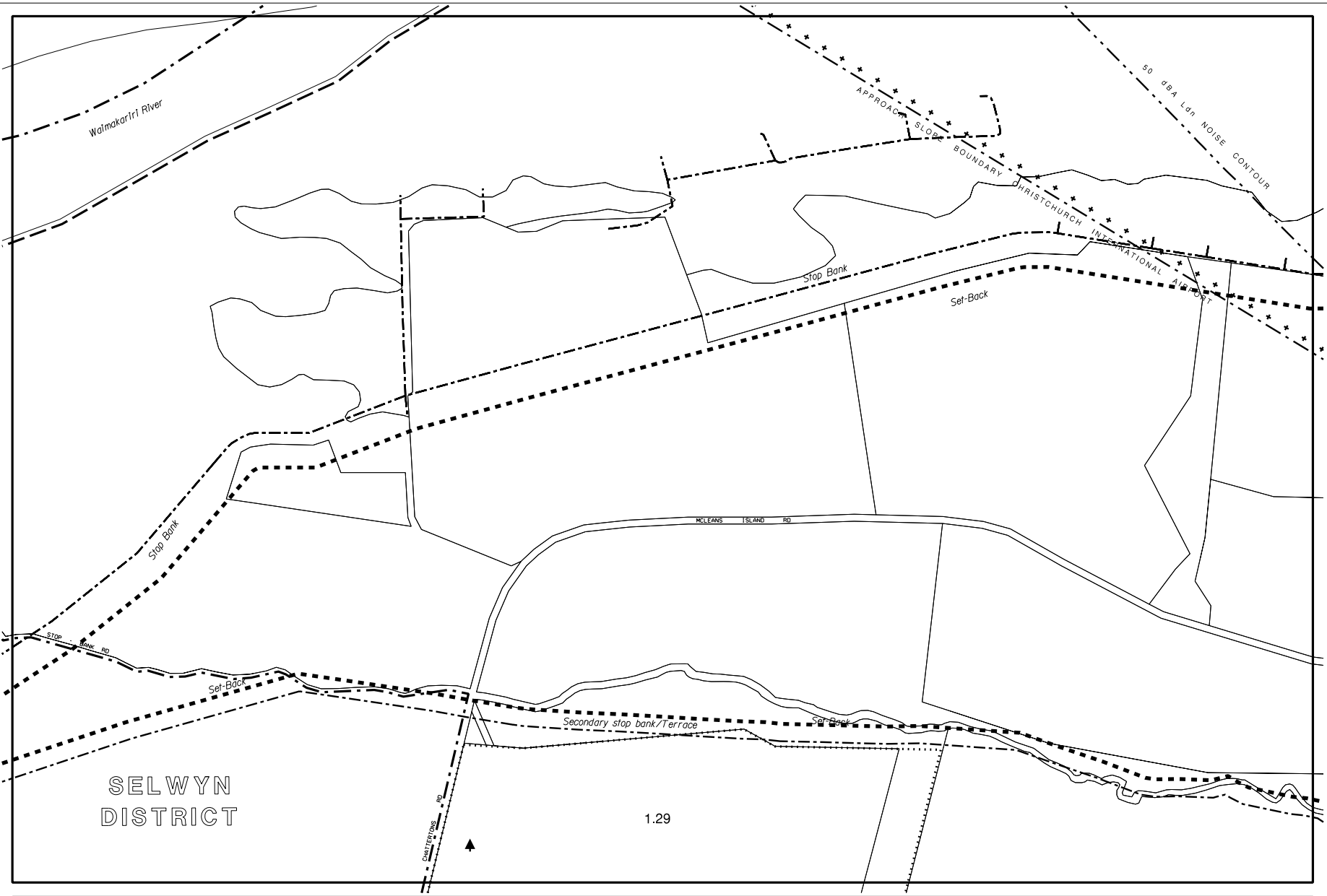
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**City of Christchurch**  
**City Planning Maps**

Scale 1:15,000  
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**NORTH**

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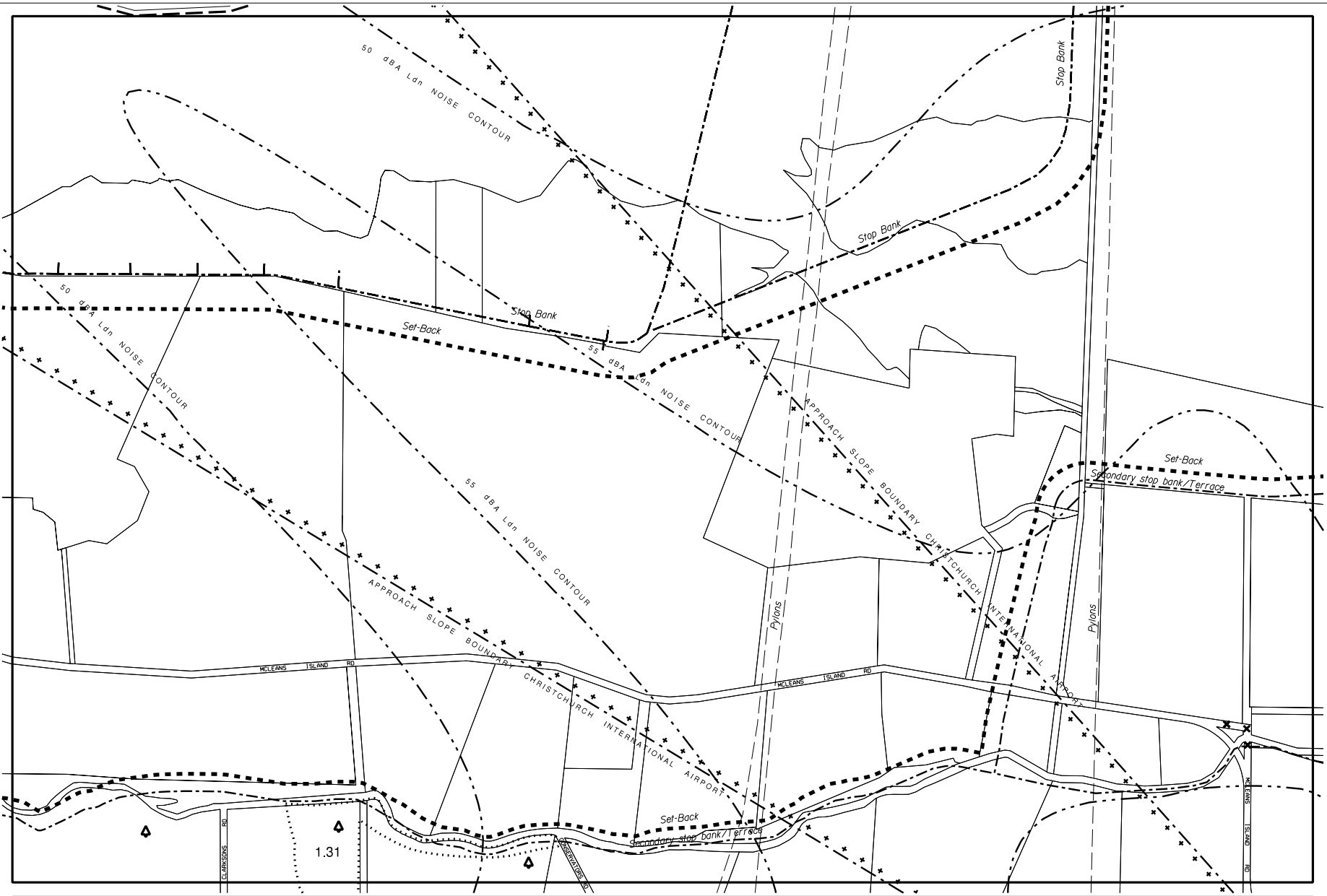
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| <ul style="list-style-type: none"> <li>--- City Boundary</li> <li>--- Summit Road Protection Act</li> <li>--- Airport/Airfield Approach Slope Boundaries</li> <li>--- Airport/Airfield Noise Control Boundaries</li> <li>--- Pylons</li> <li>..... Hazard line 1</li> <li>--- Esplanade Reserve or strip required</li> <li>● Scheduled Activity</li> <li>M.H.W.S. Mean High Water Springs</li> </ul> | <ul style="list-style-type: none"> <li>xxx Road to be stopped</li> <li>--- Limited Access Roads</li> <li>--- Indicative Roads</li> <li>■ Protected Object, Place or Building</li> <li>▲ Protected Tree</li> <li>▲ Group of Protected Trees</li> <li>□ Non-Operative Area</li> </ul> | <ul style="list-style-type: none"> <li>□ Waimakariri River stopbank floodplain (pc32 non-operative)</li> <li>□ High hazard area (pc32 non-operative)</li> <li>□ Area exempted from Waimakariri River Stopbank Floodplain flood protection provisions (pc32 non-operative)</li> <li>□ Flood Ponding Area</li> <li>□ Flood Management Area</li> </ul> | <ul style="list-style-type: none"> <li>ISAm# Special Amenity Area</li> <li>Community Footprint</li> <li># Ecological Heritage "A" Sites</li> <li>Comprehensive Housing Improvement Area</li> </ul> |
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**City of Christchurch**  
**City Planning Maps**  
 Scale 1:15,000  
 0 100 200 300 400m  
 NORTH

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13	<b>14B</b>	15
	21	





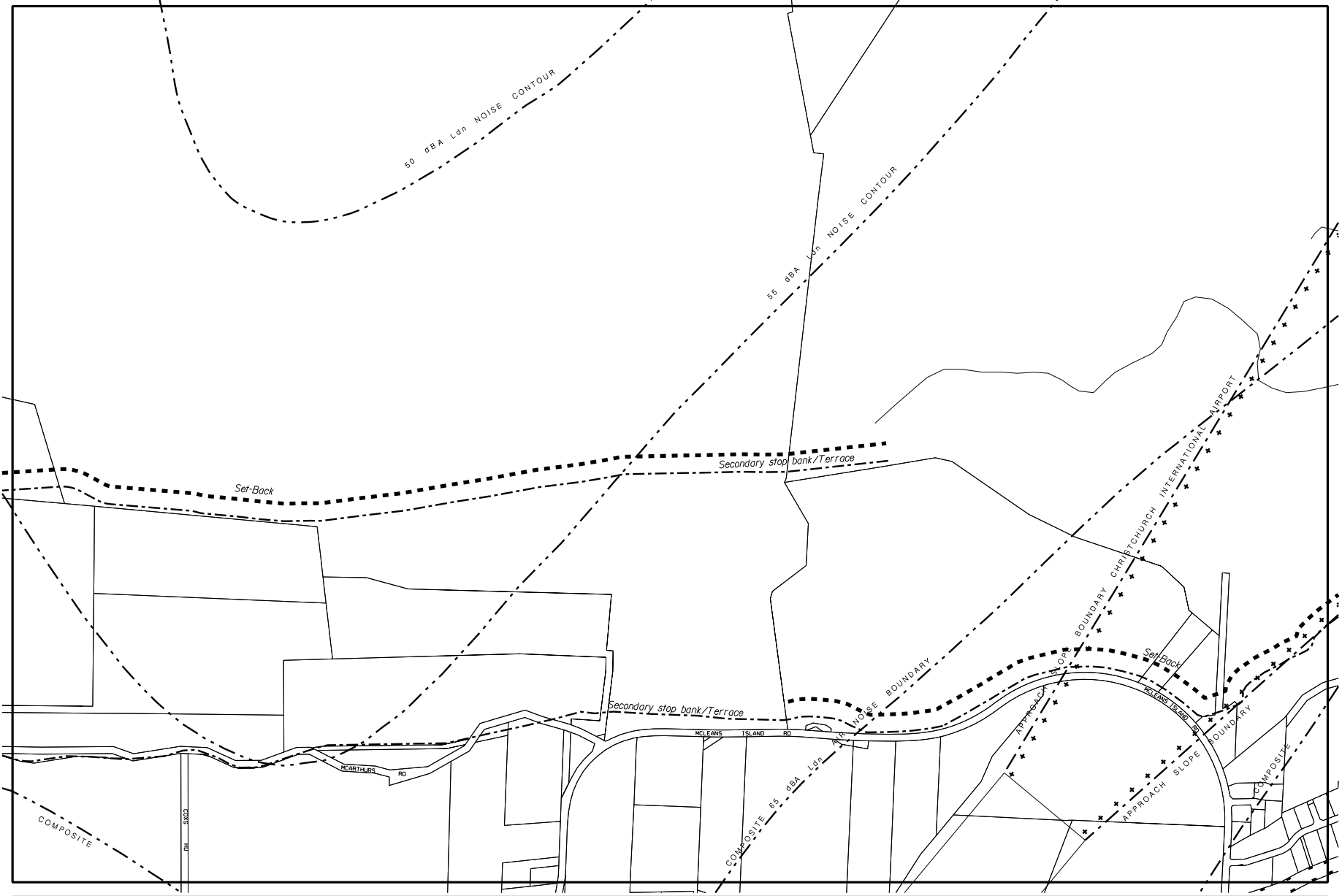
**Key .... cont'd**

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|--|---|---|--|
| <ul style="list-style-type: none"> <li>--- City Boundary</li> <li>--- Summit Road Protection Act</li> <li>--- Airport/Airfield Approach Slope Boundaries</li> <li>--- Airport/Airfield Noise Control Boundaries</li> <li>--- Pylons</li> <li>..... Hazard line 1</li> <li>--- Esplanade Reserve or strip required</li> <li>● Scheduled Activity</li> <li>M.H.W.S. Mean High Water Springs</li> </ul> | <ul style="list-style-type: none"> <li>xxx Road to be stopped</li> <li>--- Limited Access Roads</li> <li>--- Indicative Roads</li> <li>■ Protected Object, Place or Building</li> <li>▲ Protected Tree</li> <li>▲ Group of Protected Trees</li> <li>▨ Non-Operative Area</li> </ul> | <ul style="list-style-type: none"> <li>▨ Waimakariri River stopbank floodplain (pc32 non-operative)</li> <li>▨ High hazard area (pc32 non-operative)</li> <li>▨ Area exempted from Waimakariri River Stopbank Floodplain flood protection provisions (pc32 non-operative)</li> <li>▨ Flood Ponding Area</li> <li>▨ Flood Management Area</li> </ul> | <ul style="list-style-type: none"> <li>SA# Special Amenity Area</li> <li>Community Footprint</li> <li># Ecological Heritage "A" Sites</li> <li>Comprehensive Housing Improvement Area</li> </ul> |
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**City of Christchurch**  
**City Planning Maps**

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14	15B	16
	22	

Scale 1:15,000



**Key .... cont'd**

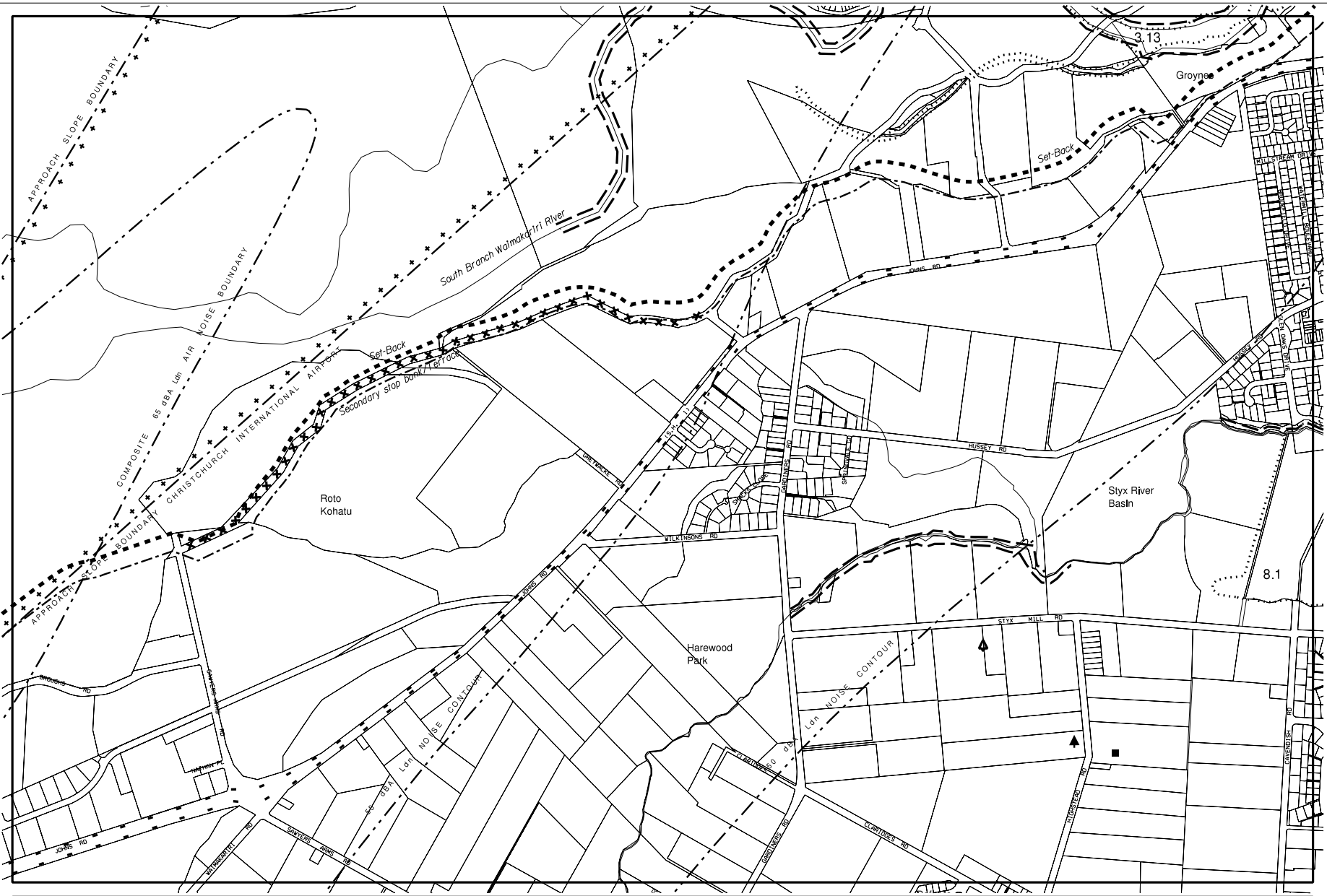
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|--|---|---|--|
| <ul style="list-style-type: none"> <li>--- City Boundary</li> <li>--- Summit Road Protection Act</li> <li>--- Airport/Airfield Approach Slope Boundaries</li> <li>--- Airport/Airfield Noise Control Boundaries</li> <li>--- Pylons</li> <li>..... Hazard line 1</li> <li>--- Esplanade Reserve or strip required</li> <li>● Scheduled Activity</li> <li>M.H.W.S. Mean High Water Springs</li> </ul> | <ul style="list-style-type: none"> <li>xxx Road to be stopped</li> <li>--- Limited Access Roads</li> <li>--- Indicative Roads</li> <li>■ Protected Object, Place or Building</li> <li>▲ Protected Tree</li> <li>▲ Group of Protected Trees</li> <li>□ Non-Operative Area</li> </ul> | <ul style="list-style-type: none"> <li>□ Waimakariri River stopbank floodplain (pc32 non-operative)</li> <li>□ High hazard area (pc32 non-operative)</li> <li>□ Area exempted from Waimakariri River Stopbank Floodplain flood protection provisions (pc32 non-operative)</li> <li>□ Flood Ponding Area</li> <li>□ Flood Management Area</li> </ul> | <ul style="list-style-type: none"> <li>SA# Special Amenity Area</li> <li>Community Footprint</li> <li># Ecological Heritage "A" Sites</li> <li>Comprehensive Housing Improvement Area</li> </ul> |
|--|---|---|--|

**City of Christchurch**  
City Planning Maps

Scale 1:15,000

NORTH

	8
15	<b>16B</b>
	17
	23



**Key .... cont'd**

- |  |   |   |  |
|--|---|---|--|
| <ul style="list-style-type: none"> <li>--- City Boundary</li> <li>--- Summit Road Protection Act</li> <li>--- Airport/Airfield Approach Slope Boundaries</li> <li>--- Airport/Airfield Noise Control Boundaries</li> <li>--- Pylons</li> <li>..... Hazard line 1</li> <li>--- Esplanade Reserve or strip required</li> <li>● Scheduled Activity</li> <li>M.H.W.S. Mean High Water Springs</li> </ul> | <ul style="list-style-type: none"> <li>xx Road to be stopped</li> <li>xx Limited Access Roads</li> <li>--- Indicative Roads</li> <li>■ Protected Object, Place or Building</li> <li>▲ Protected Tree</li> <li>▲ Group of Protected Trees</li> <li>□ Non-Operative Area</li> </ul> | <ul style="list-style-type: none"> <li>■ Waimakariri River stopbank floodplain (pc32 non-operative)</li> <li>■ High hazard area (pc32 non-operative)</li> <li>▨ Area exempted from Waimakariri River Stopbank Floodplain flood protection provisions (pc32 non-operative)</li> <li>■ Flood Ponding Area</li> <li>■ Flood Management Area</li> </ul> | <ul style="list-style-type: none"> <li>SA# Special Amenity Area</li> <li>■ Community Footprint</li> <li># Ecological Heritage "A" Sites</li> <li>■ Comprehensive Housing Improvement Area</li> </ul> |
|--|---|---|--|

**City of Christchurch**  
**City Planning Maps**

Scale 1:15,000

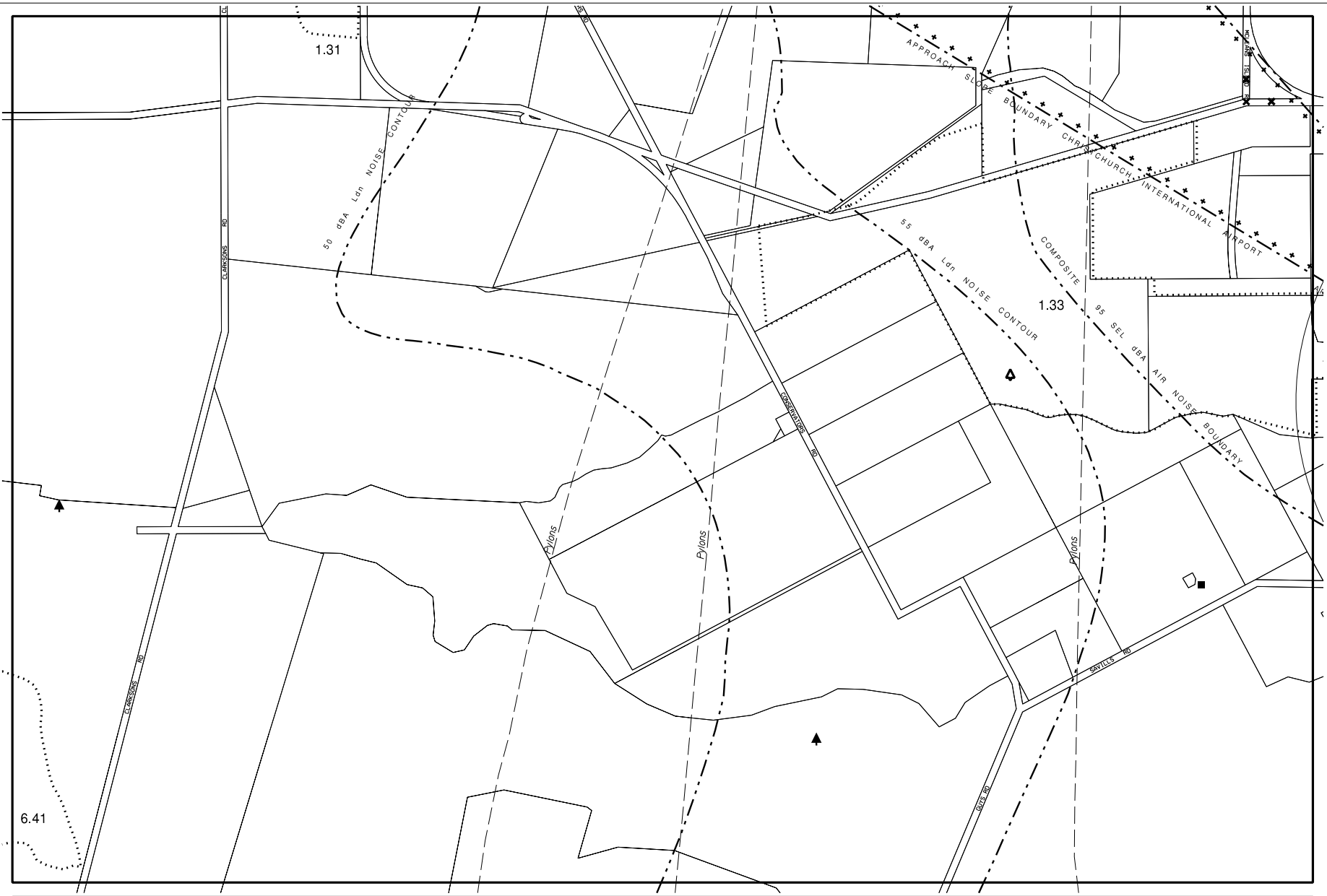
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NORTH

	9	
16	<b>17B</b>	18
	24	

**Proposed Plan Change 74 - Airport Noise Contour**





**Key .... cont'd**

- |  |  |   |  |
|--|--|---|--|
| <ul style="list-style-type: none"> <li>--- City Boundary</li> <li>--- Summit Road Protection Act</li> <li>--- Airport/Airfield Approach Slope Boundaries</li> <li>--- Airport/Airfield Noise Control Boundaries</li> <li>--- Pylons</li> <li>..... Hazard line 1</li> <li>--- Esplanade Reserve or strip required</li> <li>● Scheduled Activity</li> <li>M.H.W.S. Mean High Water Springs</li> </ul> | <ul style="list-style-type: none"> <li>xxx Road to be stopped</li> <li>--- Limited Access Roads</li> <li>--- Indicative Roads</li> <li>■ Protected Object, Place or Building</li> <li>▲ Protected Tree</li> <li>▲▲ Group of Protected Trees</li> <li>▨ Non-Operative Area</li> </ul> | <ul style="list-style-type: none"> <li>▨ Waimakariri River stopbank floodplain (pc32 non-operative)</li> <li>▨ High hazard area (pc32 non-operative)</li> <li>▨ Area exempted from Waimakariri River Stopbank Floodplain flood protection provisions (pc32 non-operative)</li> <li>▨ Flood Ponding Area</li> <li>▨ Flood Management Area</li> </ul> | <ul style="list-style-type: none"> <li>SA# Special Amenity Area</li> <li>Community Footprint</li> <li># Ecological Heritage "A" Sites</li> <li>Comprehensive Housing Improvement Area</li> </ul> |
|--|--|---|--|

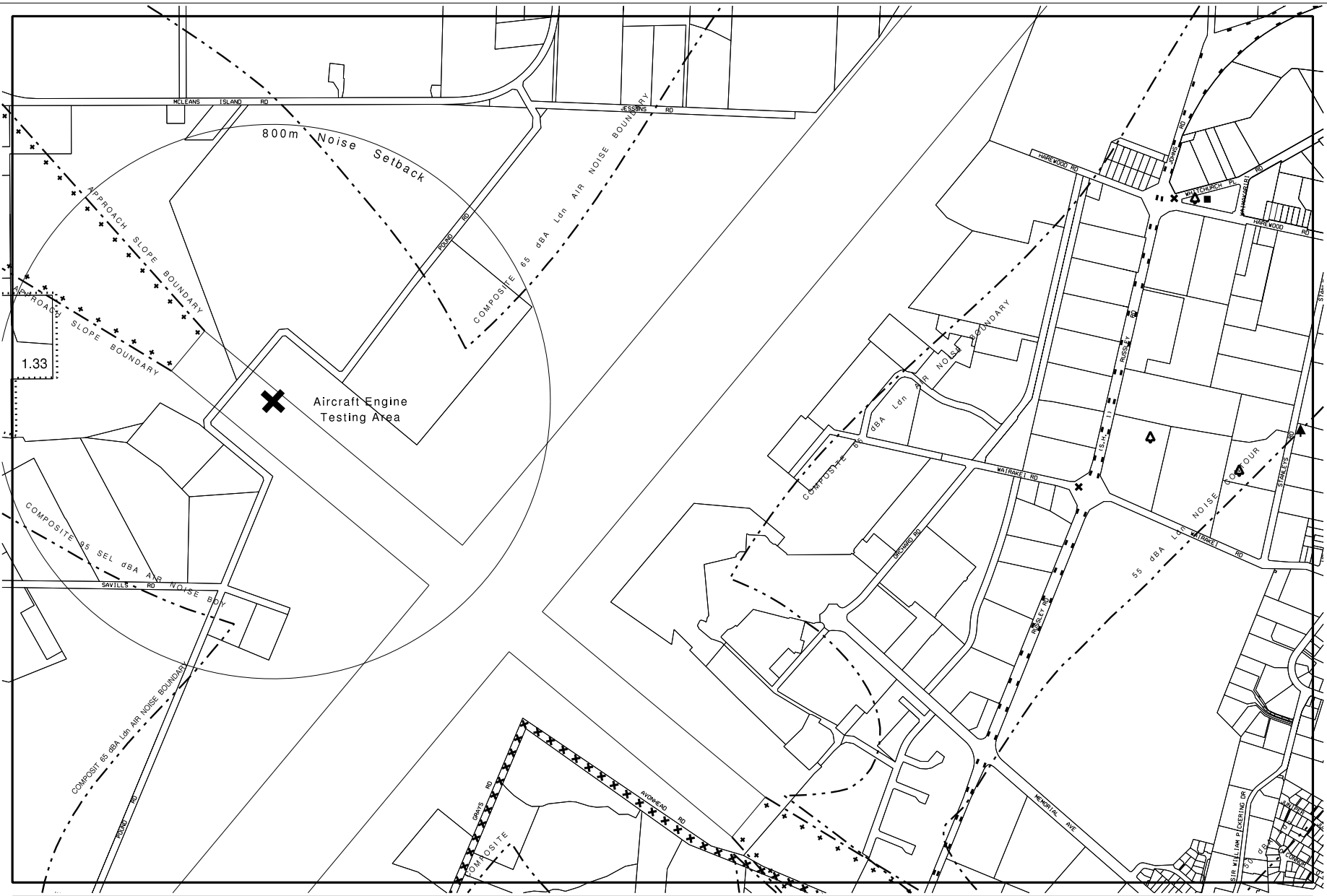
**City of Christchurch**  
City Planning Maps

Scale 1:15,000

NORTH

	15	
21	<b>22B</b>	23
	29	

**Proposed Plan Change 74 - Airport Noise Contour**



**Key .... cont'd**

- |  |                                       |   |  |
|--|---------------------------------------|---|--|
| --- City Boundary                                    | xxx Road to be stopped                | ▨ Waimakariri River stopbank floodplain (pc32 non-operative)  | SA# Special Amenity Area               |
| - - - Summit Road Protection Act                     | == Limited Access Roads               | ▩ High hazard area (pc32 non-operative)   | Community Footprint                    |
| - * - * - Airport/Airfield Approach Slope Boundaries | - - - Indicative Roads                | ▨ Area exempted from Waimakariri River Stopbank Floodplain flood protection provisions (pc32 non-operative) | # Ecological Heritage "A" Sites        |
| - * - * - Airport/Airfield Noise Control Boundaries  | ▣ Protected Object, Place or Building | ▨ Flood Ponding Area  | Comprehensive Housing Improvement Area |
| - - - Pylons   | ▲ Protected Tree                      |   |  |
| ..... Hazard line 1                                  | ▲▲ Group of Protected Trees           |   |  |
| - - - Esplanade Reserve or strip required            | ▨ Non-Operative Area                  |   |  |
| ● Scheduled Activity                                 | ▨ Flood Management Area               |   |  |
| M.H.W.S. Mean High Water Springs                     |                                       |   |  |

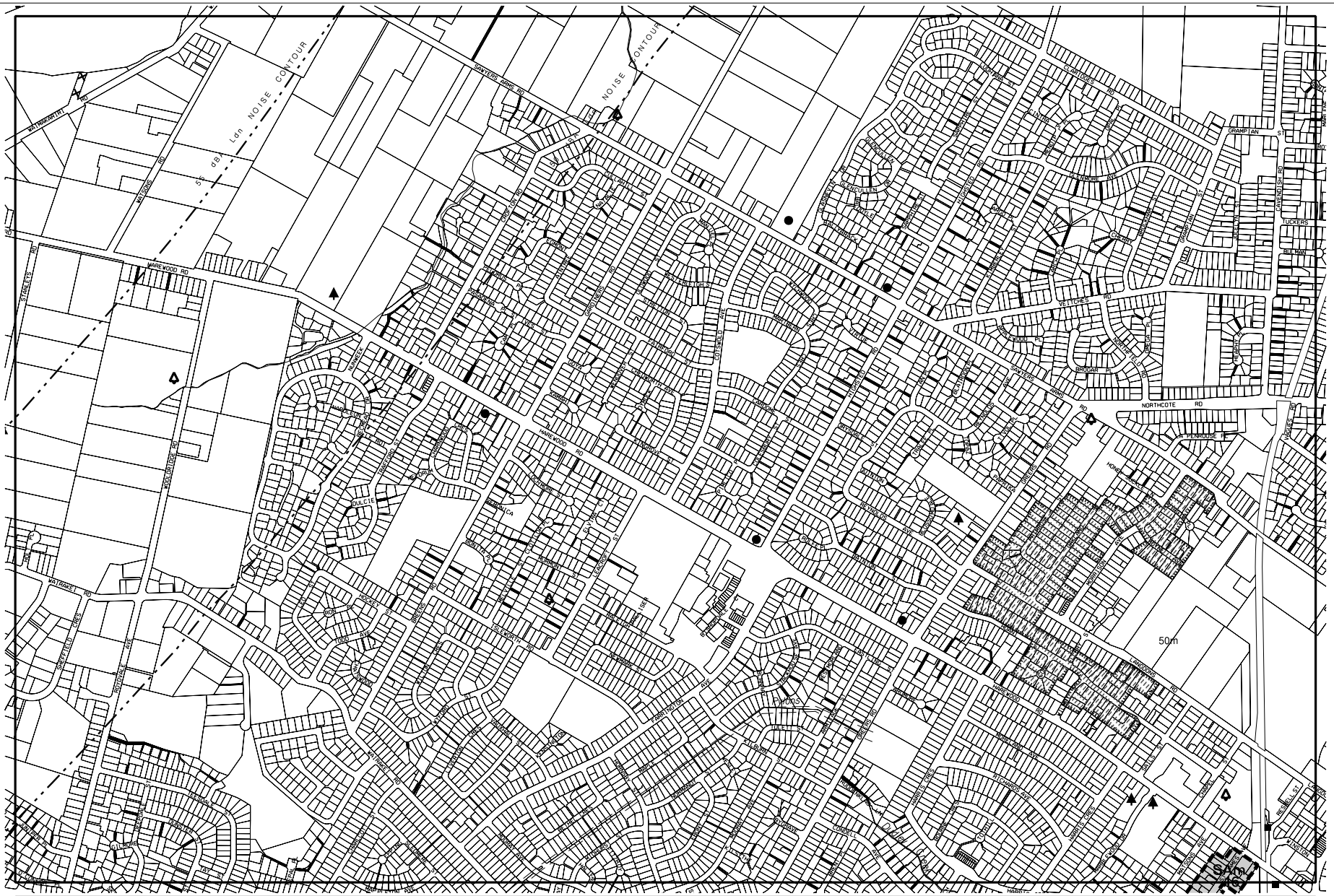
**City of Christchurch**  
**City Planning Maps**

Scale 1:15,000

0 100 200 300 400m

NORTH

	16	
22	<b>23B</b>	24
	30	



**Key .... cont'd**

- City Boundary
- Summit Road Protection Act
- Airport/Airfield Approach Slope Boundaries
- Airport/Airfield Noise Control Boundaries
- Pylons
- ..... Hazard line 1
- Esplanade Reserve or strip required
- Scheduled Activity
- M.H.W.S. Mean High Water Springs

- ✕ ✕ Road to be stopped
- ✕ ✕ Limited Access Roads
- Indicative Roads
- Protected Object, Place or Building
- ▲ Protected Tree
- ▲▲ Group of Protected Trees
- ▨ Non-Operative Area

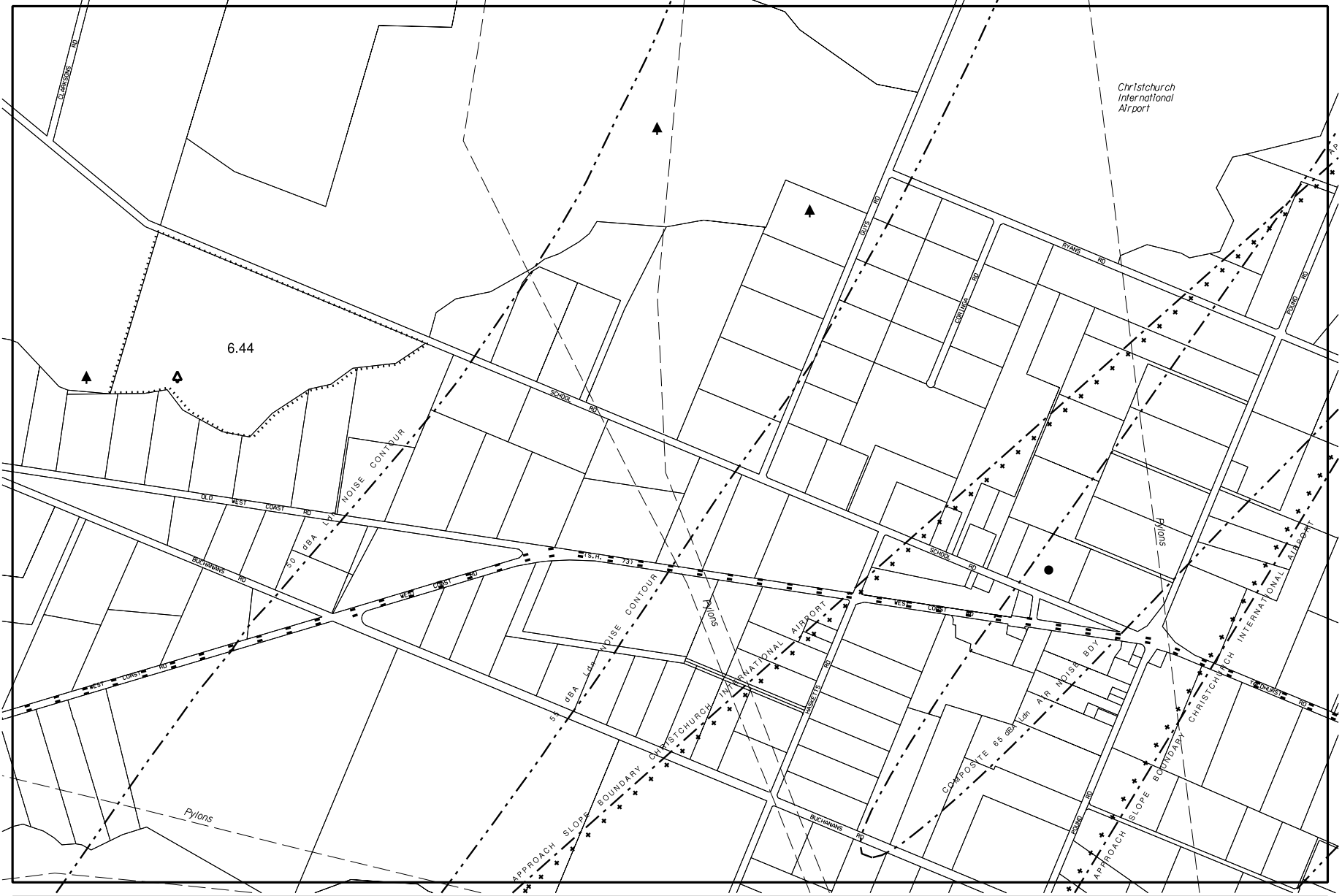
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- ▨ High hazard area (pc32 non-operative)
- ▨ Area exempted from Waimakariri River Stopbank Floodplain flood protection provisions (pc32 non-operative)
- ▨ Flood Ponding Area
- ▨ Flood Management Area

- SA# Special Amenity Area
- Community Footprint
- # Ecological Heritage "A" Sites
- Comprehensive Housing Improvement Area

**City of Christchurch**  
**City Planning Maps**  
 Scale 1:15,000  
 0 100 200 300 400m

	17	
23	<b>24B</b>	25
	31	

**Proposed Plan Change 74 - Airport Noise Contour**



Christchurch International Airport

6.44

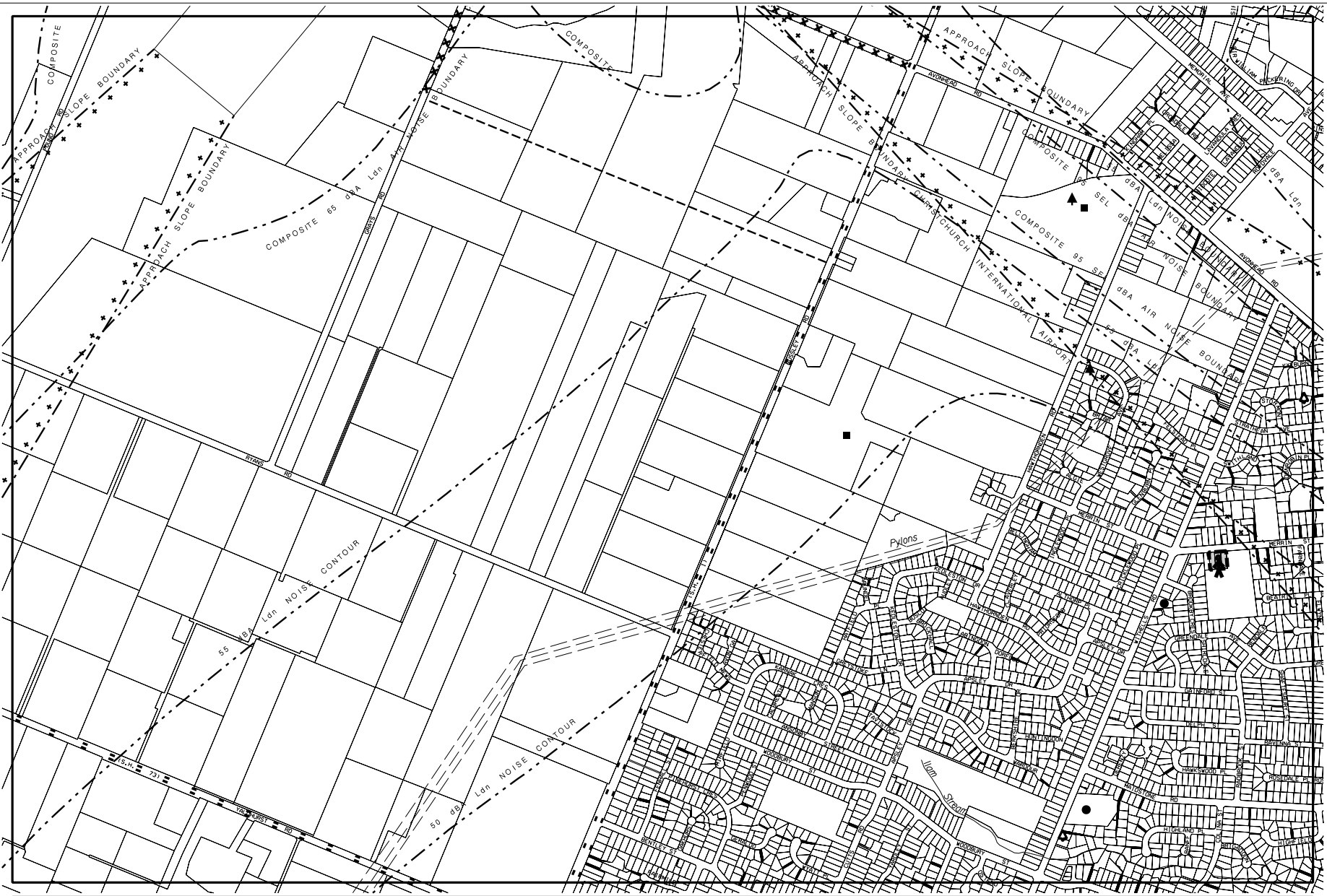
**Key .... cont'd**

- City Boundary
- Summit Road Protection Act
- Airport/Airfield Approach Slope Boundaries
- Airport/Airfield Noise Control Boundaries
- Pylons
- ..... Hazard line 1
- Esplanade Reserve or strip required
- Scheduled Activity
- M.H.W.S. Mean High Water Springs
- xx Road to be stopped
- xx Limited Access Roads
- Indicative Roads
- Protected Object, Place or Building
- ▲ Protected Tree
- ▲▲ Group of Protected Trees
- Non-Operative Area
- Waimakariri River stopbank floodplain (pc32 non-operative)
- High hazard area (pc32 non-operative)
- ▨ Area exempted from Waimakariri River Stopbank Floodplain flood protection provisions (pc32 non-operative)
- Flood Ponding Area
- Flood Management Area
- Special Amenity Area (SAM#)
- Community Footprint
- Ecological Heritage "A" Sites
- Comprehensive Housing Improvement Area

**City of Christchurch**  
**City Planning Maps**  
 Scale 1:15,000  
 0 100 200 300 400m  
 NORTH

		22	
28	<b>29B</b>	30	
		36	





**Key .... cont'd**

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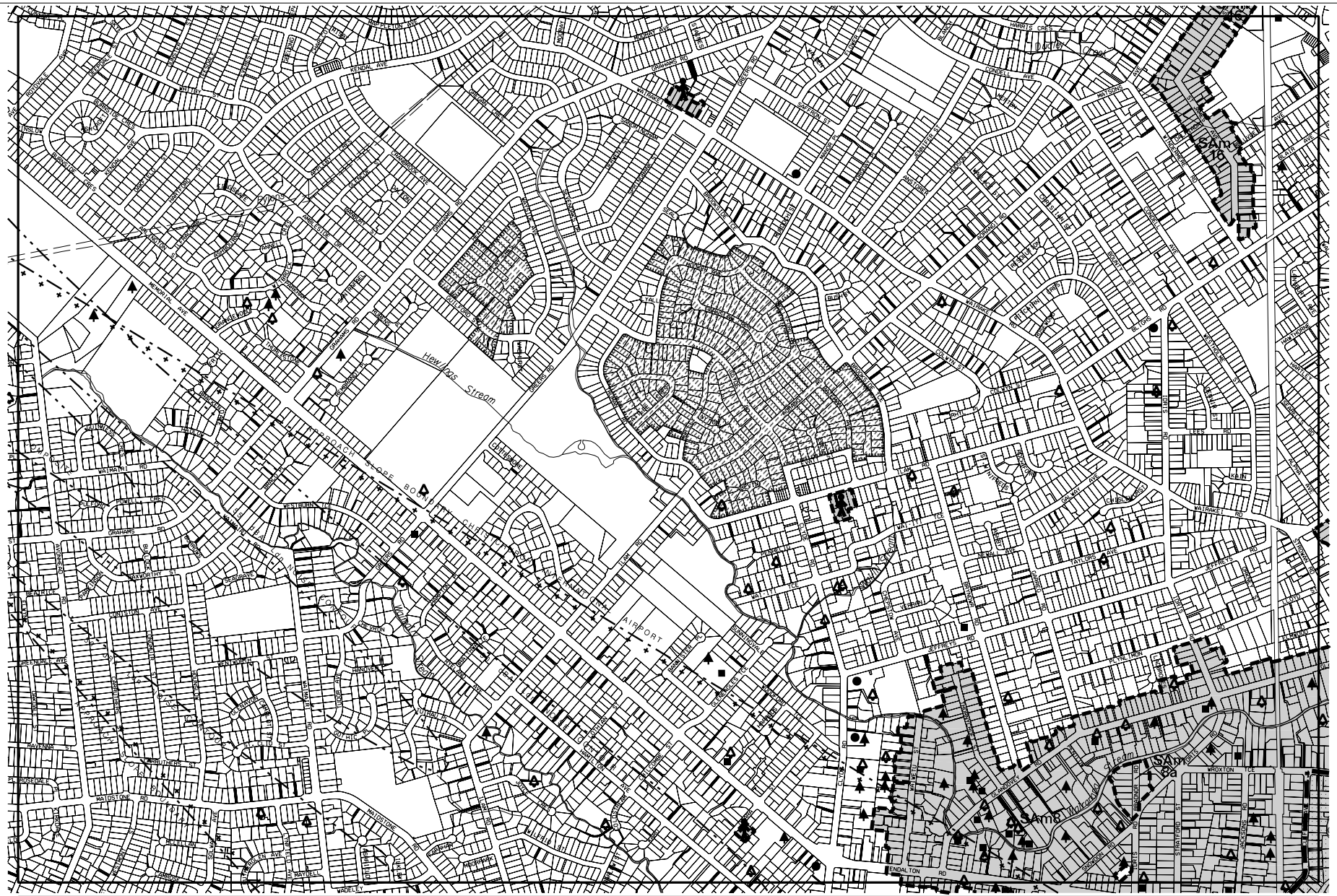
**City of Christchurch**  
City Planning Maps

Scale 1:15,000

NORTH


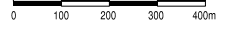
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		37	

**Proposed Plan Change 74 - Airport Noise Contour**



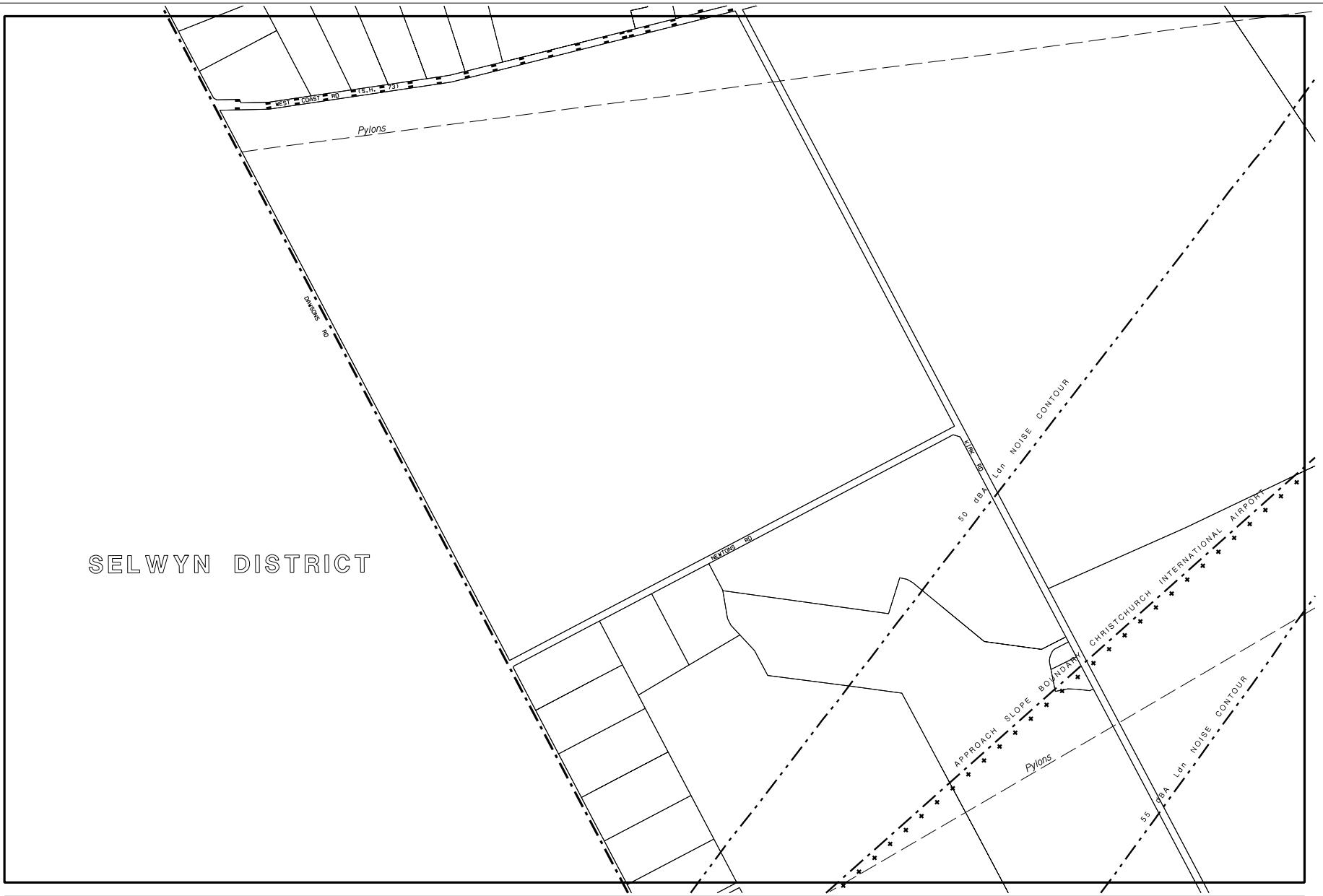
**Key .... cont'd**

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|--|---|---|---|


**City of Christchurch**  
 City Planning Maps  
 Scale 1:15,000  


	24	
	<b>31B</b>	32
	38	

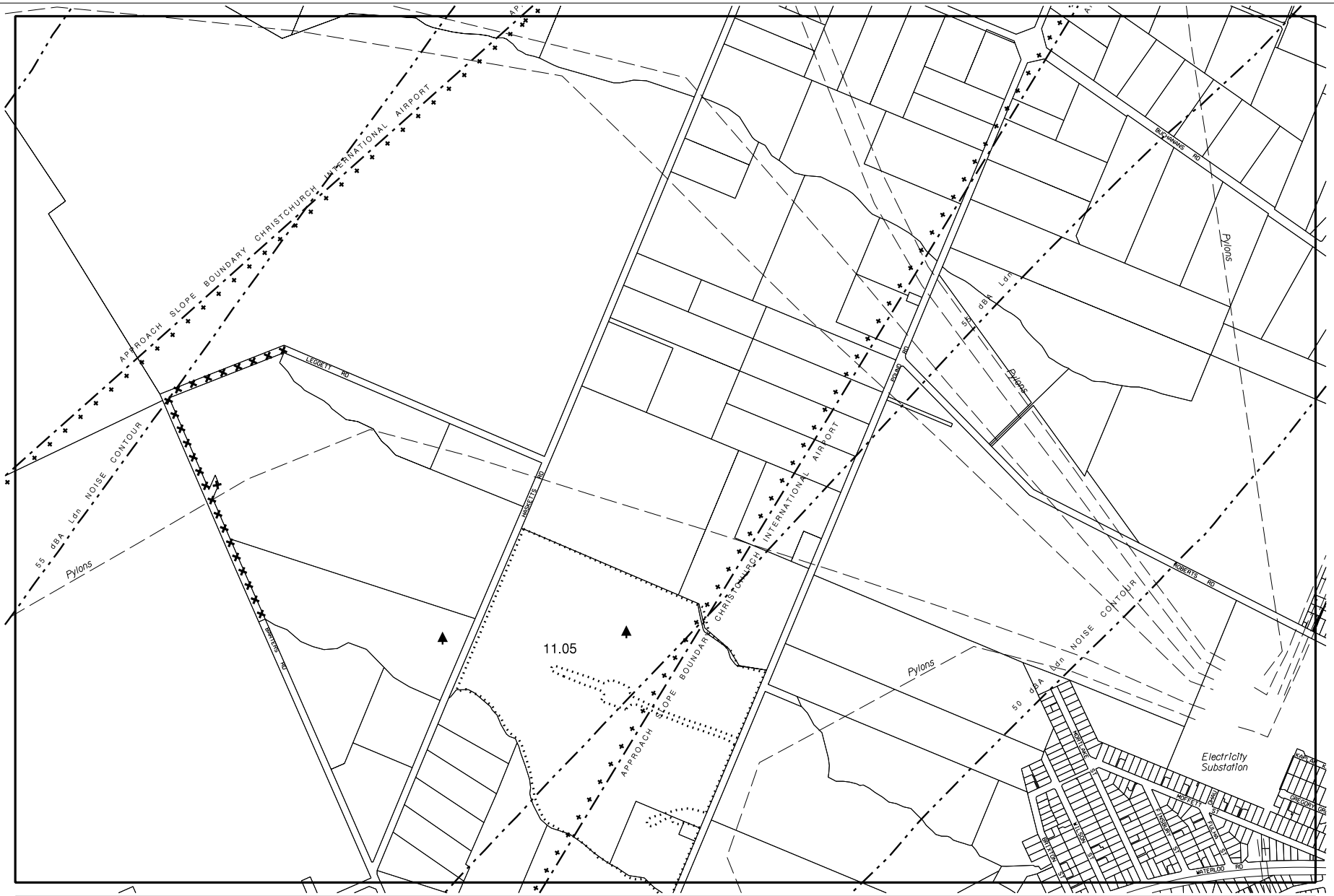
**Proposed Plan Change 74 - Airport Noise Contour**



SELWYN DISTRICT

<b>Key .... cont'd</b> - - - City Boundary - - - Summit Road Protection Act + + Airport/Airfield Approach Slope Boundaries - - - Airport/Airfield Noise Control Boundaries - - - Pylons ..... Hazard line 1 - - - Esplanade Reserve or strip required ● Scheduled Activity M.H.W.S. Mean High Water Springs		x x x Road to be stopped = = Limited Access Roads - - - Indicative Roads ■ Protected Object, Place or Building ▲ Protected Tree ▲ Group of Protected Trees □ Non-Operative Area		Waimakariri River stopbank floodplain (pc32 non-operative) High hazard area (pc32 non-operative) Area exempted from Waimakariri River Stopbank Floodplain flood protection provisions (pc32 non-operative) Flood Ponding Area Flood Management Area		[SAM#] Special Amenity Area Community Footprint Ecological Heritage "A" Sites Comprehensive Housing Improvement Area		<b>City of Christchurch</b> <b>City Planning Maps</b> Scale 1:15,000 0 100 200 300 400m NORTH		<table border="1"> <tr> <td></td> <td>28</td> <td></td> </tr> <tr> <td>-</td> <td><b>35B</b></td> <td>36</td> </tr> <tr> <td></td> <td>42</td> <td></td> </tr> </table>			28		-	<b>35B</b>	36		42	
	28																			
-	<b>35B</b>	36																		
	42																			

Proposed Plan Change 74 - Airport Noise Contour



**Key .... cont'd**

- |  |  |   |  |
|--|--|---|--|
| <ul style="list-style-type: none"> <li>--- City Boundary</li> <li>--- Summit Road Protection Act</li> <li>--- Airport/Airfield Approach Slope Boundaries</li> <li>--- Airport/Airfield Noise Control Boundaries</li> <li>--- Pylons</li> <li>..... Hazard line 1</li> <li>--- Esplanade Reserve or strip required</li> <li>● Scheduled Activity</li> <li>M.H.W.S. Mean High Water Springs</li> </ul> | <ul style="list-style-type: none"> <li>✕ ✕ ✕ Road to be stopped</li> <li>✕ ✕ Limited Access Roads</li> <li>--- Indicative Roads</li> <li>■ Protected Object, Place or Building</li> <li>▲ Protected Tree</li> <li>▲▲ Group of Protected Trees</li> <li>▭ Non-Operative Area</li> </ul> | <ul style="list-style-type: none"> <li>▭ Waimakariri River stopbank floodplain (pc32 non-operative)</li> <li>▭ High hazard area (pc32 non-operative)</li> <li>▨ Area exempted from Waimakariri River Stopbank Floodplain flood protection provisions (pc32 non-operative)</li> <li>▭ Flood Ponding Area</li> <li>▭ Flood Management Area</li> </ul> | <ul style="list-style-type: none"> <li>SA# Special Amenity Area</li> <li>Community Footprint</li> <li># Ecological Heritage "A" Sites</li> <li>Comprehensive Housing Improvement Area</li> </ul> |
|--|--|---|--|

**City of Christchurch**  
**City Planning Maps**

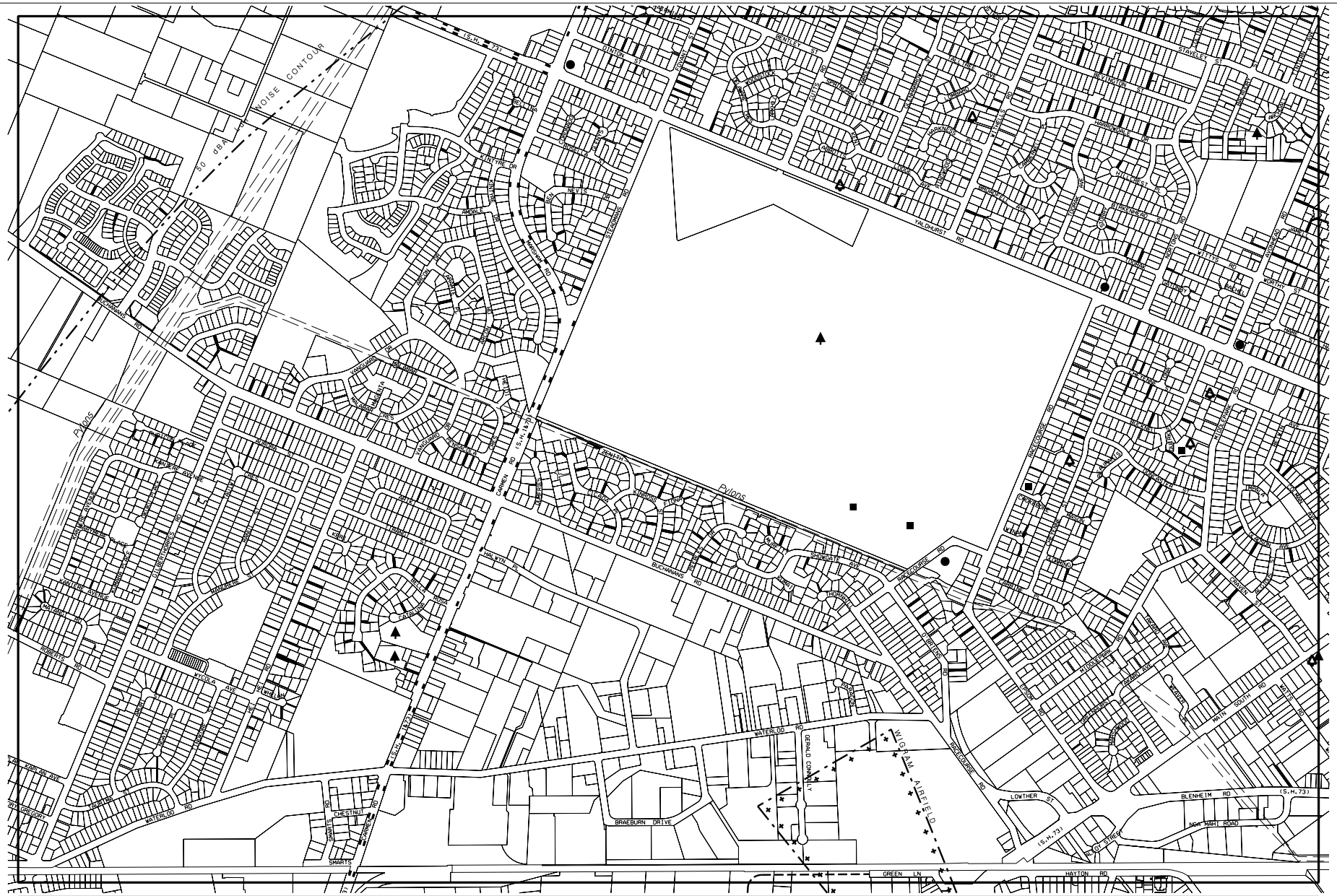
Scale 1:15,000

0 100 200 300 400m

NORTH

29
35 <b>36B</b> 37
43

**Proposed Plan Change 74 - Airport Noise Contour**



**Key .... cont'd**

- City Boundary
- Summit Road Protection Act
- Airport/Airfield Approach Slope Boundaries
- Airport/Airfield Noise Control Boundaries
- Pylons
- ..... Hazard line 1
- Esplanade Reserve or strip required
- Scheduled Activity
- M.H.W.S. Mean High Water Springs

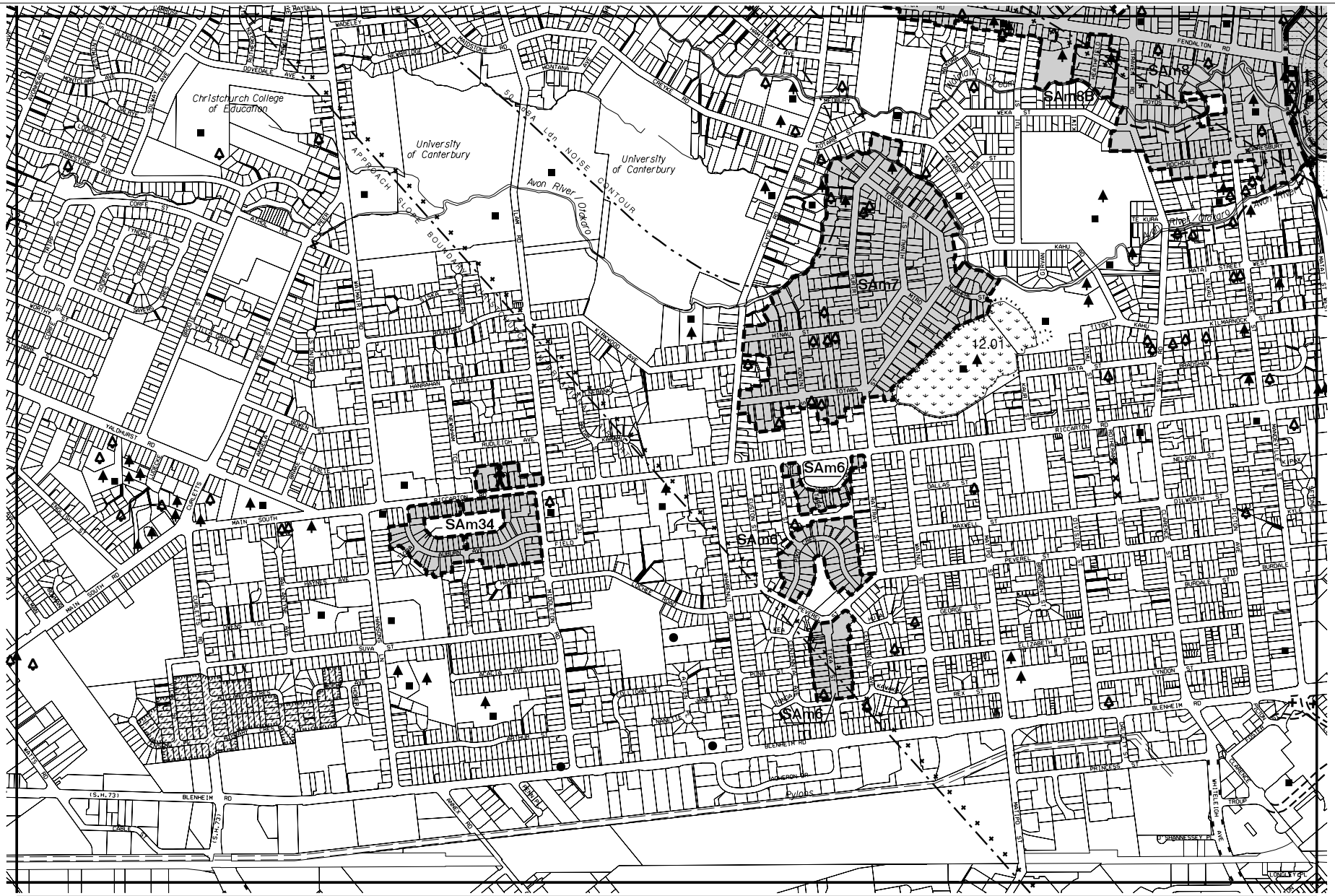
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- ⊘ Limited Access Roads
- Indicative Roads
- Protected Object, Place or Building
- ▲ Protected Tree
- ▲▲▲ Group of Protected Trees
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- SA# Special Amenity Area
- Community Footprint
- # Ecological Heritage "A" Sites
- Comprehensive Housing Improvement Area

**City of Christchurch**  
**City Planning Maps**  
 Scale 1:15,000  
 0 100 200 300 400m  
 NORTH

		30	
36	<b>37B</b>	38	
		44	



**Key .... cont'd**

- |  |   |   |   |
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|--|---|---|---|

**City of Christchurch**  
**City Planning Maps**

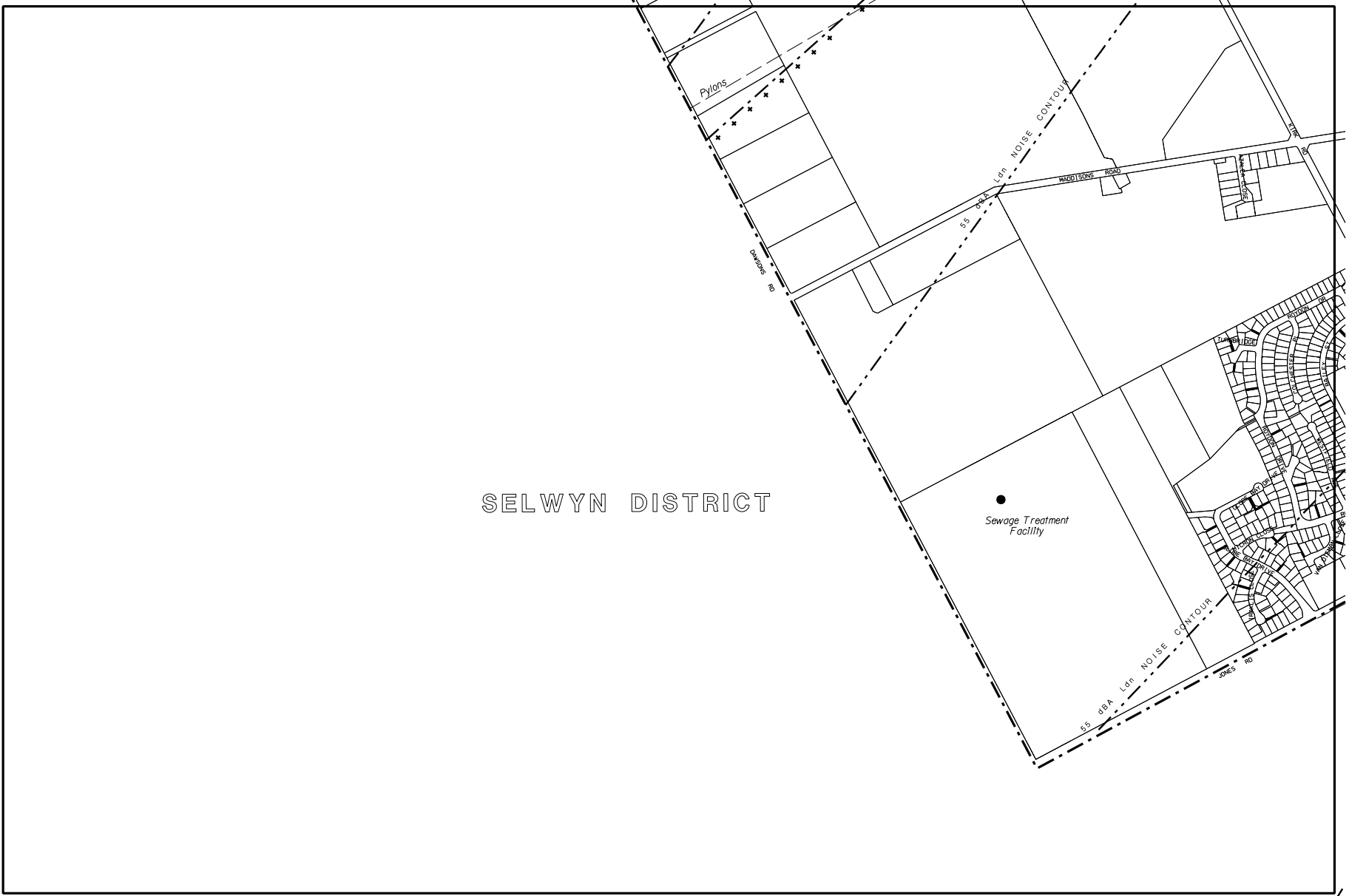
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Scale 1:15,000  
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NORTH

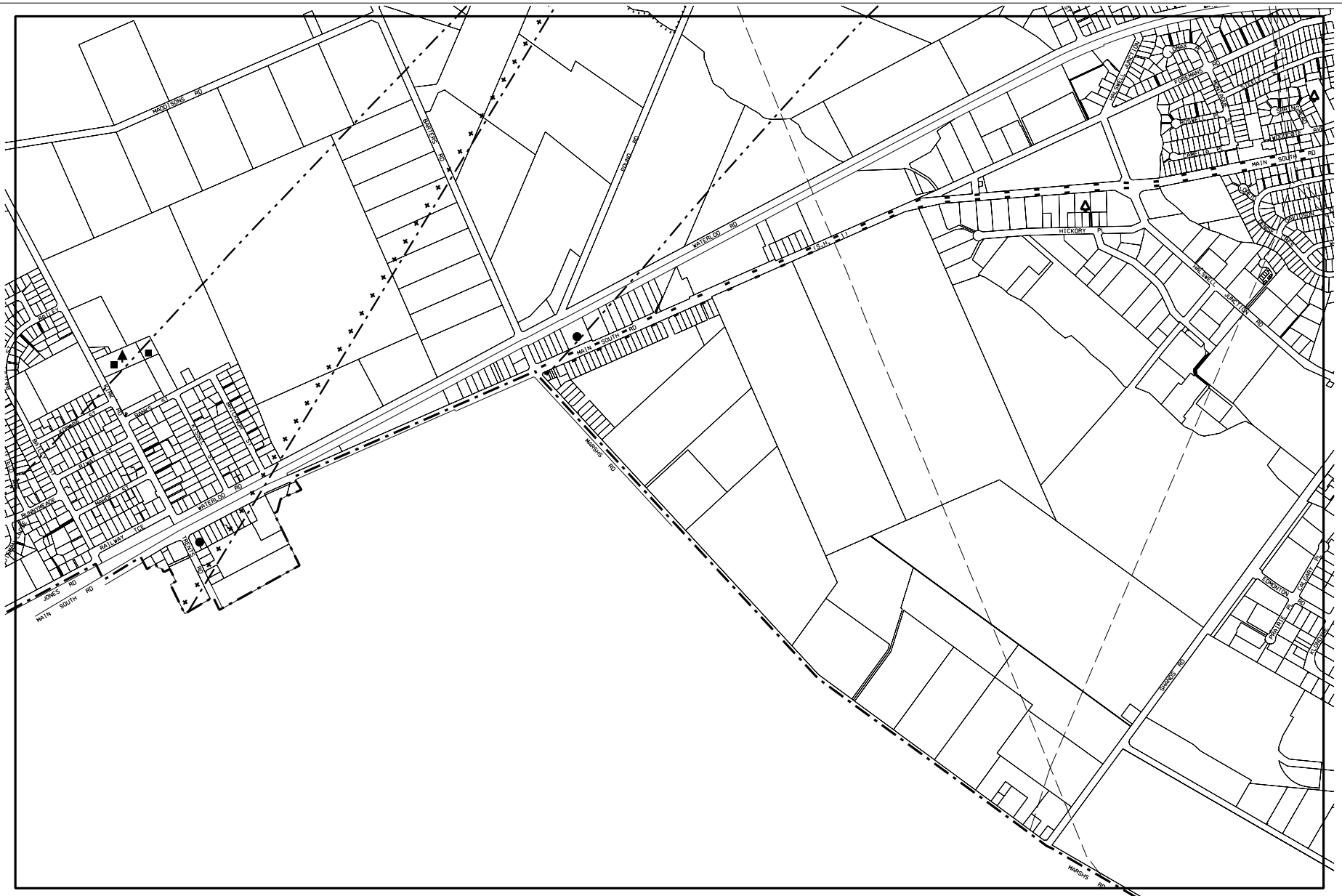
**Proposed Plan Change 74 - Airport Noise Contour**

# SELWYN DISTRICT



<b>Key .... cont'd</b>		<b>City of Christchurch</b> <b>City Planning Maps</b>		<table border="1"> <tr> <td></td> <td>35</td> <td></td> </tr> <tr> <td>-</td> <td><b>42B</b></td> <td>43</td> </tr> <tr> <td></td> <td>-</td> <td></td> </tr> </table>		35		-	<b>42B</b>	43		-	
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-	<b>42B</b>	43											
	-												
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## Proposed Plan Change 74 - Airport Noise Contour



**Key .... cont'd**




- |  |   |   |  |
|--|---|---|--|
| <ul style="list-style-type: none"> <li>--- City Boundary</li> <li>- - - Summit Road Protection Act</li> <li>+ + + Airport/Airfield Approach Slope Boundaries</li> <li>- . - Airport/Airfield Noise Control Boundaries</li> <li>— Pylons</li> <li>..... Hazard line 1</li> <li>- - - Esplanade Reserve or strip required</li> <li>● Scheduled Activity</li> <li>M.H.W.S. Mean High Water Springs</li> </ul> | <ul style="list-style-type: none"> <li>x x x Road to be stopped</li> <li>- x - Limited Access Roads</li> <li>- - - Indicative Roads</li> <li>■ Protected Object, Place or Building</li> <li>▲ Protected Tree</li> <li>▲▲▲ Group of Protected Trees</li> <li>▭ Non-Operative Area</li> </ul> | <ul style="list-style-type: none"> <li>▭ Waimakariri River stopbank floodplain (pc32 non-operative)</li> <li>▭ High hazard area (pc32 non-operative)</li> <li>▨ Area exempted from Waimakariri River Stopbank Floodplain flood protection provisions (pc32 non-operative)</li> <li>▭ Flood Ponding Area</li> <li>▭ Flood Management Area</li> </ul> | <ul style="list-style-type: none"> <li>SA# Special Amenity Area</li> <li>Community Footprint</li> <li># Ecological Heritage "A" Sites</li> <li>Comprehensive Housing Improvement Area</li> </ul> |
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<p><b>City of Christchurch</b></p> <p><b>City Planning Maps</b></p> <p>Scale 1:15,000 Date 29/02/2012</p>	<table border="1"> <tr> <td style="width: 20px; height: 20px;"></td> <td style="width: 20px; height: 20px; text-align: center;">36</td> <td style="width: 20px; height: 20px;"></td> </tr> <tr> <td style="width: 20px; height: 20px; text-align: center;">42</td> <td style="width: 20px; height: 20px; text-align: center; font-size: 1.2em;"><b>43B</b></td> <td style="width: 20px; height: 20px; text-align: center;">44</td> </tr> <tr> <td style="width: 20px; height: 20px;"></td> <td style="width: 20px; height: 20px; text-align: center;">50</td> <td style="width: 20px; height: 20px;"></td> </tr> </table>		36		42	<b>43B</b>	44		50	
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42	<b>43B</b>	44								
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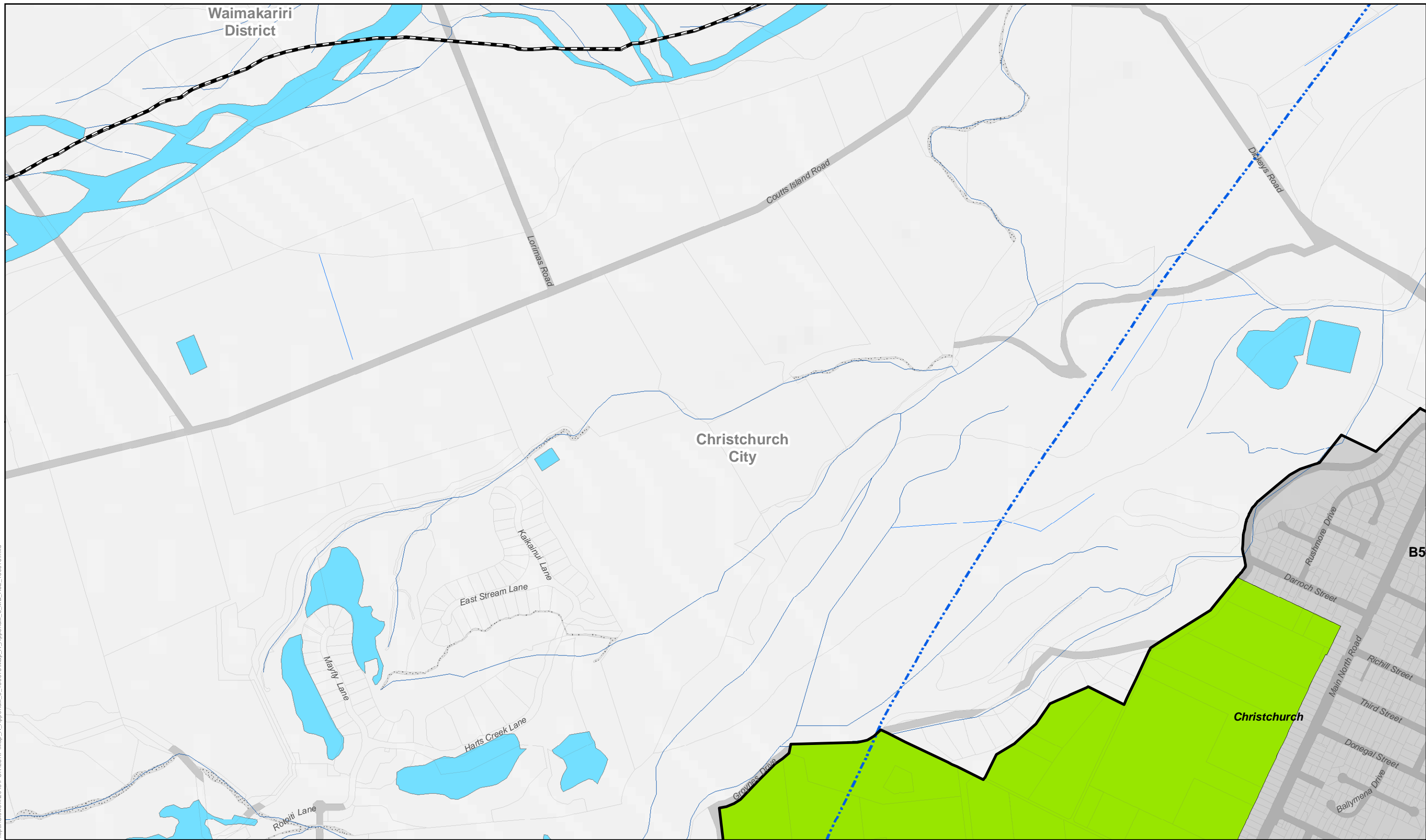
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# SELWYN DISTRICT

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-	<b>50B</b>	51													
	-														
<ul style="list-style-type: none"> <li>--- City Boundary</li> <li>--- Summit Road Protection Act</li> <li>--- Airport/Airfield Approach Slope Boundaries</li> <li>--- Airport/Airfield Noise Control Boundaries</li> <li>--- Pylons</li> <li>..... Hazard line 1</li> <li>--- Esplanade Reserve or strip required</li> <li>● Scheduled Activity</li> <li>M.H.W.S. Mean High Water Springs</li> </ul>	<ul style="list-style-type: none"> <li>xxx Road to be stopped</li> <li>== Limited Access Roads</li> <li>--- Indicative Roads</li> <li>■ Protected Object, Place or Building</li> <li>▲ Protected Tree</li> <li>▲ Group of Protected Trees</li> <li>▨ Non-Operative Area</li> </ul>	<ul style="list-style-type: none"> <li>▨ Waimakariri River stopbank floodplain (pc32 non-operative)</li> <li>▨ High hazard area (pc32 non-operative)</li> <li>▨ Area exempted from Waimakariri River Stopbank Floodplain flood protection provisions (pc32 non-operative)</li> <li>▨ Flood Ponding Area</li> <li>▨ Flood Management Area</li> </ul>	<ul style="list-style-type: none"> <li>SA# Special Amenity Area</li> <li>Community Footprint</li> <li># Ecological Heritage "A" Sites</li> <li>Comprehensive Housing Improvement Area</li> </ul>	Scale 1:15,000 											

## Proposed Plan Change 74 - Airport Noise Contour

**Attachment 1: Enlargements of Map A for the Christchurch City Council  
Territorial Authority Area.**



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- ★ Key Activity Centres
- ▭ Boundary of infrastructure supported priority, existing urban and greenfield areas.
- ▭ Territorial Authority Boundary
- ⋯ 50dba Airport Noise Contour
- CERA Red Zoned Land
- Christchurch Central Recovery Plan Area
- Existing Urban Area - Pre 2011

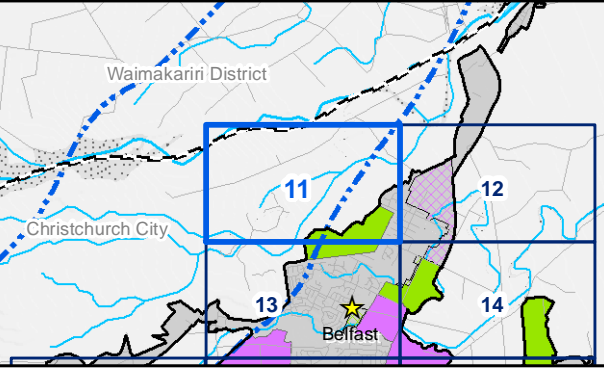
- Priority Areas - Business**
- Zoned
  - Not Yet Zoned
- B## Business Identifier Number
- Greenfield Areas - post Land Use Recovery Plan**
- Residential
  - Business

- Priority Areas - Residential**
- Zoned
  - Not Yet Zoned
- R## Residential Identifier Number

Greenfield Priority Areas

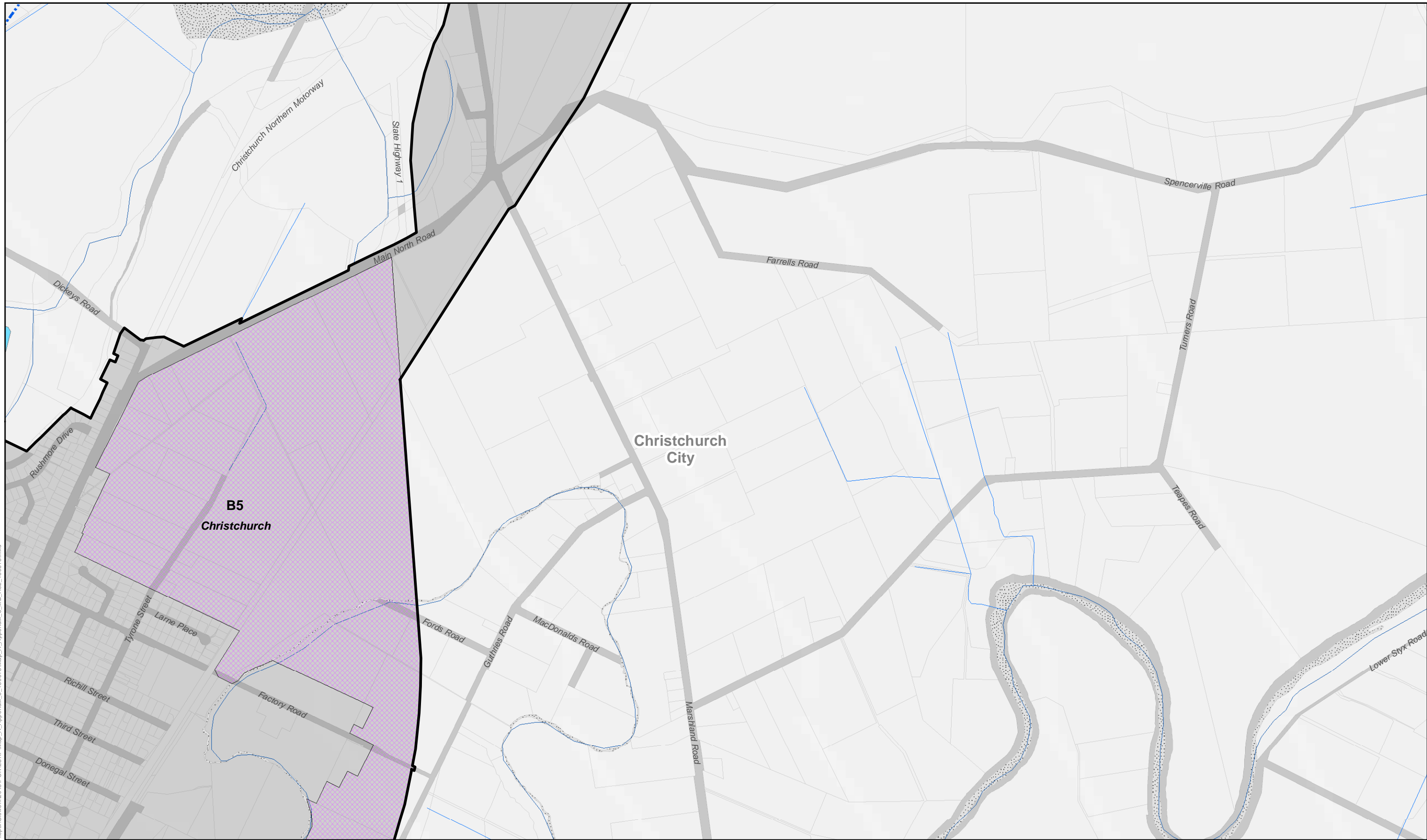
0 100 200 300 400 500 Metres

**Environment Canterbury**  
Regional Council  
Kaitiaki Taiao ki Waikaha



MAP

**11**



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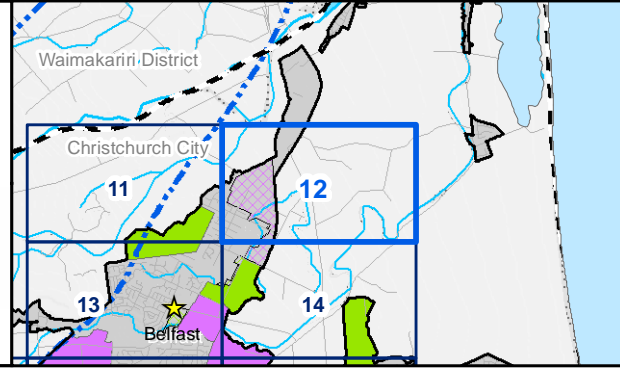
- ★ Key Activity Centres
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- CERA Red Zoned Land
- Christchurch Central Recovery Plan Area
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- Priority Areas - Business**
- Zoned
  - Not Yet Zoned
- B## Business Identifier Number**
- Greenfield Areas - post Land Use Recovery Plan**
- Residential
  - Business

- Priority Areas - Residential**
- Zoned
  - Not Yet Zoned
- R## Residential Identifier Number**

Greenfield Priority Areas

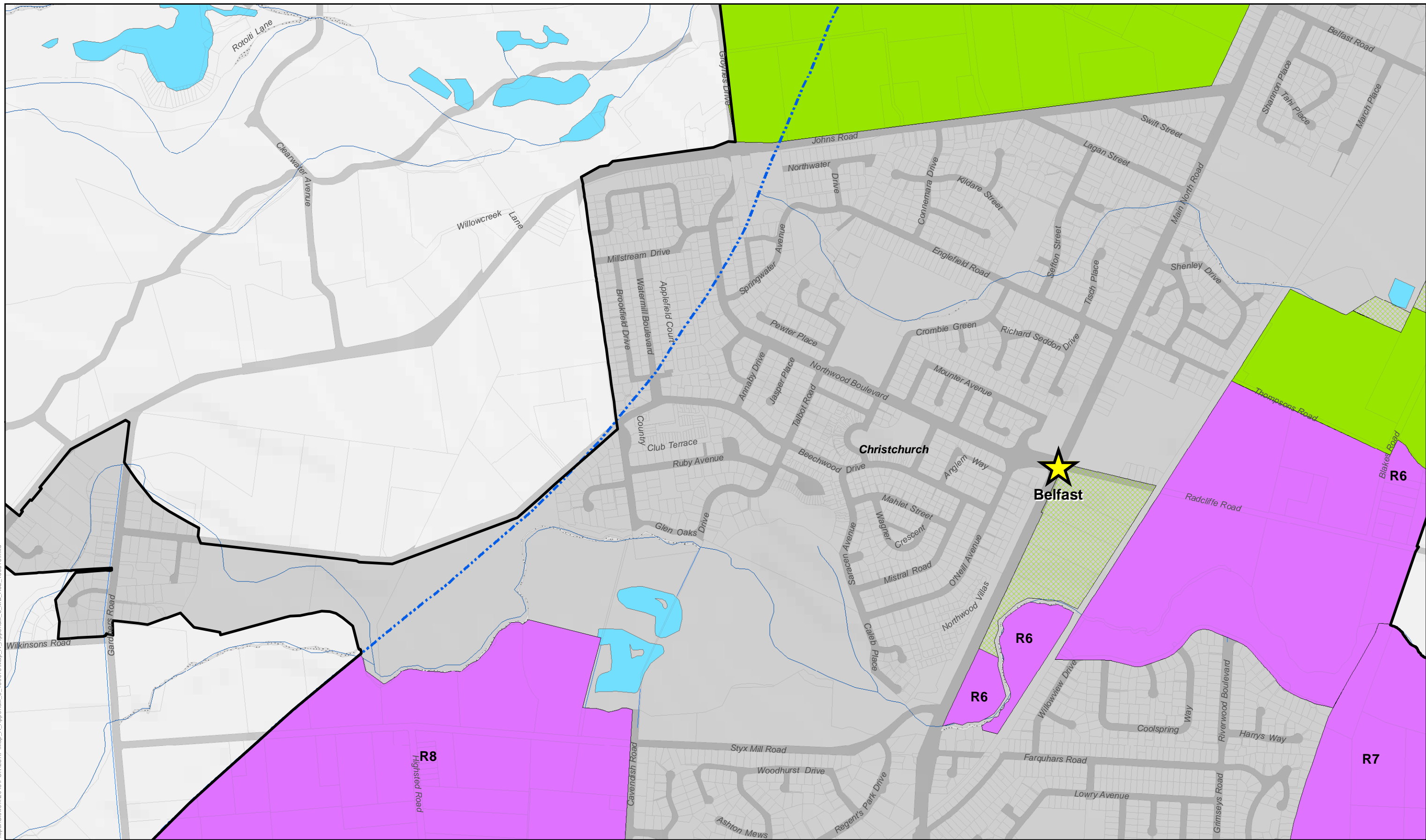
**Environment Canterbury**  
Regional Council  
Kaitiaki Taiao ki Waikaha



MAP

**12**





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- Key Activity Centres
- Boundary of infrastructure supported priority, existing urban and greenfield areas.
- Territorial Authority Boundary
- 50dba Airport Noise Contour
- CERA Red Zoned Land
- Christchurch Central Recovery Plan Area
- Existing Urban Area - Pre 2011

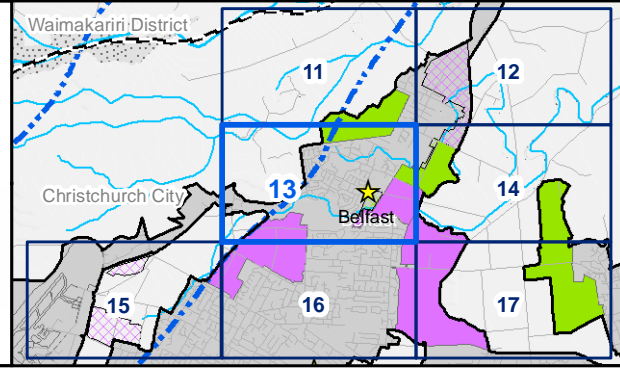
- Priority Areas - Business**
- Zoned
  - Not Yet Zoned
  - B## Business Identifier Number**
- Greenfield Areas - post Land Use Recovery Plan**
- Residential
  - Business

- Priority Areas - Residential**
- Zoned
  - Not Yet Zoned
  - R## Residential Identifier Number**

Greenfield Priority Areas

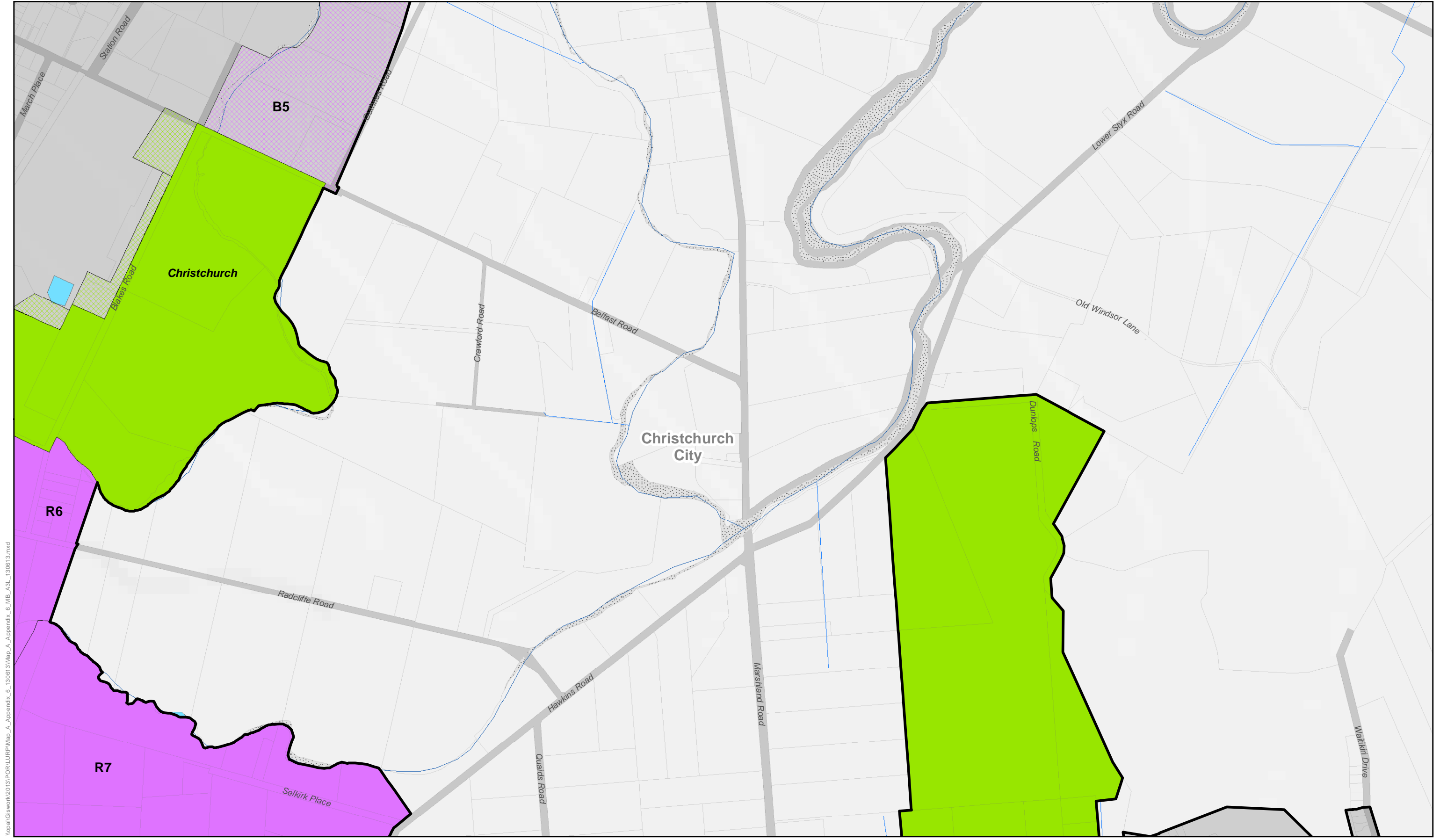
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Environment Canterbury Regional Council  
Kaitiaki Takekōwhiri



MAP

**13**



V:\opall\GISwork\2013\POR\LURP\Map\_A\_Appendix\_6\_130613\Map\_A\_Appendix\_6\_MB\_A3L\_130613.mxd

- ★ Key Activity Centres
- Boundary of infrastructure supported priority, existing urban and greenfield areas.
- Territorial Authority Boundary
- 50dba Airport Noise Contour
- CERA Red Zoned Land
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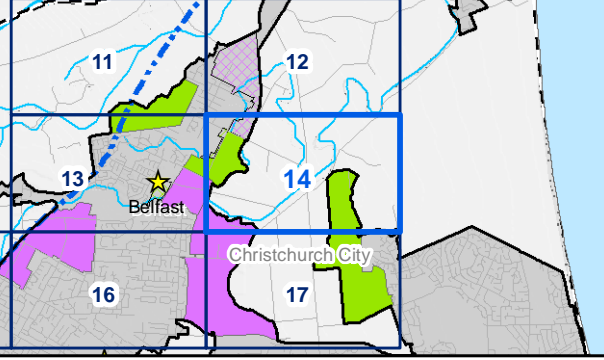
- Priority Areas - Business**
- Zoned
  - Not Yet Zoned
  - B## Business Identifier Number

- Priority Areas - Residential**
- Zoned
  - Not Yet Zoned
  - R## Residential Identifier Number

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  - Business

Greenfield Priority Areas

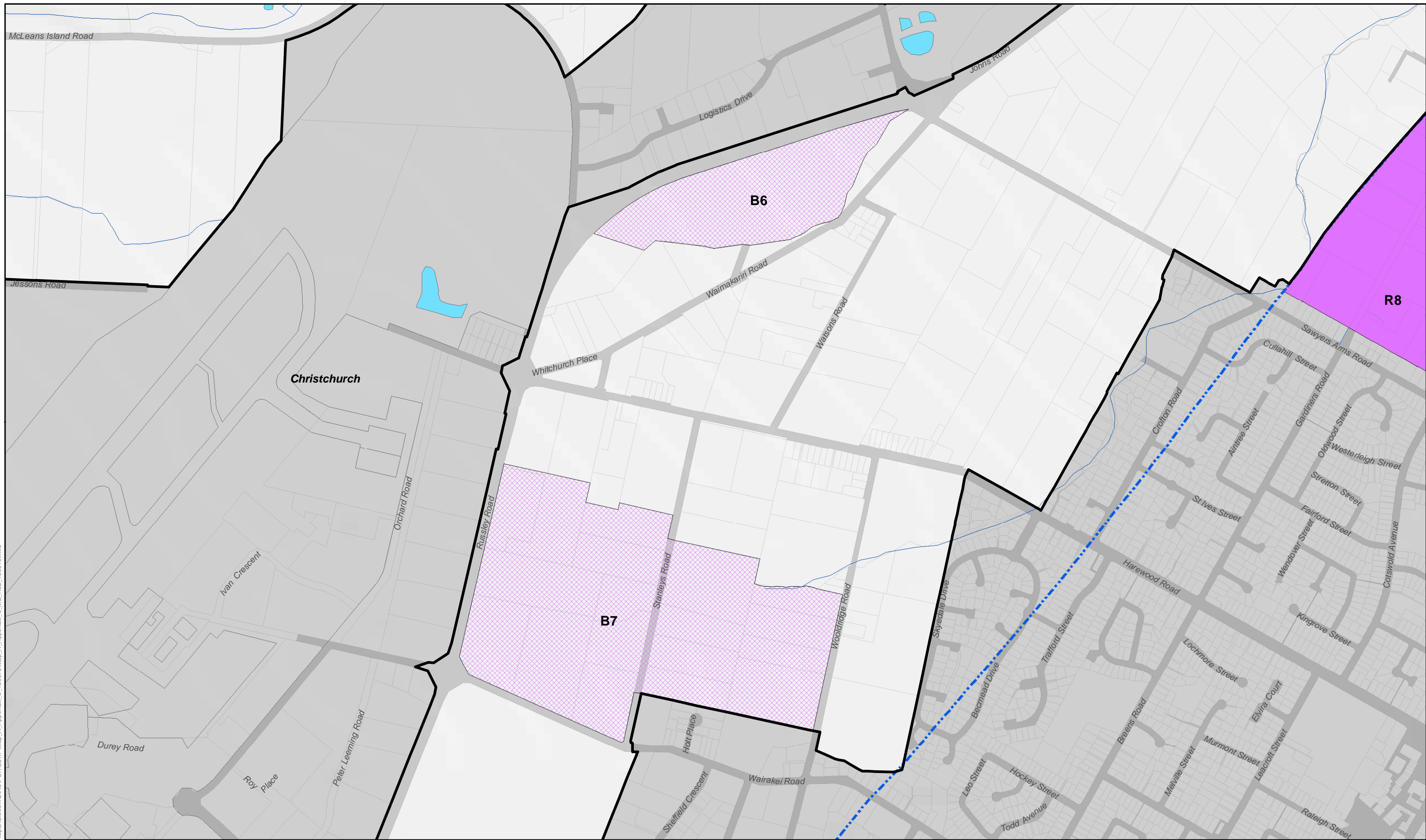
**Environment Canterbury**  
Regional Council  
Kaitiaki Taiao ki Waikato



MAP

**14**





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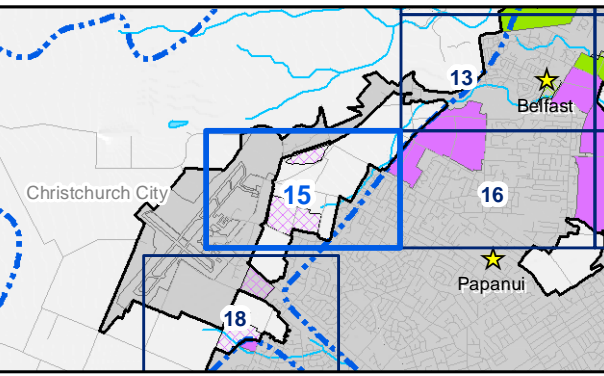
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- ▭ Boundary of infrastructure supported priority, existing urban and greenfield areas.
- ▭ Territorial Authority Boundary
- ⋯ 50dba Airport Noise Contour
- CERA Red Zoned Land
- Christchurch Central Recovery Plan Area
- Existing Urban Area - Pre 2011

- Priority Areas - Business**
- Zoned
  - Not Yet Zoned
- B## Business Identifier Number
- Greenfield Areas - post Land Use Recovery Plan**
- Residential
  - Business

- Priority Areas - Residential**
- Zoned
  - Not Yet Zoned
- R## Residential Identifier Number

Greenfield Priority Areas

**Environment Canterbury**  
Regional Council  
Kaitiaki Taiao ki Waitaha



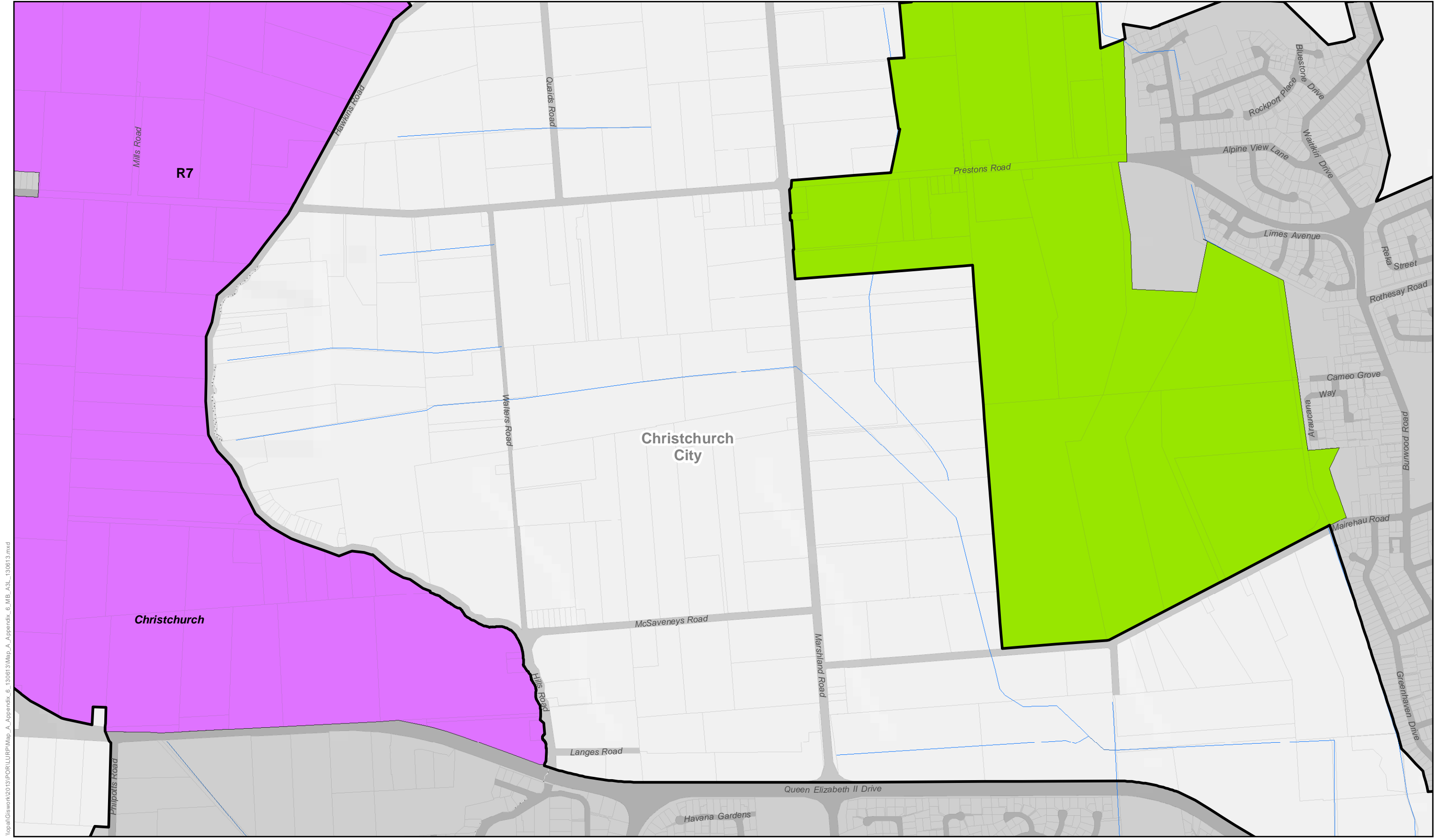
MAP

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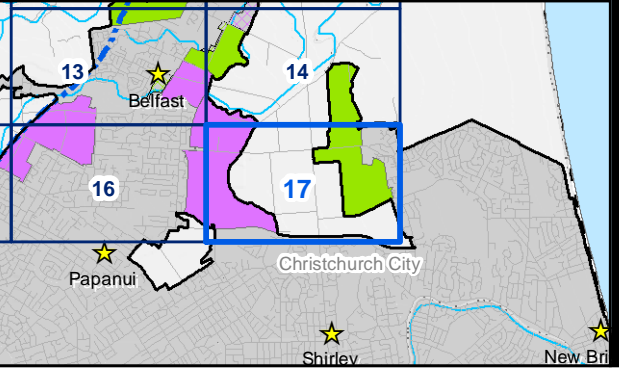
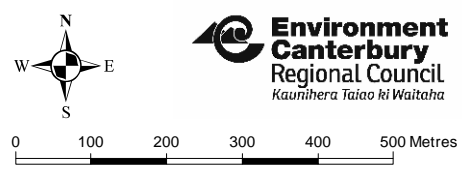
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- CERA Red Zoned Land
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- Existing Urban Area - Pre 2011

- Priority Areas - Business**
- ▨ Zoned
  - ▨ Not Yet Zoned
  - B## Business Identifier Number

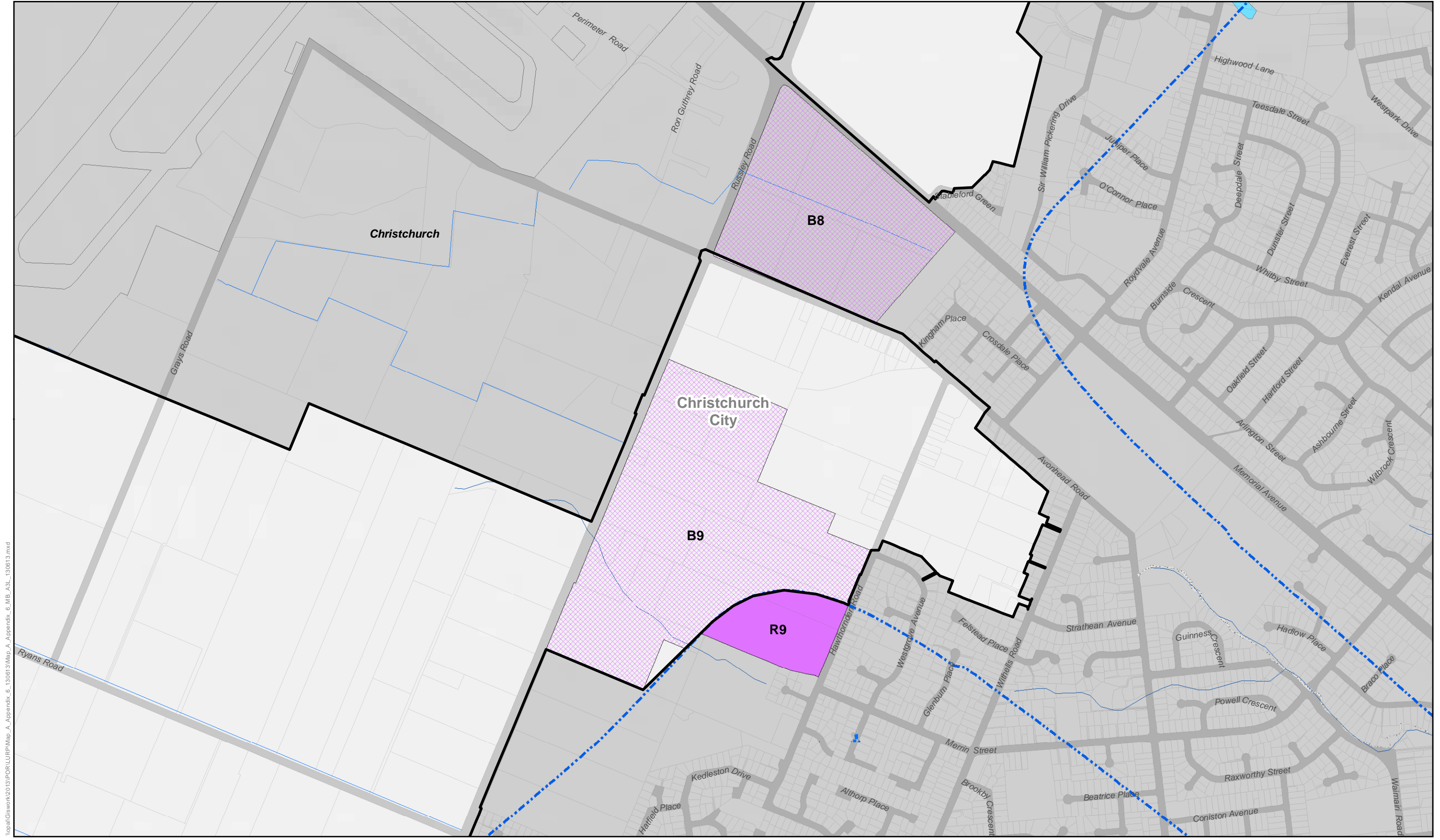
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- ▨ Zoned
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  - R## Residential Identifier Number

- Greenfield Areas - post Land Use Recovery Plan**
- ▨ Residential
  - ▨ Business

Greenfield Priority Areas



MAP  
**17**



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- ★ Key Activity Centres
- Boundary of infrastructure supported priority, existing urban and greenfield areas.
- Territorial Authority Boundary
- 50dba Airport Noise Contour
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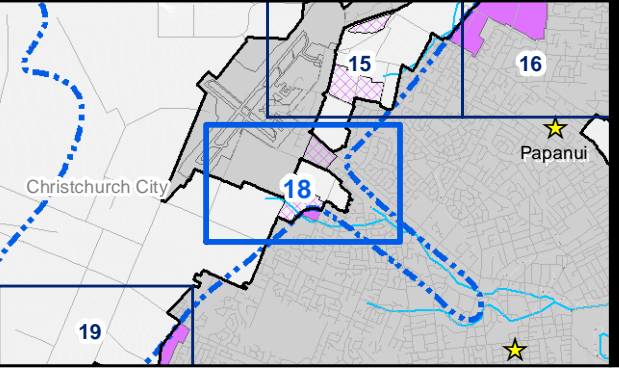
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- Priority Areas - Residential**
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- Greenfield Areas - post Land Use Recovery Plan**
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  - Business

Greenfield Priority Areas

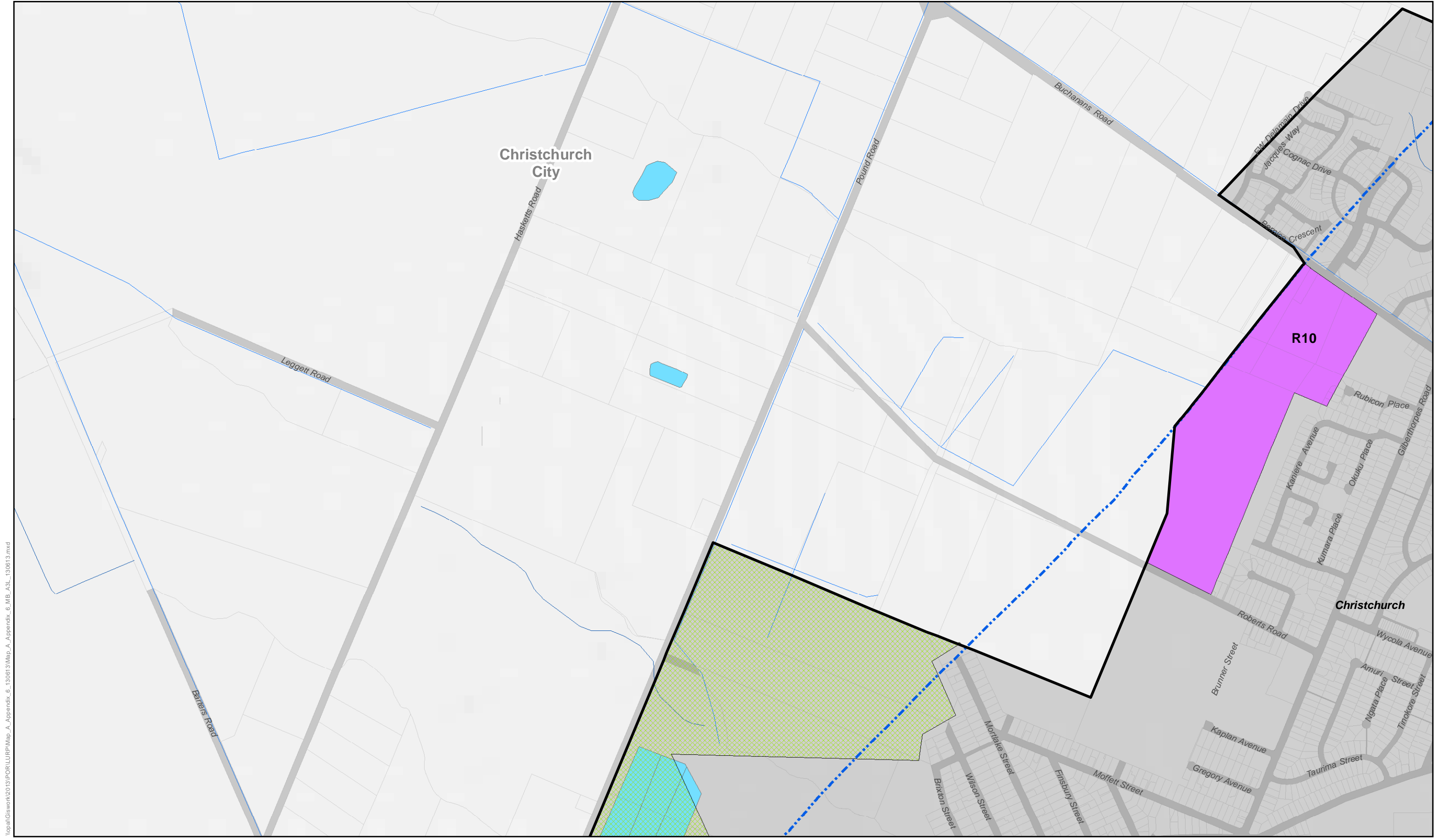
Environment Canterbury Regional Council  
Kaitiaki Taiao ki Waitaha



MAP

**18**





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- ★ Key Activity Centres
- Boundary of infrastructure supported priority, existing urban and greenfield areas.
- Territorial Authority Boundary
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- Existing Urban Area - Pre 2011

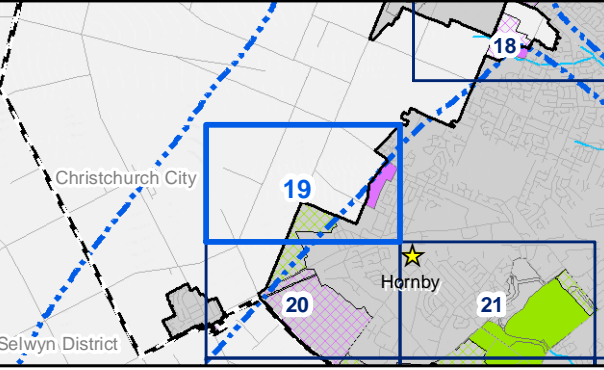
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  - B## Business Identifier Number

- Priority Areas - Residential**
- Zoned
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  - R## Residential Identifier Number

- Greenfield Areas - post Land Use Recovery Plan**
- Residential
  - Business

Greenfield Priority Areas

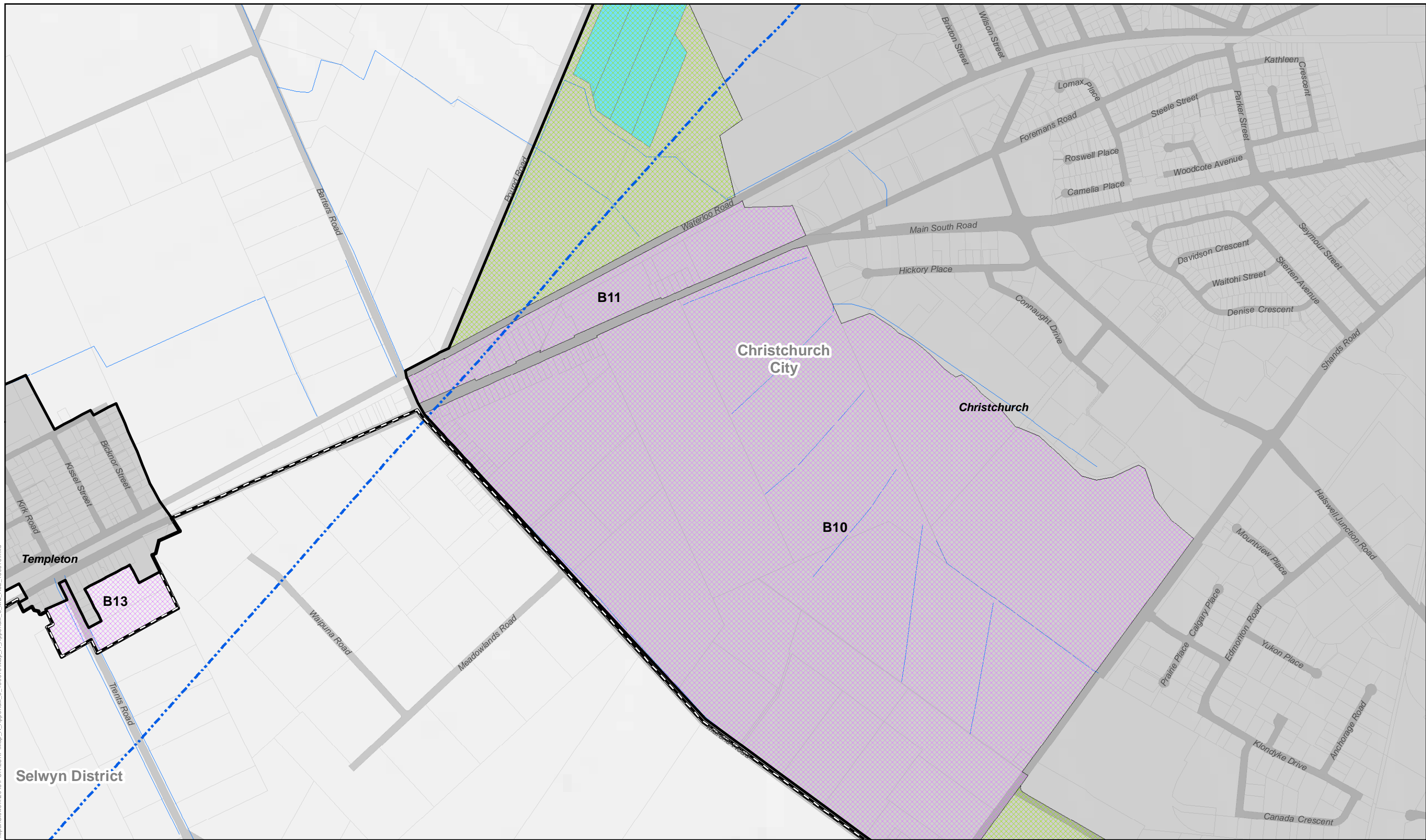
**Environment Canterbury Regional Council**  
Kaitiaki Taiao ki Waitaha



MAP

**19**





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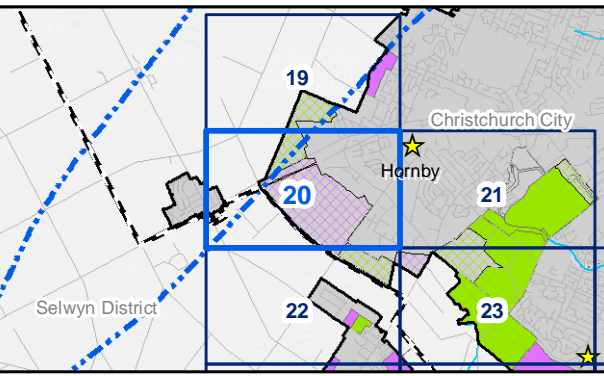
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- Greenfield Areas - post Land Use Recovery Plan**
- Residential
  - Business

- Priority Areas - Residential**
- Zoned
  - Not Yet Zoned
  - R## Residential Identifier Number

Greenfield Priority Areas

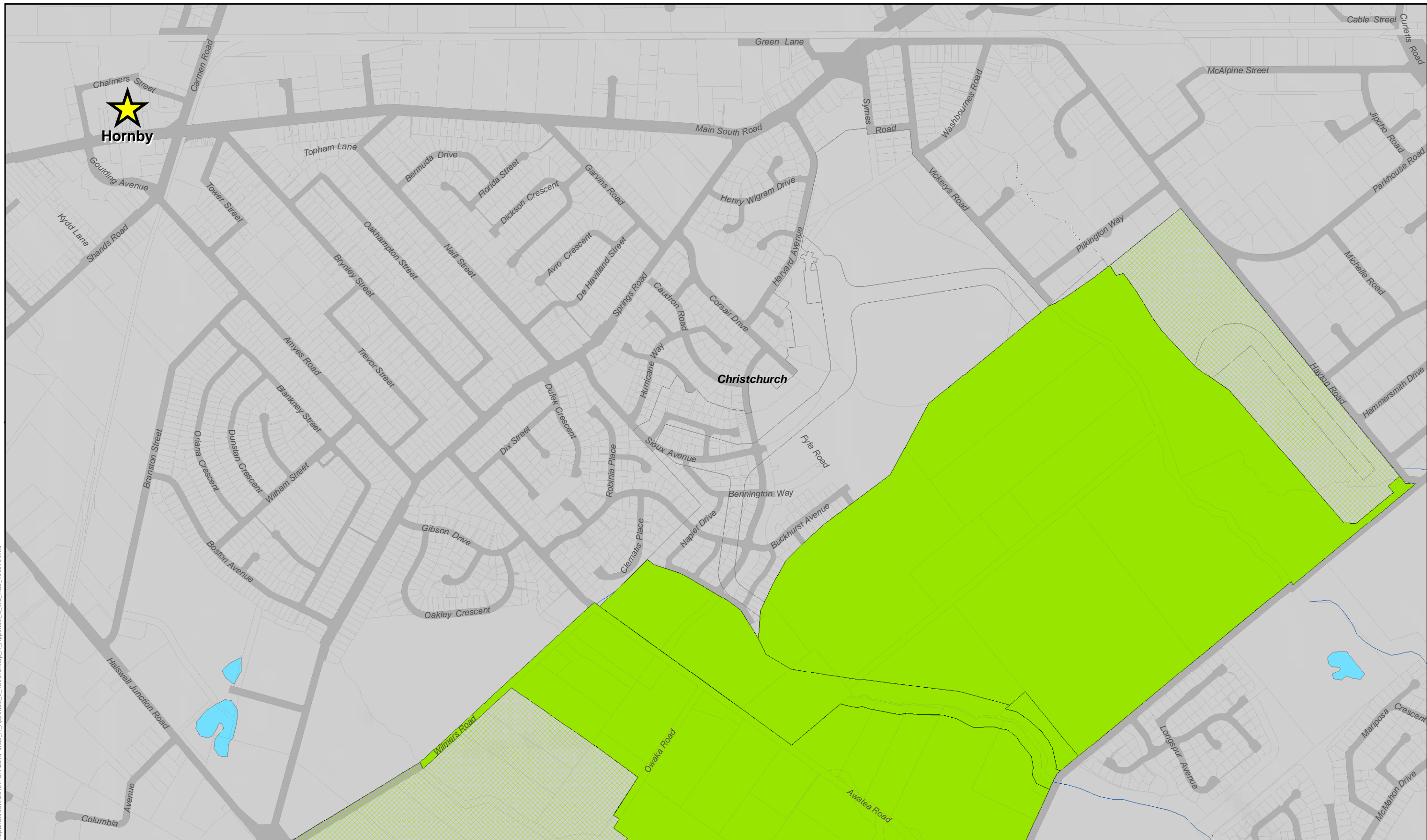
Environment Canterbury Regional Council  
Kaitiaki Taiao ki Waikaha



MAP

**20**

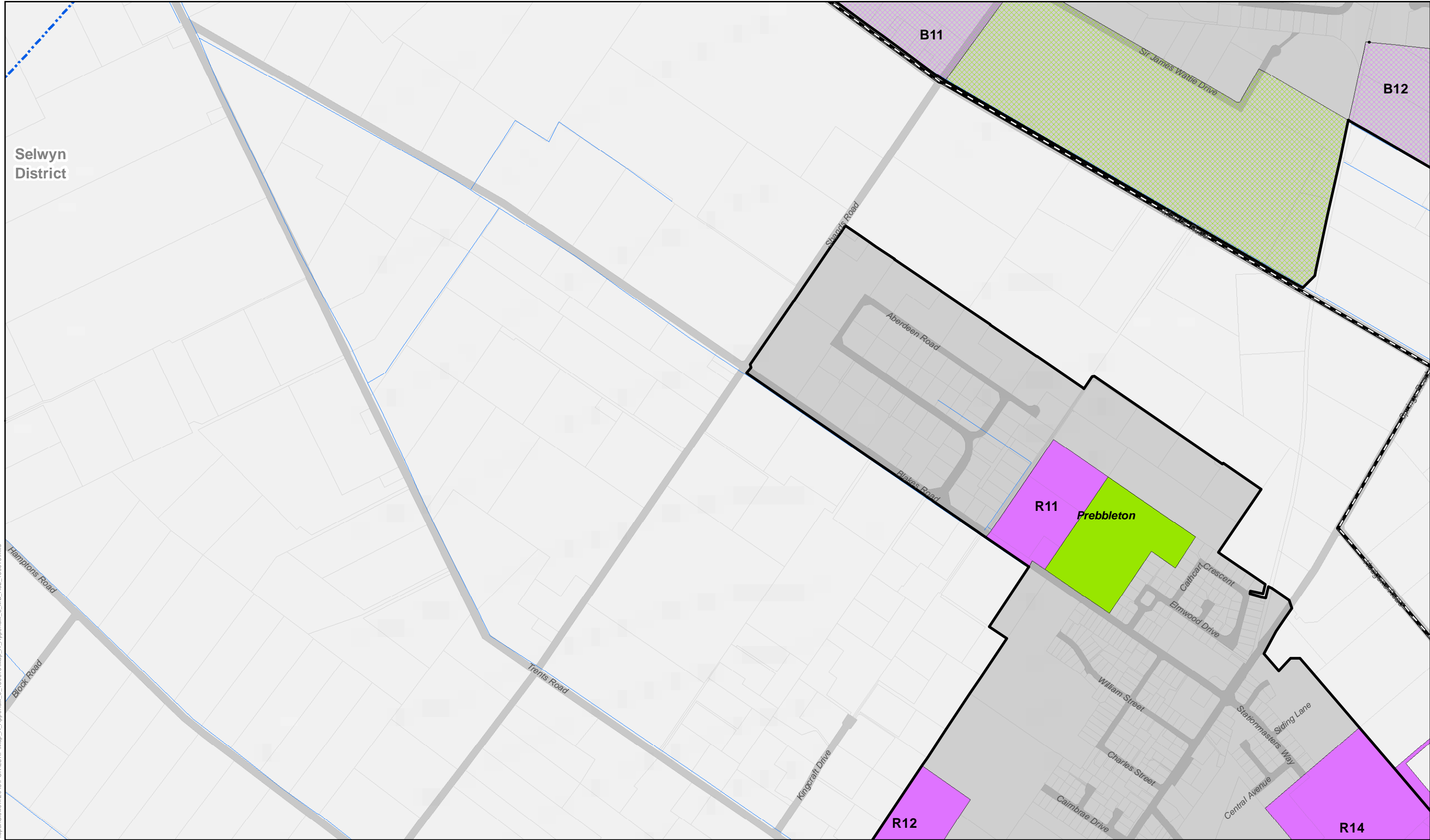




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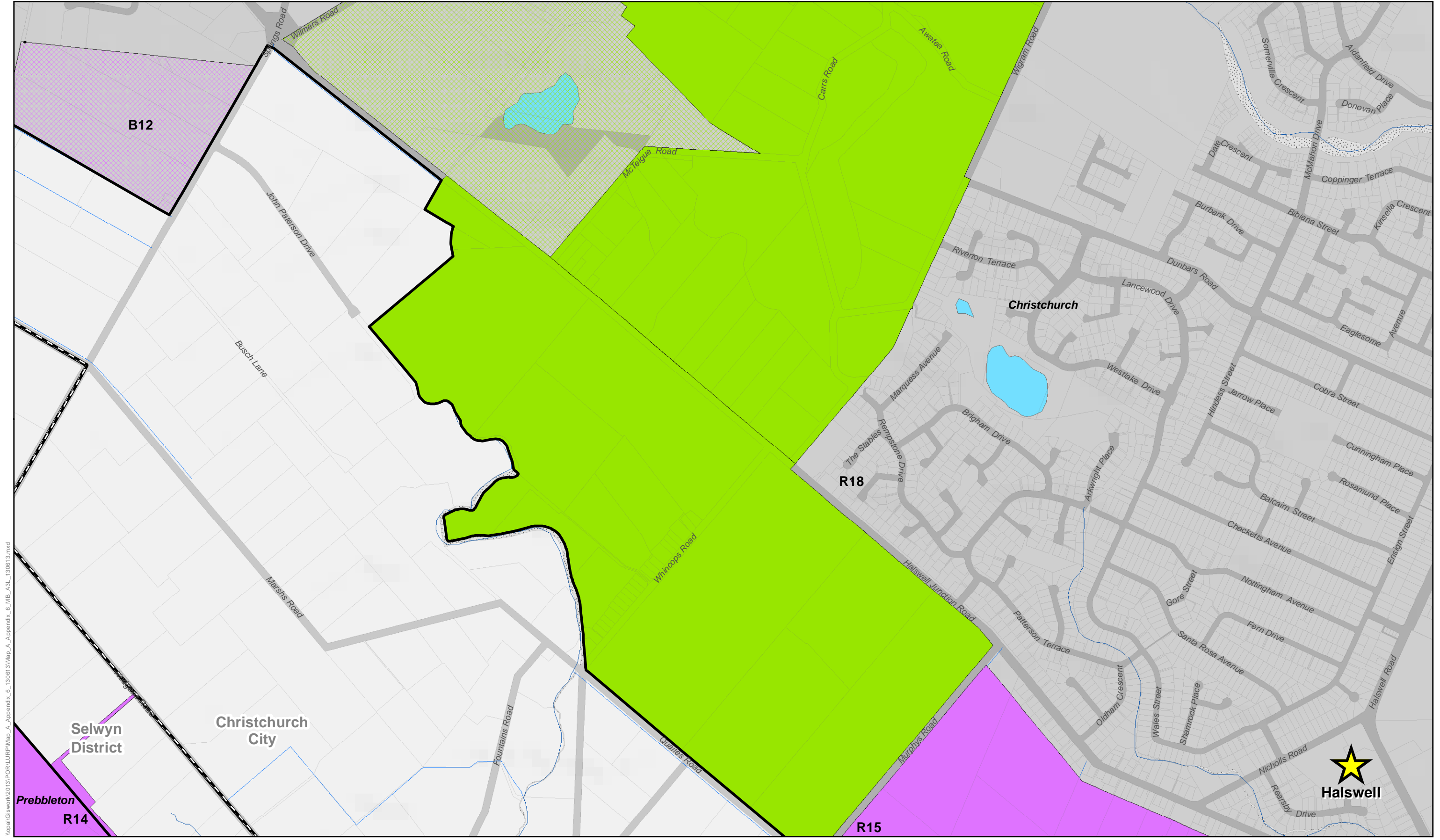




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- ★ Key Activity Centres
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- Priority Areas - Business**
- Zoned
  - Not Yet Zoned
  - B## Business Identifier Number

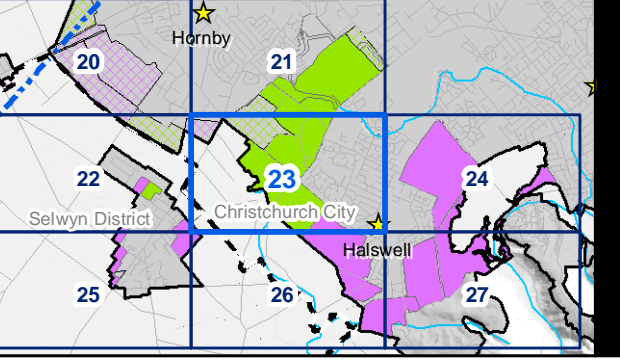
- Priority Areas - Residential**
- Zoned
  - Not Yet Zoned
  - R## Residential Identifier Number

- Greenfield Areas - post Land Use Recovery Plan**
- Residential
  - Business

Greenfield Priority Areas

0 100 200 300 400 500 Metres

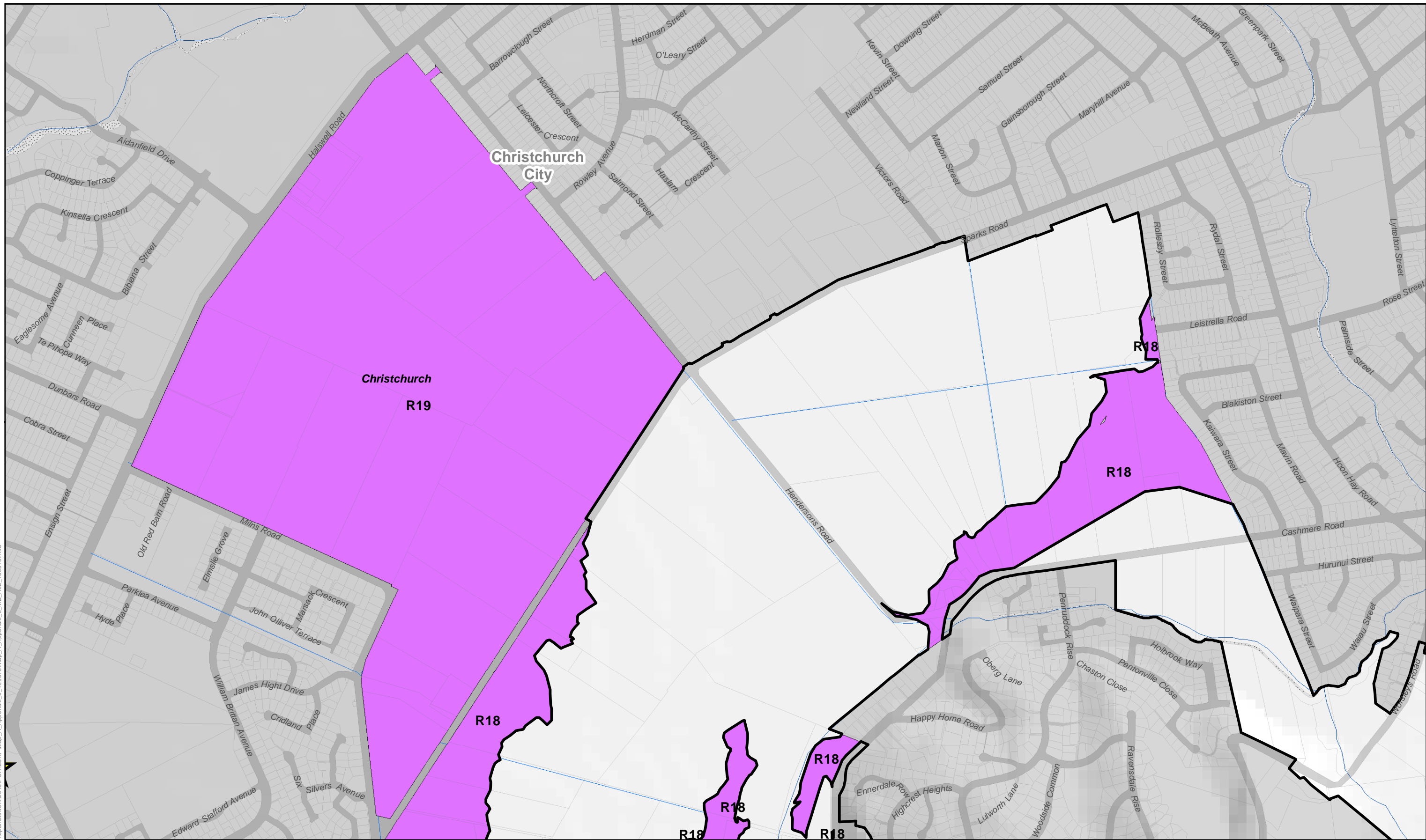
**Environment Canterbury**  
Regional Council  
Kaitiaki Taiao ki Waitaha



MAP

**23**





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- ★ Key Activity Centres
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- CERA Red Zoned Land
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- Existing Urban Area - Pre 2011

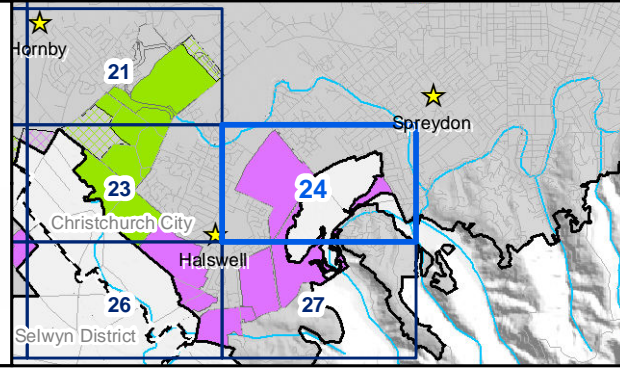
- Priority Areas - Business**
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  - Business

- Priority Areas - Residential**
- Zoned
  - Not Yet Zoned
- R## Residential Identifier Number

Greenfield Priority Areas

0 100 200 300 400 500 Metres

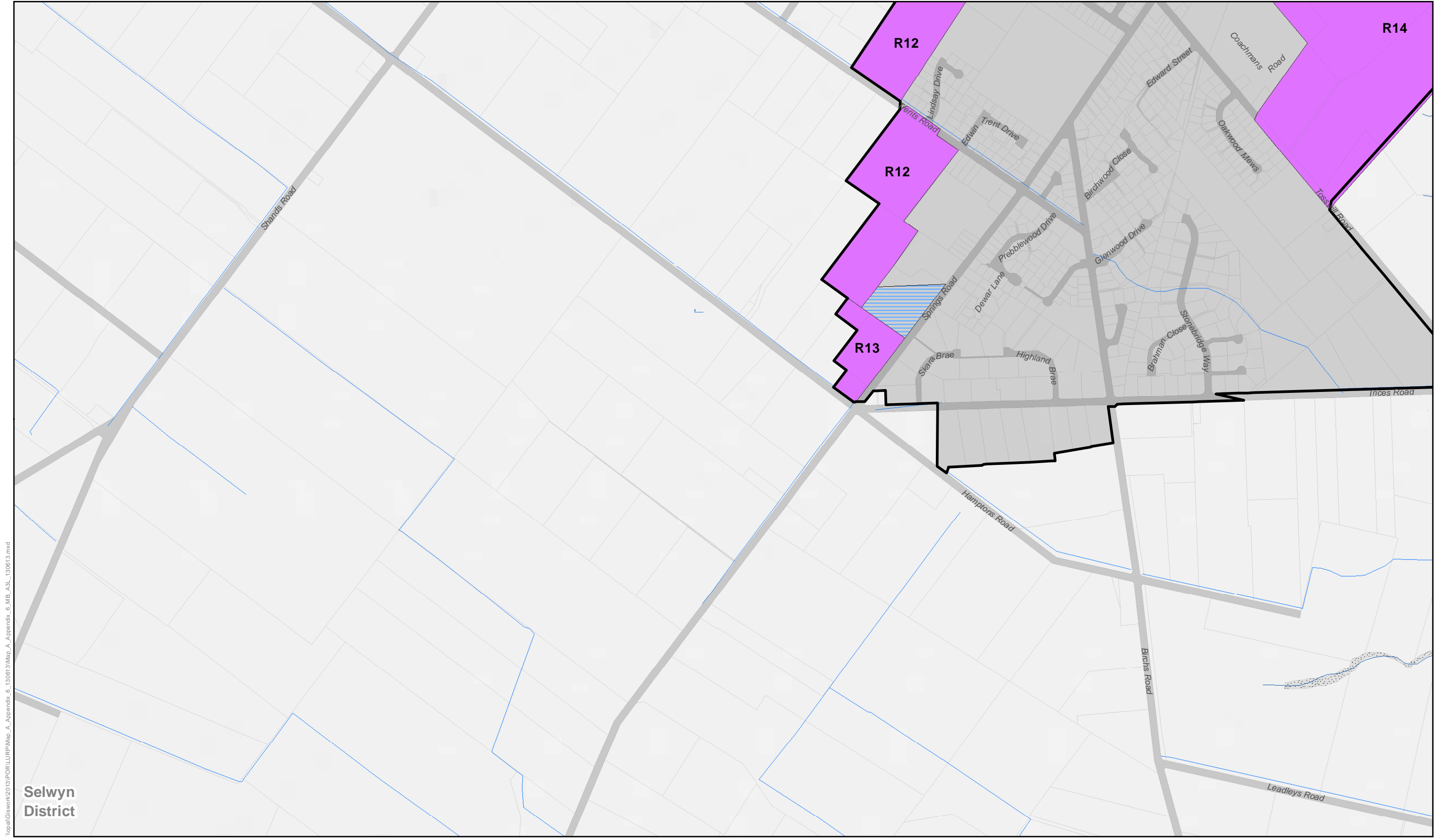
**Environment Canterbury**  
Regional Council  
Kaitiaki Taiao ki Waikato



MAP

**24**





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Selwyn District

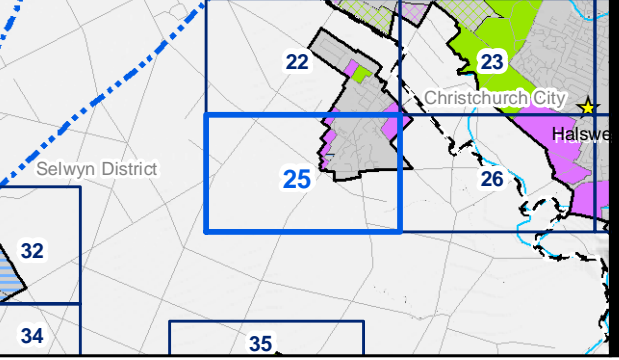
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- Christchurch Central Recovery Plan Area
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  - Business

- Priority Areas - Residential**
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  - Not Yet Zoned
- R## Residential Identifier Number

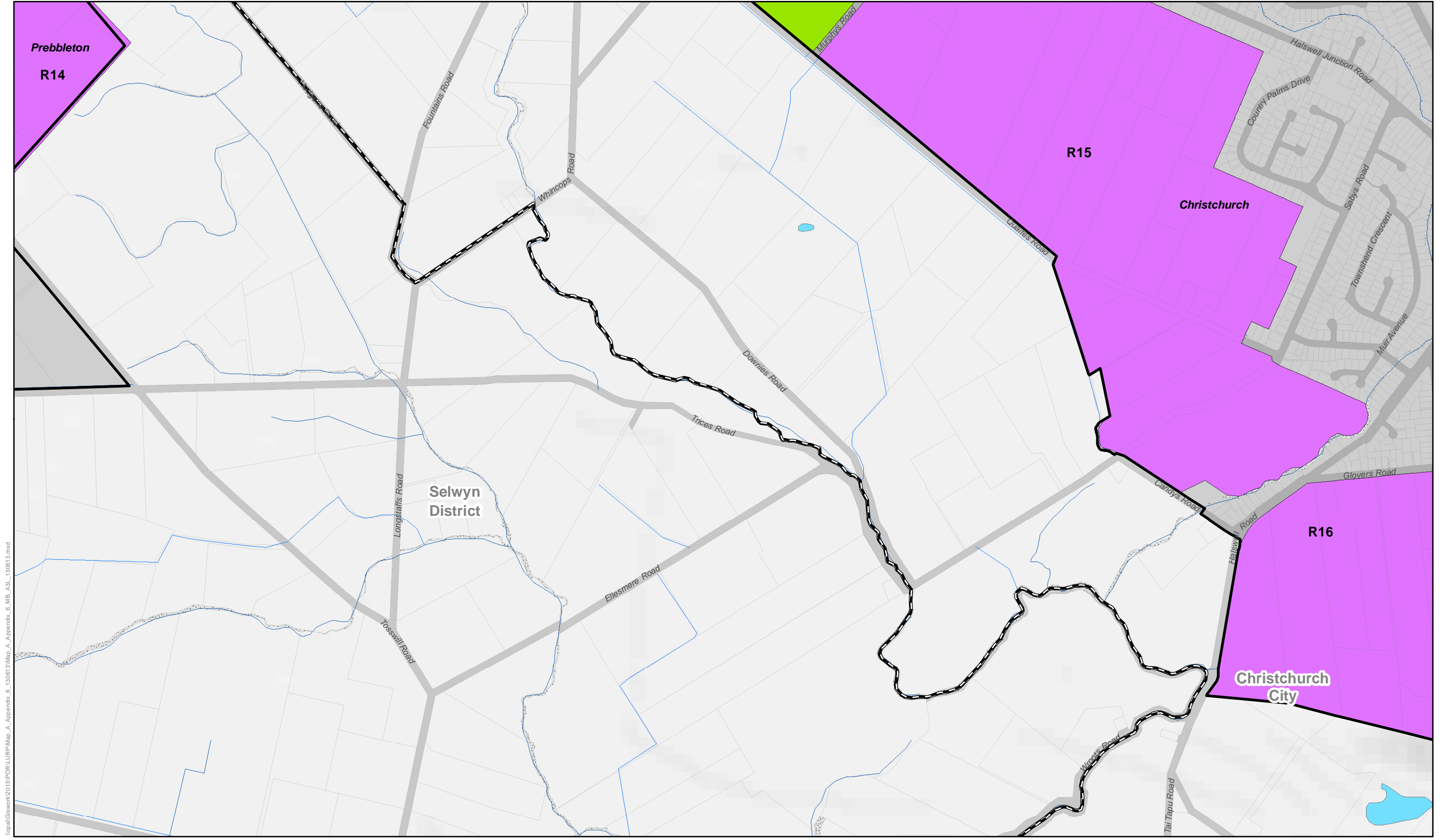
Greenfield Priority Areas

**Environment Canterbury Regional Council**  
Kaitiaki Taiao ki Waitaha



MAP

**25**



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- ★ Key Activity Centres
- ▭ Boundary of infrastructure supported priority, existing urban and greenfield areas.
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- 50dba Airport Noise Contour
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- B## Business Identifier Number

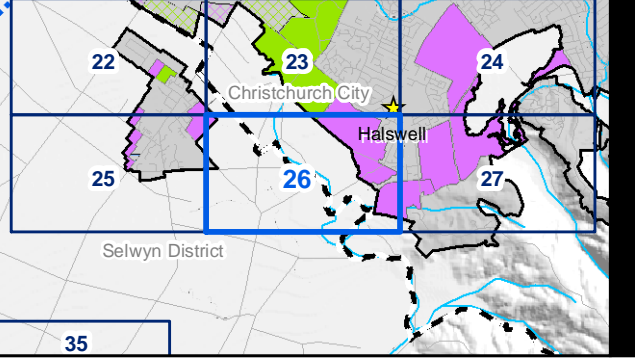
- Priority Areas - Residential**
- Zoned
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- R## Residential Identifier Number

- Greenfield Areas - post Land Use Recovery Plan**
- Residential
  - Business

Greenfield Priority Areas

0 100 200 300 400 500 Metres

**Environment Canterbury**  
Regional Council  
Kaitiaki Taiao ki Waikato

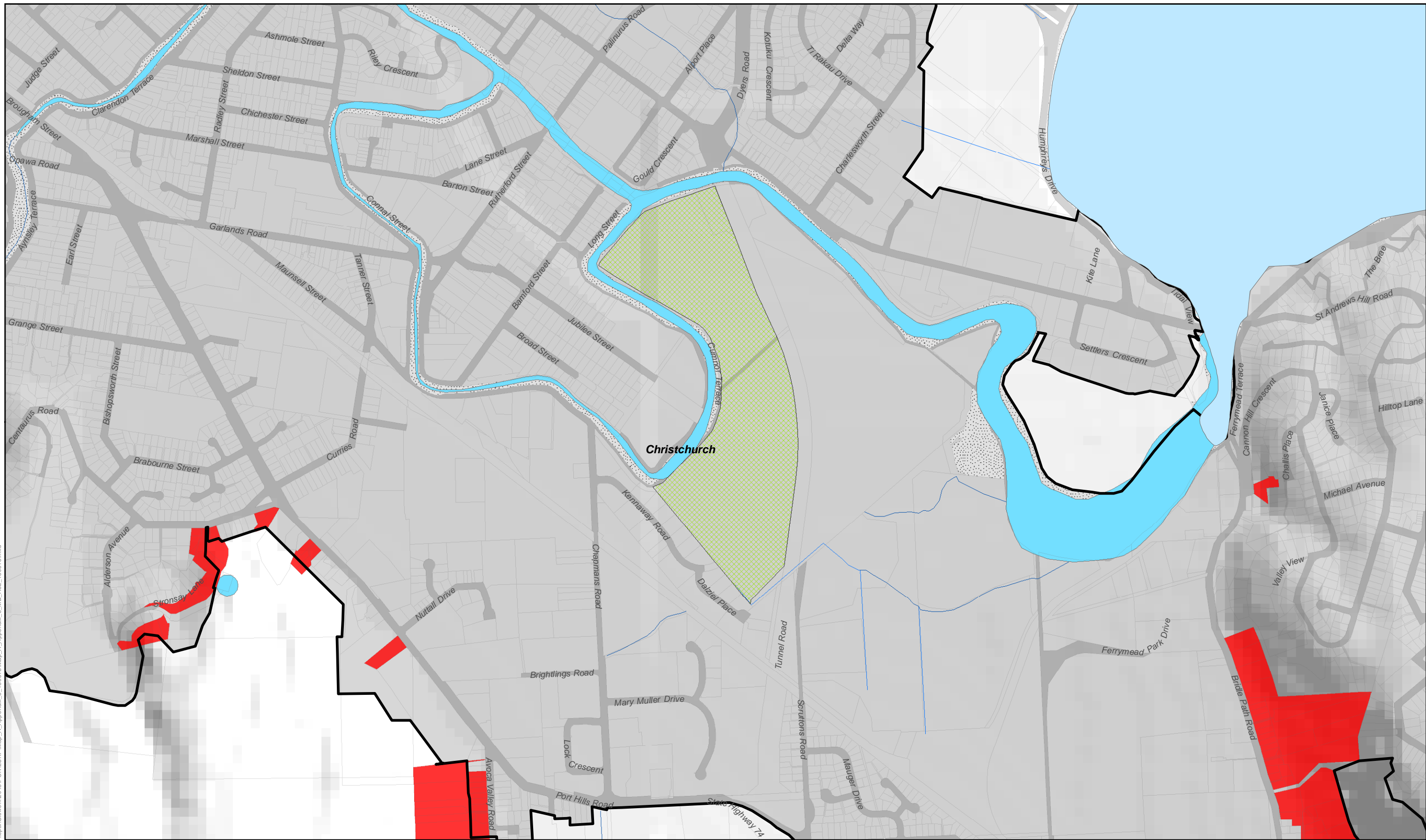


MAP

**26**







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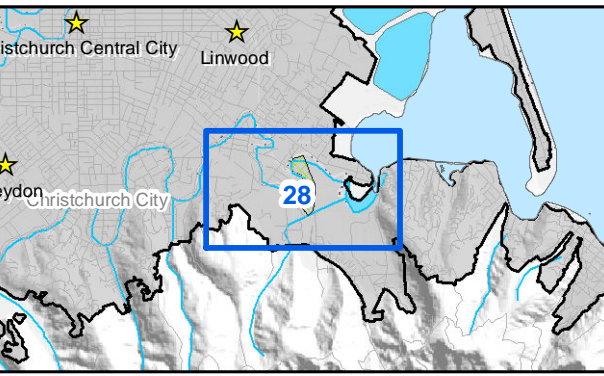
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Greenfield Priority Areas

**Environment Canterbury**  
Regional Council  
Kaitiaki Taiao ki Waikato



MAP

**28**



## **Appendix 4: Amendments to the Waimakariri District Plan**

### **Changes to the Waimakariri District Plan**

The proposed changes to the Waimakariri District Plan set out in this Appendix cover the following:

- Recognising and providing for the relationship of Ngāi Tuahuriri with the land and associated resources in Māori Reserve 873 to enable the land to be used as intended by Kemps Deed of 1848 and the Crown Grants Act (No.2) 1882, for places of residence and living activities for the original grantees and their descendants;
- Re-zoning of land for Rangiora priority areas including:
  - Rezoning approximately 17 hectares of land at west Rangiora to Residential 2;
  - Rezoning approximately 37 hectares of land at south west Rangiora to Residential 2;
  - Inclusion of ODP's and associated policies and methods for each of the priority areas rezoned; and
  - Changing the relevant planning maps to illustrate these areas;
- Amending Business 1 Zone provisions in Kaiapoi, Rangiora and Woodend to avoid adverse effects from erecting dwelling houses on the ground floor; and
- Amendments to other planning maps to identify the priority areas set out in Map A of Chapter 6 of the RPS (Appendix 2 of the Land Use Recovery Plan). These areas identify where rebuilding and development should and should not occur before 2028.

## **Need for recovery purposes**

South West Rangiora  
Oxford Road, West Rangiora

The South West Rangiora and Oxford Road Rangiora priority areas identify and re-zone land for residential development contiguous to Rangiora. The re-zonings provide for additional households to enable redevelopment as part of the earthquake recovery response. Provisions within the District Plan intend to manage the extent and layout of development. Re-zoning of these two priority areas in Rangiora applies existing and new District Plan provisions taking into account previous infrastructure and planning assessment.

Māori Reserve 873

The Canterbury earthquakes and the subsequent damage and red zoning of properties in Waimakariri District and Christchurch City has led to a number of Māori seeking opportunities to return to ancestral lands at Māori Reserve 873. The District Plan provisions recognise the historical and cultural associations of Māori with the land in Māori Reserve 873. The Council has resolved to recognise ongoing rights which stem from Kemps Deed of 1848 and the Crown Grants Act (No.2) of 1862. Provisions in the District Plan and its ongoing administration are intended to support applications and development proposals for the land in Māori Reserve 873 to be used for places of residence and living activities for descendants of the original grantees.

Dwellinghouses in the Business 1 Zone

The Business 1 Zones are located within the centre of the District's main towns and provide the dominant focal point for the business sector for the towns and their surrounding areas including the Rural Zones. Due to extensive damage from consequences of the Canterbury earthquakes to the Kaiapoi and Rangiora Business 1 Zones areas need to be demolished and rebuilt. Given the need for rebuilding and that parts of the zones are occupied by existing dwellinghouses it is envisaged that as the zone redevelops it will intensify over time for business activities and these dwellinghouses will be replaced by commercial buildings. Residential activities can offer vibrancy to the function of the zones, particularly where co-located with business activities that are designed and developed comprehensively but should not become an inhibitor to redevelopment and business activity. This is considered a likelihood if new dwellinghouses establish on ground floor business space.

Note: For the purposes of these plan changes, any text proposed to be added by these plan changes is shown as **bold underlined** and text to be deleted as ~~**bold strikethrough**~~.

## **Rezone South West Rangiora**

### **Chapter 27. Natural Hazards**

Add new Rule 27.1.1.24 to read as follows:

**27.1.1.24** **Within the South West Rangiora Outline Development Plan area shown on District Plan Map 173 any dwellinghouse shall have a minimum floor level of 400mm above the 0.5% Annual Exceedence Probability flood event.**

**Cross Reference: Rule 32.1.1.76**

### **Chapter 30. Utilities and Traffic Management – Rules**

Delete Table 30.1: Road Design Attributes by Zone and insert new Table 30.1: Road Design Attributes by Zone.

	<u>Strategic</u>		<u>Arterial</u>		<u>Collector/Urban Collector</u>		<u>Local</u>		<u>Cul-de-sac</u>	
	<u>Rural</u>	<u>Res &amp; Bus</u>	<u>Rural</u>	<u>Res &amp; Bus</u>	<u>Rural</u>	<u>Res &amp; Bus</u>	<u>Rural</u>	<u>Res &amp; Bus</u>	<u>Rural</u>	<u>Res &amp; Bus</u>
<u>Min. no. of footpaths</u>		<u>2</u>		<u>2</u>		<u>2</u>		<u>Res &amp; Bus 2 = 1</u>  <u>(See Note)</u>		<u>1</u>
								<u>Bus 1 &amp; adjoining roads = 2</u>		
								<u>Bus 3 = 0</u>		



	<u>Strategic</u>		<u>Arterial</u>		<u>Collector/Urban Collector</u>		<u>Local</u>		<u>Cul-de-sac</u>	
	<u>Rural</u>	<u>Res &amp; Bus</u>	<u>Rural</u>	<u>Res &amp; Bus</u>	<u>Rural</u>	<u>Res &amp; Bus</u>	<u>Rural</u>	<u>Res &amp; Bus</u>	<u>Rural</u>	<u>Res &amp; Bus</u>
<u>Street lighting</u>		<u>AS/NZ S 1158 V3 lighting g</u>	<u>No</u>	<u>AS/NZS 1158 V4 lighting</u>	<u>No</u>	<u>AS/NZS 1158 V4 lighting</u>	<u>No</u>	<u>NZS 6701 lighting</u>	<u>No</u>	<u>NZS 6701 lighting g</u>

**Note: Except for the South West Rangiora Residential 2 Zone Outline Development Plan area shown on District Plan Map 173 where two footpaths shall be provided.**

**Add** new Exemption Rule 30.6.2.6 to read as follows:

**30.6.2.6 The intersections of roads within the South West Rangiora Outline Development Plan area shown on District Plan Map 173 are exempt from complying with Rule 30.6.1.24.**

### **Chapter 31 Health, Safety and Wellbeing - Rules**

**Add** new Rule 31.1.1.38 to read as follows:

**31.1.1.38 Within the South West Rangiora Residential 2 Zone shown on District Plan Map 173 all fencing:**

- a. where located between a residential property and reserve land shall have a maximum height of 1.8m and have a minimum visual permeability/openness of 45%; and**
- b. where located on and between the road boundary and any part of a dwellinghouse shall have a maximum height of 1.2m.**

**Add** new exemption Rule 31.17.2.2 to read as follows:

**31.17.2.2 Any dwellinghouse erected within the South West Rangiora Residential 2 Zone Outline Development Plan area shown on District Plan Map 173 is exempt from complying with Rule 31.17.1.10.**

### **Chapter 32. Subdivision – Rules**

**Insert** new clause (aa) in Rule 32.1.1.21 to read as follows:

**aa. South West Rangiora identified on District Plan Map 173.**

Delete Rule 32.1.1.71 and add new Rule 32.1.1.71 to read as follows:

**32.1.1.71 Accidental discovery protocol within the East Woodend, Waikuku Beach, West Kaiapoi, the North Woodend and South West Rangiora Outline Development Plan Areas as shown on District Plan Maps 153, 158, 161, 164 and 173:**

- a Within the North Woodend Outline Development Plan area a suitably skilled and experienced cultural monitor or monitors nominated by the Runanga and agreed by the landowner shall be appointed to monitor all bulk earthworks on site.**
- b Where suspected urupa, wahi tapu, wahi taonga or any place associated with Human activity that occurred before 1900 and is, or may be able, through investigation by archeological methods to provide evidence relating to the Māori history of the area is discovered:**
- earthworks shall stop immediately;**
  - all machinery shall be shut down immediately;**
  - all persons shall leave the area;**
  - the land owner shall be advised of the discovery;**
  - the NZ Historic Places Trust shall be advised of the discovery and either the Regional Archaeologist of the NZ Historic Places Trust has confirmed in writing that the archaeologist provisions of the Historic Places Act do not apply; or the requirements of the Historic Places Act have been met and, if required, an archaeological authority (consent) has been granted by the NZ Historic Places Trust;**
  - Where the place is associated with Māori activity that occurred before 1900 the land owner shall consult Te Ngāi Tūāhuriri Runanga to determine in consultation with the NZ Historic Places Trust, what further actions are appropriate to safeguard the site or its contents, and to avoid, reduce, remedy or mitigate any damage to the site; and**

- the Waimakariri District Council shall be notified.

**c. Where koiwi tangata (human remains) are suspected:**

- steps shall be taken immediately to secure the area in a way that ensures the koiwi tangata are untouched;
- Te Ngāi Tūāhuriri Runanga, the Police, the Waimakariri District Council and the New Zealand Historic Places Trust shall be notified;
- the land owner shall arrange for his/her representatives to be available to meet and guide kaumatua, Police, Waimakariri District Council and New Zealand Historic Places Trust staff to the site, assisting with any requests that they may make;
- earthworks in the affected area shall remain halted until the kaumatua, the Police and Historic Places Trust staff have marked off the area around the site and have given approval for earthworks to recommence, including notification that the archeological provisions of the Historic Places Act have been met and, if required, an archaeological authority (consent) has been granted by the NZ Historic Places Trust; and
- where the kaumatua are satisfied that the koiwi tangata are of Māori origin the kaumatua will decide in consultation with the NZ Historic Places Trust, what happens to the koiwi tangata and will give their decision to the Police, the New Zealand Historic Places Trust and the land owner.

Add new Rule 32.1.1.76 to read as follows:

**32.1.1.76 Within the South West Rangiora Outline Development Plan area shown on District Plan Map 173 all residential allotments shall have a finished ground level that avoids inundation in a 0.5% Annual Exceedence Probability flood event.**

**Cross Reference: Rule 27.1.1.24**

Add new Rule 32.4.6 to read as follows:

**32.4.6 Any subdivision of land within the South West Rangiora Outline Development Plan area shown on District Plan Map 173 that does not comply with Rule 32.1.1.76 is a non-complying activity.**

#### DISTRICT PLAN MAPS

**Delete** District Plan Maps 46 and 116 and **add** new District Plan Maps 46,116 and 173 to rezone 11, 29, 39, 51 and 59 Pentecost Road; 131 and 137 Johns Road; and parts of 91 and 113 Townsend Road, Rangiora being Lot 1 DP 27779, PT RS 569, PT RS 474, Lot 1 DP 80253, Lot 2 80253, Lot 2 DP 434889 and Lot 1 DP 434889 to Residential 2.

Any consequential renumbering or amendments throughout the District Plan as necessary.

## **Rezone Oxford Road, West Rangiora**

### **Chapter 17. Residential Zone**

Add new Policy 17.1.1.4 to read as follows:

#### **Policy 17.1.1.4**

**Ensure that subdivision and development within the Oxford Road, West Rangiora Outline Development Plan area achieves a minimum net density of 10 households per hectare averaged over the entire Outline Development Plan area.**

### **Chapter 27. Natural Hazards**

Add new Rule 27.1.1.23 to read as follows:

**27.1.1.23 Within the Oxford Road, West Rangiora Outline Development Plan area shown on District Plan Map 168 any dwellinghouse shall have a minimum floor level of 350mm above the 0.5% Annual Exceedence Probability flood event.**

**Cross Reference: Rule 32.1.1.75.**

### **Chapter 31. Health, Safety & Wellbeing**

Add new Rule 31.1.1.9 to read as follows:

**31.1.1.9 Within the Oxford Road, West Rangiora Outline Development Plan area shown on District Plan Map 168, any dwellinghouse on a site greater than 1,200m<sup>2</sup> in area shall be contained within its own delineated area, where that delineated area:**

- a. complies with the area and dimensions set out in Table 32.1 (Subdivision – Rules) as though the site was an allotment; and**

**b. is of an area and dimension, and located in such a position, that does not frustrate compliance with Rule 32.1.1.10.**

**Delete** Rule 31.1.1.37 and **add** new Rule 31.1.1.37 to read as follows:

**31.1.1.36 Within the Enverton Drive – Ballarat Road and Enverton Drive – Ashley Street, North Rangiora, and Oxford Road, West Rangiora Outline Development Plans shown on District Plan Maps 165, 166 and 168, fencing within 2 metres of the boundary of the walkway links shall have a maximum height of 1 metre.**

**Add** new Rule 31.5.4 to read as follows:

**31.5.4 Any land use which does not comply with Rule 31.1.1.9 (location of dwellinghouses within the Oxford Road, West Rangiora Outline Development Plan area) is a non-complying activity.**

## **Chapter 32. Subdivision**

**Delete** Table 32.1 and **Add** new Rule Table 32.1, to read as follows:

<u>Zone</u>	<u>Minimum Allotment Area</u>	<u>Minimum Dimensions of Allotment (m)</u>	
		<u>Internal Square</u>	<u>Frontage</u>
<b><u>Business 2</u></b>	<b><u>700m<sup>2</sup></u></b>		
<b><u>Residential 1</u></b>	<b><u>300m<sup>2</sup></u></b>	<b><u>15 x 15</u></b>	<b><u>15</u></b>
<b><u>Residential 2</u></b>	<b><u>600m<sup>2</sup></u></b> <small><u>NOTE: See Rules 32.1.1.4, 32.1.1.8 and 32.1.1.10</u></small>	<b><u>18 x 18</u></b>	<b><u>15</u></b>
<b><u>Residential 3</u></b>	<b><u>600m<sup>2</sup></u></b> <small><u>NOTE: See Rules 32.1.1.6, 32.1.1.7</u></small>	<b><u>15 x 15</u></b>	<b><u>15</u></b>
<b><u>Residential 6</u></b>	<b><u>400m<sup>2</sup></u></b>	<b><u>13 x 18</u></b>	<b><u>13</u></b>

Zone	Minimum Allotment Area	Minimum Dimensions of Allotment (m)	
		Internal Square	Frontage
<b><u>Residential 6A</u></b>	<b><u>137.5m<sup>2</sup></u></b> <b><u>Maximum lot area on subdivision is 412.5m<sup>2</sup>, other than where a balance area is created on subdivision which exceeds 4ha</u></b>	<b><u>Except for corner sites, the length of a road boundary of the lot shall be less than the depth of the lot, measured at right angles to that road boundary</u></b>	
<b><u>Residential 4A/4B</u></b>	<b><u>NOTE: See Rules 32.1.1.10 to 32.1.1.18</u></b>		
<b><u>Residential 5</u></b>	<b><u>Shall generally comply with the Concept Plan shown in District Plan Map 140</u></b>		
<b><u>Residential 7</u></b>	<b><u>Area A 150m<sup>2</sup></u></b> <b><u>Area B 300m<sup>2</sup></u></b> <b><u>Area C 500m<sup>2</sup></u></b> <b><u>NOTE: See Rules 32.1.1.19 and 32.1.1.20</u></b>	<b><u>6 x 8m</u></b> <b><u>15 x 15 m</u></b> <b><u>15m x 15m</u></b>	<b><u>15m</u></b> <b><u>15m</u></b>
<b><u>Rural</u></b>	<b><u>4ha</u></b> <b><u>NOTE: See Rule 32.1.1.5</u></b>	<b><u>120 x 120m</u></b>	<b><u>Allotments adjoining State Highways: minimum of 200m or no greater than 10m frontage to a formed road, provided that no more than two frontages of 10m shall be provided without a separation of 200m</u></b> <b><u>NOTE: See Rule 30.6.1.13</u></b>
<b><u>Mapleham Rural 4B Zone</u></b>	<b><u>Shall generally comply with the Concept Plan shown on District Plan Map 147</u></b>		

**CROSS REFERENCE: Rules 32.1.1.4, 32.1.1.5, 32.1.1.8 to 32.1.1.20**

**Add new Rule 32.1.1.10 to read as follows:**

**32.1.1.10 Within the Oxford Road, West Rangiora Outline Development Plan area shown on District Plan Map 168 subdivision shall achieve a minimum net density of 10 allotments per hectare once the entire Outline Development**

**Plan area has been developed and achievement of this shall be demonstrated for each stage of subdivision.**

Delete Rule 32.1.3 and add new Rule 32.1.3 to read as follows:

**In considering any application for a subdivision consent under Rules 32.1.1.1 to 32.1.1.74 the Council shall, in granting consent, and in deciding whether to impose conditions, exercise its control over the following matters:**

**i. Financial Contributions:**

- **financial contributions as set out in Chapter 20: Financial Contributions and Chapter 34: Financial Contributions – Rules and development contributions as set out in Waimakariri District Council’s Development Contributions Policy.**

**ii. Allotment Area and Dimensions:**

- **allotment area and dimensions of subdivisions in the Business 1 Zone;**
- **area and dimensions of allotments for access, utilities, reserves and roads; and**
- **area and dimensions of allotments created for the purposes of conservation, restoration or enhancement of any vegetation and habitat site, notable plant or heritage resource listed in the District Plan, and any other area of significant indigenous vegetation or significant habitat of indigenous fauna, or any other heritage item, or wahi taonga.**
- **in the case of subdivision within the Oxford Road, West Rangiora Outline Development Plan area shown on District Plan Map 168, the need to ensure any balance land is able to achieve a minimum net density of 10 allotments per hectare once the entire area has been developed.**

**iii. Subdivision Design:**

- **the effects on the characteristics of the zone set out in Objective 14.1.1 and Policies 12.1.1.3, 12.1.2.1, 12.1.2.2, 12.1.2.3, 15.1.1.1, 16.1.1.1, 16.1.1.2, 16.1.1.3, 16.1.1.6, 17.1.1.1, 17.1.1.2 and 17.1.1.3;**
- **standard of construction of roads, service lanes, and accessways in terms of Tables 30.1 to 30.4 and 32.2;**
- **compliance with any relevant Outline Development Plan or concept plan;**
- **relationship and orientation of allotments;**



- the provision and location of walkways and cycleways and their separation from the road by visual and/or physical means;
- the provision and/or use of open stormwater channels, wetlands and water bodies, excluding aquifers and pipes;
- the provision, location and design of reserves;
- protection of areas of significant indigenous vegetation or significant habitats of indigenous fauna, other significant plants, heritage items, or wahi taonga;
- in the case of subdivision in the Residential 6, 6A and B business 1 Zones at Pegasus or Pegasus Rural Zone, the need to ensure consistency with Policies 2.1.3.5, 3.5.1.1, 6.1.1.8, 8.2.1.7, 11.1.1.8, 14.4.1.1 and 18.1.1.11 for Pegasus; and
- in the case of areas which are associated with Outline Development Plans, the protection of future roads and other public features of the subdivision from being built on.

**iv. Property Access:**

- the location, design, alignment and pattern of roads in relation to lots in terms of Rules 30.1.1.9, 30.6.1.1 and 30.6.1.25;
- the location, design and provision of access in terms of Rules 30.6.1.1 to 30.6.1.12;
- the location, design and provision of vehicle crossings in terms of Rules 30.6.1.13 to 30.6.1.23;
- the provision and location and design of cycleways to make them safe, including separation from roads by visual and/or physical means;
- road reserves and provision for future subdivision on adjoining land;
- the standard of construction required for vehicle crossings and accessways;
- standard for street lighting;
- naming of private vehicular access, roads, accessways and reserves; and
- standards for signs.

**v. Esplanade Provision:**

- in the case of the subdivision of land adjoining the Coastal Marine Area and rivers specified in Rule 33.1.1, and any other river under Rule 33.1.2, whether an

esplanade reserve or strip should be provided and the matters to be included in the esplanade strip instrument.

**vi. Hazards:**

— provision of works, the location and type of services, location of structures and floor levels, and location and quantity of filling and earthworks that can be affected by the following hazards or which could affect the impact of those hazards on any allotment or other land in the vicinity:

**a. erosion;**

**b. flooding and inundation;**

**c. landslip;**

**d. rockfall;**

**e. alluvion;**

**f. avulsion;**

**g. unconsolidated fill;**

**h. defensible space for fire safety;**

**i. soil contamination;**

**j. subsidence; and**

**k. liquefaction**

**vii. Contaminated Sites:**

— the extent of the previous contamination of any site (whether it is in this Plan or not) and the remedial work that is to be undertaken on the site and in adjacent areas.

**viii. Water Supply:**

— the quantity, security and potability of the water and means, location and design of supply;

— water supplies for fire fighting purposes; and

— the location, scale, construction and environmental, including public health, effects of water supply infrastructure and the adequacy of existing supply systems outside the subdivision.

**ix. Stormwater Disposal:**

- **the means, design, scale, construction and standard of stormwater disposal systems (including soakage areas and the means and location of any outfall);**
- **the effectiveness and environmental impacts of any measures proposed for mitigating the effects of stormwater runoff, including the control of water-borne contaminants, litter and sediments;**
- **the location, scale, construction and environmental effects of stormwater infrastructure;**
- **the effect on water quality; and**
- **within any area subject to an Outline Development Plan, provision for future connection from the boundary of each allotment to the stormwater management system, as shown on the relevant District Plan Map, is to be secured by a condition of subdivision consent imposed under section 222 of the Resource Management Act 1991.**

**x. Sewage Disposal:**

- **the means, design and standard of sewage treatment and disposal where a public reticulation and treatment system is not available; and**
- **the location, scale, construction, maintenance and environmental effects of the proposed sewerage system.**

**xi. Waste Water Disposal:**

- **the means, design, scale, construction and environmental effects of waste water disposal systems.**

**xii. Energy Supply and Communications:**

- **the adequacy and standard of energy utility installation; and**
- **the adequacy and standard of communication utility installation.**

**xiii. Special Features:**

- **the positive effects arising from the retention and protection of vegetation and habitat sites, notable plants, and heritage resources listed in the District Plan, and other significant plants;**
- **provision for planting and other landscape treatments on roads and land to be vested as reserves;**

- the effects on mineral resources;
- provisions to safeguard the life supporting capacity of soils;
- reverse sensitivity effects;
- the effects of reverse sensitivity on Rangiora Racecourse; and
- The downstream effects of sedimentation and ecological changes from the development of Ravenswood on the waterways and wetlands of the Taranaki Stream.

**xiv. Easements:**

- the need to create easements for any purposes; and
- rights over access.

**xv. Structure Location:**

- the need to put constraints on the location of structures; and
- the effects of aircraft noise.

**xvi. Tangata Whenua:**

- the protection of sites of significant value to tangata whenua including wahi taonga identified through Policies 2.1.3.2, 2.1.3.3 and 2.1.3.5;
- the appropriateness of providing for access along and in the vicinity of the Taranaki Stream; and
- the effectiveness and environmental impacts of any measures proposed for mitigating the effects of subdivision on wahi taonga identified by Ngāi Tūāhuriri (Appendix 2.1).

**xvii. Archaeological Sites:**

- provisions to identify and/or protect archaeological sites;
- protocols to provide for wahi taonga, wahi tapu, urupa and other historic cultural sites; and
- alternative processes and protocols that protect the interests of Te Runanga o Ngai Tahu and Ngāi Tūāhuriri Runanga.

**xviii. Airport and Aircraft Noise:**

- the effect of the operation of the Christchurch International Airport on subdivision; and

— the effects of aircraft noise.

**xix. Intensive Farming and Liquid Effluent Spreading:**

— the need to protect separation distances required by the Plan.

**xx. Fill**

— the need to ensure overland flow paths are maintained;

— the effects of the fill on stormwater management on the site and adjoining properties;

— the appropriateness of the fill material;

— the effects of fill and difference in finished ground levels between adjoining properties; and

— timing and methodology.

**Add** new Clause (v) to Rule 32.1.1.21 to read as follows:

**v. The Residential 2 Zone Oxford Road, West Rangiora identified on District Plan Map 168.**

**Add** new Rule 32.1.1.75 to read as follows:

**32.1.1.75 Finished Section Levels Oxford Road, West Rangiora Outline Development Plan area.**

**Within the Oxford Road, West Rangiora Outline Development Plan Area as shown on District Plan Map 168 all residential allotments shall have a finished ground level that avoids inundation in a 0.5% Annual Exceedence Probability flood event from the Ashley River.**

**Cross Reference: Rule 27.1.1.23.**

**Add** new Rule 32.4.5 to read as follows:

**32.4.5 Any subdivision of land within the Oxford Road, West Rangiora Outline Development Plan area shown on District Plan Map 168 that does not comply with Rule 32.1.1.75 is a non-complying activity.**

#### DISTRICT PLAN MAPS

**Delete** District Plan Maps 34, 110 and 112 and **add** District Plan Maps 34, 110, 112 and 168 to rezone 52 and 74 Oxford Road being RS 1133 and Pt RS 968 to Residential 2.

Any consequential renumbering or amendments throughout the District Plan as necessary.

## **Māori Reserve 873**

### **Chapter 2. Māori**

Delete Policy 2.1.3.4 and add new Policy 2.1.3.4 to read as follows:

**Recognise the relationship of Ngāi Tūāhuriri with the land and associated resources in Māori Reserve 873 so as to enable the land to be used as intended by Kemps Deed of 1848 and the Crown Grants Act (No.2) of 1862, for places of residence and living activities for the original grantees and their descendants.**

Add new Policy 2.1.3.5 to read as follows:

#### **Policy 2.1.3.5**

**Provide for the use, development and protection of Māori Reserve 873 based on its unique character and cultural values by:**

- a. **maintaining and enhancing contained village areas providing for community facilities, convenience retail, recreational and business opportunities, comprehensive designed clustered housing, and compact and walkable neighbourhoods;**
- b. **maintaining and enhancing an extensive rural hinterland in immediate proximity to, and surrounding villages, within MR873 so that each village retains a rural outlook;**
- c. **connecting to Council reticulated water and sewer;**
- d. **avoiding land considered a high hazard risk including flood-prone land to the south;**
- e. **protecting and ecologically enhancing the Cam River corridor;**
- f. **re-establishing, protecting and ecologically enhancing the historic streams draining the eastern basin;**
- g. **protect and enhance views from settlement areas to Maungatere and Ngā Tiritiri o te Moana;**
- h. **recognising historic landscape and land use patterns; and**
- i. **recognising a range of housing options.**

## **Chapter 13. Resource Management Framework**

Add new Clause (h) to Policy 13.1.1.1 to read as follows:

**h. historical and cultural associations with Māori Reserve 873**

## **Chapter 14. Rural Zones**

Add new Objective 14.1.2 to read as follows:

### **Objective 14.1.2**

**Recognise the historic and cultural significance of Māori Reserve 873 to Ngāi Tūāhuriri and the different rural character arising from settlement by the original grantees and their descendants.**

Add new Policy 14.1.2.1 to read as follows:

### **Policy 14.1.2.1**

**Provide for subdivision and/or dwellinghouse development that enables Rural Zone land within Māori Reserve 873 to be used as intended by Kemps Deed of 1848 and the Crown Grants Act (No.2) of 1862 for places of residence for the original grantees and their descendants whilst:**

- a. **recognising, within the intensive development opportunities, a range of housing options provided housing density less than one dwellinghouse per 4ha is designed and constructed as part of an integrated comprehensive design for the whole site;**
- b. **connecting to Council reticulated water and sewer for residential development at a density less than one dwellinghouse per 4ha;**
- c. **avoiding residential development on land subject to hazards, including flooding from either breakouts or localised ponding;**
- d. **recognising the connections, open space, outlook, rural character and separation between contained village areas and more intensive residential development in the Rural Zone;**



- e. protecting and ecologically enhancing the Cam River corridor;
- f. re-establishing, protecting and ecologically enhancing the historic streams draining the eastern basin;
- g. maintaining rural environment, including between any intensive development opportunities; and
- h. implementing an Outline Development Plan.

## **Chapter 17. Residential**

**Delete** Table 17.1: Residential Zone Characteristics – Residential 3 and 4A/B and **add** new Table 17.1: Residential Zone Characteristics – Residential 3 and 4A/B to read as follows:

<u>Residential 3</u>	<u>Residential 4A/B</u>
<ul style="list-style-type: none"> <li>— <u>Predominant activity is living;</u></li> <li>— <u>detached dwellings including a number of baches;</u></li> <li>— <u>high proportion of smaller dwellings;</u></li> <li>— <u>diverse styles and ages of dwellings;</u></li> <li>— <u>range of housing options, including comprehensive housing development focussed around village areas at Tuahiwi;</u></li> <li>— <u>settlements are significantly smaller than other main towns;</u></li> <li>— <u>wide range of lot sizes;</u></li> <li>— <u>reduced size and nature of individual lots within Tuahiwi;</u></li> <li>— <u>a rural outlook and setting;</u></li> <li>— <u>access to public open space including the coastal environment</u></li> <li>— <u>– easy access to walking and cycling opportunities;</u></li> <li>— <u>churches, local shops, reserves and camping grounds are important activities within the zone;</u></li> <li>— <u>provision of a mixed use centre at Tuahiwi focusing on community facilities, convenience retail, recreational and business opportunities;</u></li> <li>— <u>limited advertising;</u></li> <li>— <u>intimate and informal streetscapes;</u></li> <li>— <u>the streets are areas of community interaction;</u></li> <li>— <u>limited footpaths;</u></li> <li>— <u>minimal kerb and channels;</u></li> <li>— <u>low noise levels;</u></li> <li>— <u>low traffic volumes on side streets;</u></li> </ul>	<ul style="list-style-type: none"> <li>— <u>Predominant activity is living;</u></li> <li>— <u>detached dwellings and associated buildings;</u></li> <li>— <u>some limited farming and horticulture;</u></li> <li>— <u>dwellling density is lowest for Residential Zones;</u></li> <li>— <u>dwellings in generous settings;</u></li> <li>— <u>average lot size of 0.25-1.0 hectare;</u></li> <li>— <u>limited number of lots located in a rural environment;</u></li> <li>— <u>rural style roads or accessways;</u></li> <li>— <u>opportunity for a rural outlook from within the zone;</u></li> <li>— <u>few vehicle movements within the zone;</u></li> <li>— <u>access to zones not from arterial roads;</u></li> <li>— <u>community water and/or sewerage schemes; and</u></li> <li>— <u>limited kerb, channelling and street lighting</u></li> </ul>

<u>Residential 3</u>	<u>Residential 4A/B</u>
<ul style="list-style-type: none"> <li>— <u>significant traffic levels on main roads, and during holidays; and</u></li> <li>— <u>limited services and utility capacities in most zones</u></li> </ul>	

Add new Policy 17.1.1.5 to read as follows:

**Policy 17.1.1.5**

**Provide for subdivision and/or business and residential development that enables Residential 3 Zoned land within Māori Reserve 873 to be used as intended by Kemps Deed of 1848 and the Crown Grants Act (No.2) of 1862 for places of residence and living activities for the original grantees and their descendants whilst:**

- a. **recognising a range of housing options and dwellinghouse densities providing for a variety of allotment sizes;**
- b. **creating compact contained village areas;**
- c. **providing for a centrally located, mixed use centre that recognises the need for community facilities, convenience retail, recreational and business opportunities at a size and scale appropriate to the development of Tuahiwi;**
- d. **defining any increase in the Residential 3 Zone by natural features, landscape character areas and built features;**
- e. **implementing an Outline Development Plan;**
- f. **connecting to Council reticulated water and sewer;**
- g. **avoiding residential development on land subject to hazards, including flooding from either breakouts or localised ponding;**
- h. **maintaining a rural outlook and setting;**
- i. **providing good connectivity via walking and cycling linkages;**
- j. **providing active interaction with the street frontages;**
- k. **re-establishing, protecting and ecologically enhancing the historic streams draining the eastern basin; and**
- l. **protecting and enhancing views from village areas to Maungatere and NgāTiritiri o te Moana.**

## **Chapter 18. Constraints**

Delete Policy 18.1.1.1 and Add new Policy 18.1.1. to read as follows:

**Growth and development proposals should provide an assessment of how:**

- **the use, development, or protection of natural and physical resources affected by the proposal will be managed in a sustainable and integrated way; and**
- **the adverse effects on those resources and the existing community will be avoided, remedied, or mitigated.**

**In particular, proposals should not be inconsistent with other objectives and policies in the District Plan, and show how and the extent to which they will:**

- a. protect areas of significant indigenous vegetation and habitats of indigenous fauna including vegetation and habitat sites listed in Appendix 25.1;**
- b. protect the outstanding landscape area as defined in the District Plan Maps;**
- c. avoid or mitigate natural hazards including:**
  - **flooding as defined in the District Plan Maps,**
  - **flooding from the Waimakariri or Ashley/Rakahuri Rivers,**
  - **seismic conditions including the potential for liquefaction and amplification effects,**
  - **damage from the sea, including erosion, storm and tsunami, and**
  - **land instability;**
- d. protect the life supporting capacity of soils;**
- e. maintain and enhance the environmental characteristics of adjoining zones, and the environment of the zone within which the proposal is located, as set out in Policies 14.1.1.2, 14.1.1.3, 14.1.2.1, 15.1.1.1, 16.1.1.1, 16.1.1.2, 16.1.1.6, 17.1.1.2, 17.1.1.3 and 17.1.1.25;**
- f. retain the rural environment between Residential 4A and 4B Zones, between the Rangiora, Kaiapoi, Woodend, Pegasus and Oxford urban areas, and other Residential 3 Zones; between any rural intensive development opportunities and villages within Māori Reserve 873; and between Kaiapoi and the Christchurch City boundary;**
- g. provide access to and along rivers, open spaces and reserves;**
- g. provide access to and along rivers, open spaces and reserves;**

- h. maintain and enhance the form and function of the District's towns;
- i. avoid or mitigate significant adverse effects on the form and function of the Business 1 Zones including its role as a dominant community focal point within the four main towns;
- j. avoid the effects of noise within the 5 0 dBA Ldn aircraft noise contour for Christchurch International Airport as defined on Plan Map 138, with the exception of the areas within Kaiapoi defined in Chapter 12A of the Canterbury Regional Council Regional Policy Statement;
- k. provide infrastructure for services and roading in a manner consistent with this District Plan;
- l. ensure the efficient and effective integration of any new infrastructure into the existing network, or ensure the efficient and effective ongoing working of a stand-alone system;
- m. avoid or mitigate potential adverse effects from sites and facilities using, storing, and/or disposing of hazardous substances;
- n. protect groundwater quality and quantity;
- o. protect surface water quality and quantity;
- p. protect wahi taonga;
- q. avoid adverse effects on heritage sites and protect those sites listed in Appendix 28.1;
- r. avoid adverse effects on significant plants and protect those notable plants listed in Appendix 29.1;
- s. avoid adverse effects on the Business 3 Zone;
- t. provide for efficiency in energy use;
- u. enable local communities to be more self-sustaining;
- v. affect the demand for transport;
- w. provide choice in transport mode, particularly modes with low adverse environmental effects; and
- x. avoid or mitigate for adverse impacts on the habitat of trout and salmon.
- aa. recognises the historical and cultural associations of Ngāi Tūāhuriri with the land in Māori Reserve 873 to provide for residential development opportunities for the original grantees and their descendants.

Any consequential renumbering or amendments throughout the District Plan as necessary.

# **Dwellinghouses in the Business 1 Zone**

## **Chapter 16. Business Zones**

Delete Policy 16.1.1.2 and add new Policy 16.1.1.2 to read as follows:

Policy 16.1.1.2

**Provide for development and activities within the Business 1 Zones where the characteristics of the Zone are observed:**

<b><u>Location</u></b>	<ul style="list-style-type: none"><li>— <b><u>Defines the town centres of Kaiapoi, Rangiora, Pegasus, Oxford and Woodend</u></b></li><li>— <b><u>Redevelopment and intensification opportunities within Kaiapoi, Rangiora, Oxford and Woodend</u></b></li><li>— <b><u>Compact, including medium to high building density</u></b></li></ul>
<b><u>Pedestrian focus on primary shopping streets</u></b>	<ul style="list-style-type: none"><li>— <b><u>Interconnected network of public car parking, pedestrian areas, lanes and footpaths</u></b></li><li>— <b><u>Public open spaces</u></b></li><li>— <b><u>High level of safety, taking into account Crime Prevention Through Environmental Design (CPTED) principles</u></b></li><li>— <b><u>Buildings and businesses directly accessed from the street, lanes and public spaces</u></b></li><li>— <b><u>Verandahs and covered shopping areas</u></b></li></ul>
<b><u>Vehicle focus</u></b>	<ul style="list-style-type: none"><li>— <b><u>Provision for car parking, private and public</u></b></li><li>— <b><u>Interconnected network of roads, car parking, pedestrian areas, footpaths, lanes and public spaces</u></b></li><li>— <b><u>Public off-street parking</u></b></li><li>— <b><u>Little on-site parking</u></b></li></ul>

<p><b><u>Amenities</u></b></p>	<ul style="list-style-type: none"> <li>— <b><u>Landscaping, plantings and public open spaces</u></b></li> <li>— <b><u>Street and pedestrian treatments, including street furniture</u></b></li> <li>— <b><u>Lighting, taking into account Crime Prevention Through Environmental Design (CPTED) principles</u></b></li> <li>— <b><u>Minimal odour</u></b></li> <li>— <b><u>Low level noise</u></b></li> <li>— <b><u>Signage mostly small scale</u></b></li> <li>— <b><u>Public facilities</u></b></li> </ul>
<p><b><u>Parking</u></b></p>	<ul style="list-style-type: none"> <li>— <b><u>Public off-street parking</u></b></li> <li>— <b><u>Limited private off-street parking</u></b></li> <li>— <b><u>Limited duration on-street parking</u></b></li> <li>— <b><u>Public parking pedestrian connections with footpaths, lanes and public spaces</u></b></li> </ul>



<p><b><u>Built environment and built form</u></b></p>	<ul style="list-style-type: none"> <li>— <b><u>Defined building heights, predominantly two storey</u></b></li> <li>— <b><u>Absence of setbacks on identified streets and limited setbacks on other streets</u></b></li> <li>— <b><u>Mostly continuous business display frontages on primary shopping streets</u></b></li> <li>— <b><u>High intensity of use from the street side</u></b></li> <li>— <b><u>Historic buildings and settings defined by heritage values within Kaiapoi, Rangiora, Woodend and Oxford</u></b></li> <li>— <b><u>Mostly older buildings on main shopping streets, with the exception of Pegasus</u></b></li> <li>— <b><u>New buildings sympathetic to existing built form and building styles</u></b></li> <li>— <b><u>Functional and adaptable buildings</u></b></li> <li>— <b><u>In Pegasus new buildings and development within a compact and identifiable centre</u></b></li> <li>— <b><u>In the commercial centre of Pegasus, no building setback, with development required to be along the full street frontage with verandahs</u></b></li> <li>— <b><u>In the outer commercial area of Pegasus, building setback is required</u></b></li> <li>— <b><u>Dwellinghouse development within Kaiapoi, Rangiora and Woodend located only at upper floor levels</u></b></li> </ul>
<p><b><u>Distribution of floorspace</u></b></p>	<ul style="list-style-type: none"> <li>— <b><u>Largest total area of retail, office, administrative floorspace in each town</u></b></li> </ul>

<b><u>Function</u></b>	<ul style="list-style-type: none"> <li>— <b><u>Community focal point for:</u></b></li> <li>— <b><u>government services;</u></b></li> <li>— <b><u>professional services;</u></b></li> <li>— <b><u>office/finance;</u></b></li> <li>— <b><u>retail;</u></b></li> <li>— <b><u>emergency services; and</u></b></li> <li>— <b><u>household services</u></b></li> <li>— <b><u>An area with safe, convenient, pleasant, attractive environments where people can enjoy extended visits to gather, socialise, and do business</u></b></li> </ul>
------------------------	--

**Add** new Policy 16.1.1.3 to read as follows:

**Policy 16.1.1.3**

**Provide for dwellinghouse development within the Business 1 Zones of Kaiapoi, Rangiora and Woodend that:**

- a. **ensures business activities are predominant within the site and across the zone;**
- b. **locates above buildings used for business activities to maximise ground floor business space; and**
- c. **achieves high levels of amenity, including mitigation of reverse sensitivity effects between business and residential activity.**

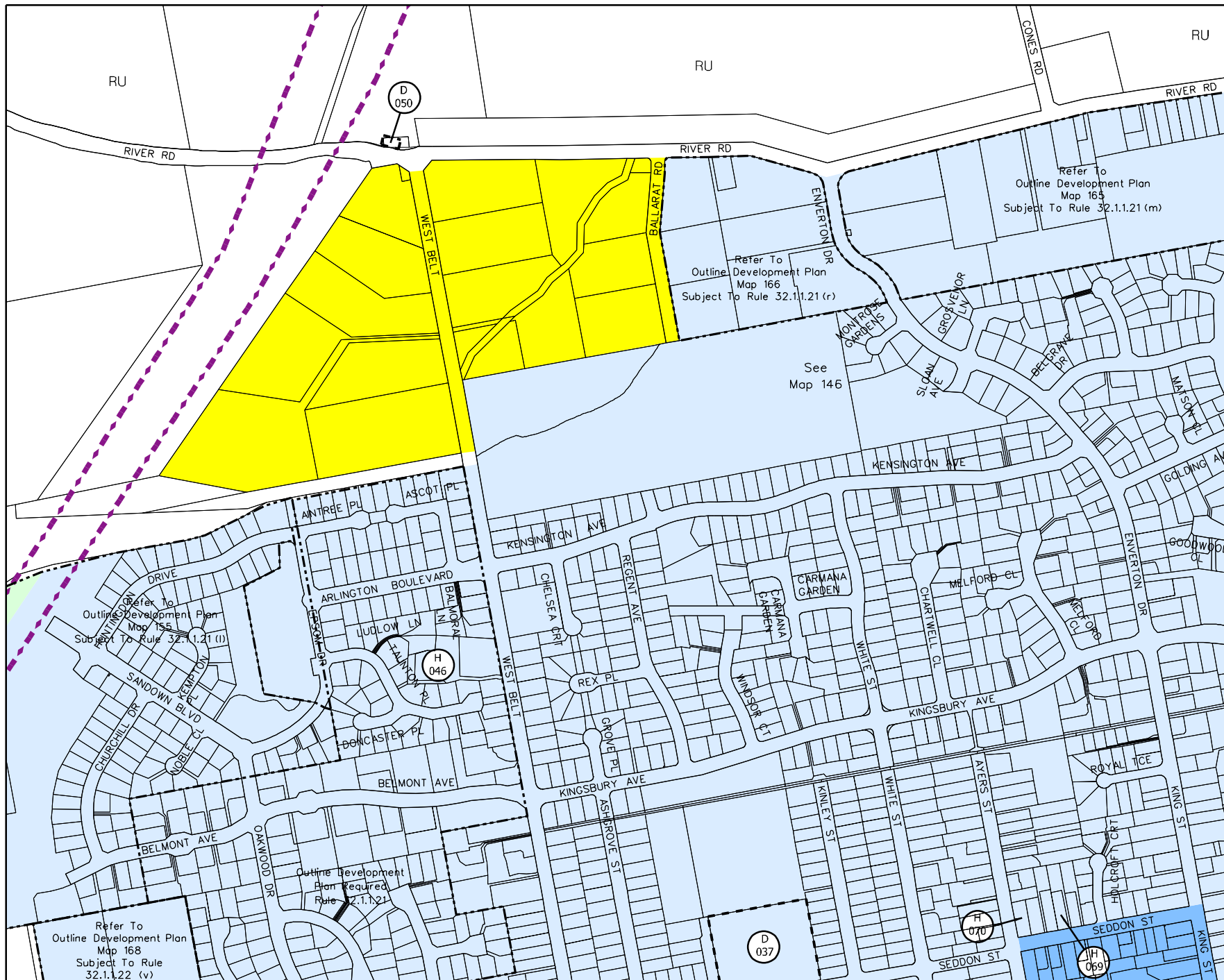
**Chapter 31. Health Safety & Wellbeing**

**Add** new Rule 31.5.5 to read as follows:

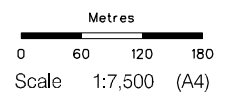
**31.5.5 The erection of any dwellinghouse at ground floor level within the Business 1 Zone at Kaiapoi, Rangiora and Woodend is a non-complying activity.**

**Delete** Planning Maps 34, 35, 46, 47, 58, 59, 101, 102, 103, 104, 105, 105A, 110, 110A, 111, 112, 113, 114, 116, 117, 118, 119, 125, 125A, 128, 128A and 131 and **add** new Planning Maps 34, 35, 46, 47, 58, 59, 101, 102, 103, 104, 105, 105A, 110, 110A, 111, 112, 113, 114, 116, 117, 118, 119, 125, 125A, 128, 128A and 131 to identify where rebuilding and development should and should not occur before 2028 in Attachment 1.

Any other consequential changes to the District Plan as necessary.

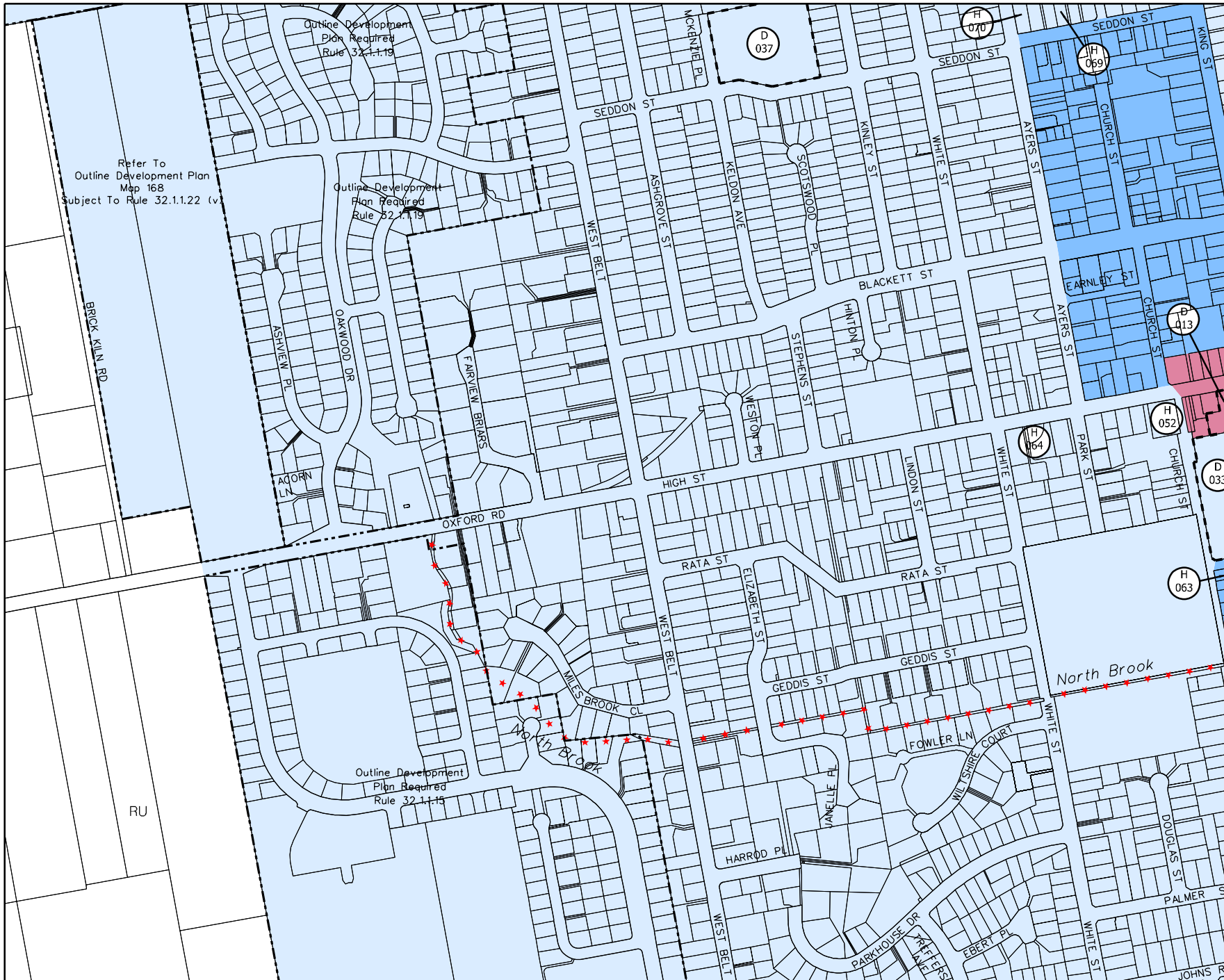


NOTE:  
Disclaimer - refer to map legend sheet



	34	
110A	110	111
34	112	113

Rangiora  
**110**



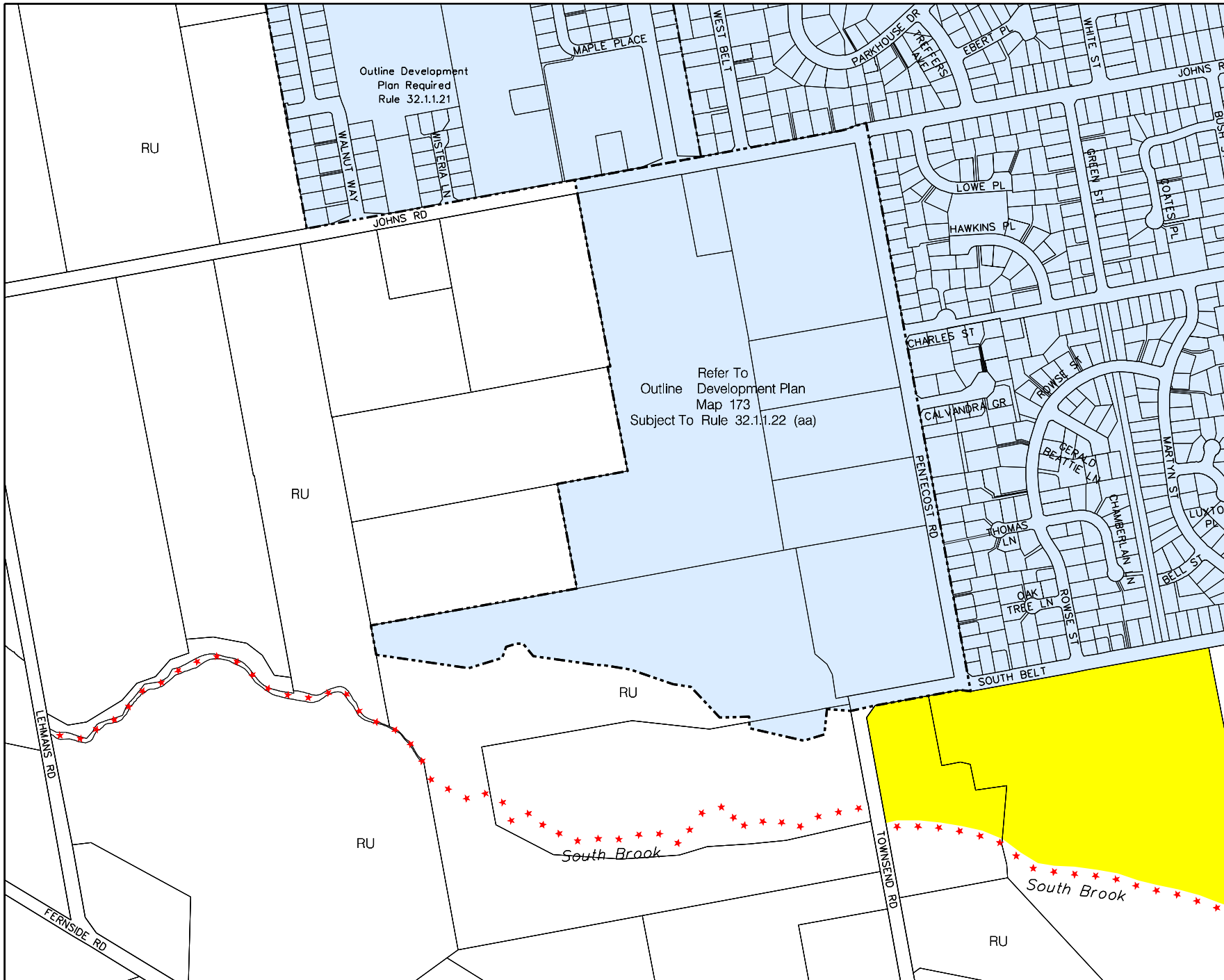
NOTE:  
Disclaimer - refer to map legend sheet



Metres  
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Scale 1:7,500 (A4)

110A	110	111
34	<b>112</b>	113
46	116	117

Rangiora  
**112**



NOTE:  
Disclaimer - refer to map legend sheet



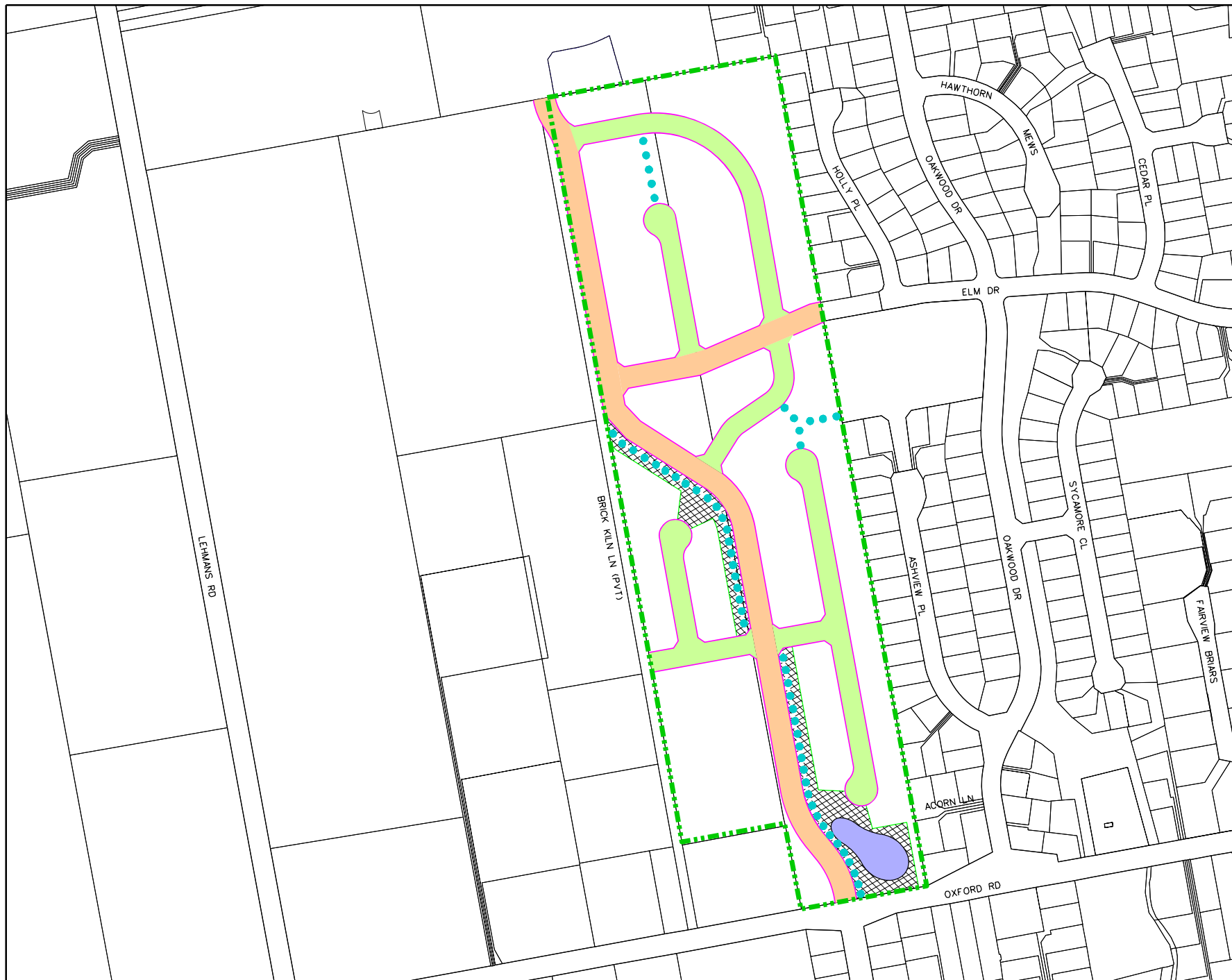
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Scale 1:7,500 (A4)









34	112	113
	116	117
46		118

Rangiora

116





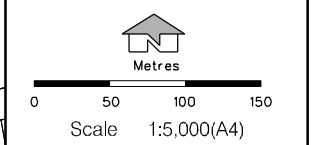
-  OUTLINE DEVELOPMENT PLAN AREA
-  EXISTING ROAD
-  LOCAL ROAD
-  URBAN COLLECTOR ROAD
-  PROPOSED ROAD DESIGN
-  PEDESTRIAN CYCLEWAY ACCESS
-  LOCAL PURPOSE RESERVE
-  STORMWATER MANAGEMENT AREA

NOTES:  
WATER RACE SHALL NOT BE LOCATED WITHIN MULTIPLE PRIVATE OWNERSHIP.













SUBDIVISION AND DEVELOPMENT SUBJECT TO GROUND AND FLOOR LEVEL CONTROLS TO ADDRESS NATURAL HAZARD ISSUES.

SUBDIVISION AND DEVELOPMENT SHALL ACCOMMODATE OVERLAND FLOW ROUTES THROUGH THE SITE.

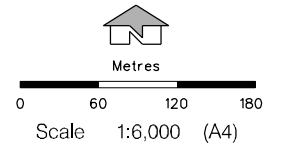
NOTE:  
Disclaimer - refer to map legend sheet



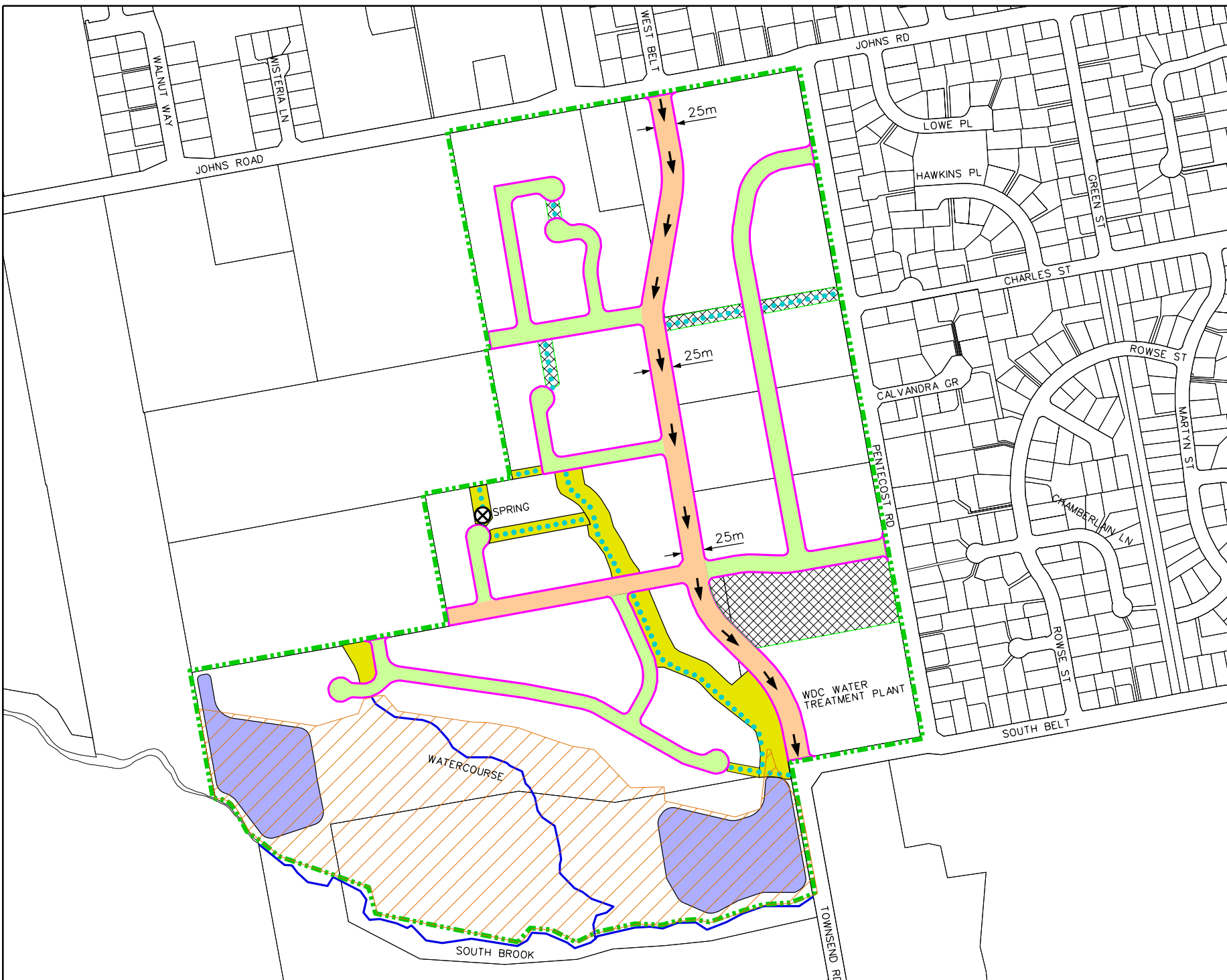
Oxford Road, West Rangiora Outline Development Plan

-  OUTLINE DEVELOPMENT PLAN AREA
  -  STORMWATER CHANNEL
  -  EXISTING ROAD
  -  PROPOSED ROAD DESIGN
  -  LOCAL ROAD
  -  URBAN COLLECTOR ROAD
  -  LOCAL RESERVE
  -  RECREATION & STORMWATER LINKAGES
  -  HIGH HAZARD FLOOD AREA
  -  PEDESTRIAN CYCLEWAY ACCESS
  -  STORMWATER MANAGEMENT AREA
  -  SPRING HEAD
- NOTE: FLOW FROM SPRING HEAD SHALL BE KEPT SEPARATE FROM THE STORMWATER MANAGEMENT SYSTEM

NOTE:  
Disclaimer - refer to map legend sheet



South West Rangiora Outline Development Plan



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# **Attachment 1**

## **Priority Area Planning Maps**

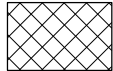

## RURAL LEGEND (Sheets 1 to 76)

	Zone Business 2
	Zone Rural
	Subdivision Constraint
	Localised Flooding Area
	Outstanding Landscape - Ashley Gorge
	Outstanding Landscape - Core
	Outstanding Landscape - Buffer
	Prominent Ridges
	Goat control area
<b>Note:</b>	See Rule 23.1.1.17 for goat control (includes Outstanding Landscape areas)
	Vegetation & Habitat Site; - V159 Oxford Conservation Area
	Vegetation & Habitat Site; - V160 Mt Thomas Forest
	Vegetation & Habitat Site; - V161 Puketeraki Forest
<b>NOTE:</b>	These notations do not necessarily indicate the precise position of the Site, nor relate to the size of any Site.



	Designations
	Heritage Sites
	Vegetation and Habitat Sites
	Notable Plant Sites
	Archaeological Sites
	Waahi Tapu / Waahi Taonga

### LURP (Land Use Recovery Plan)


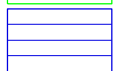
#### LURP Priority Areas - Business

	Zoned
	Not Yet Zoned

#### LURP Priority Areas - Residential

	Zoned
	Not Yet Zoned

#### Greenfields Areas

	Residential Greenfields - Post LURP
	Business Greenfields - Post LURP

★ ★ ★ ★ River Reaches subject to esplanade provisions

▲ ▲ ▲ Limited Access Road

--- Coastal Marine Area boundary

--- Average Noise Exposure Contours;  
Christchurch International Airport

**50** Noise Level in dBA Ldn

— Transit New Zealand Designation

— Tranz Rail Designation

— Transpower High Voltage Lines

— Waimakariri District boundary

 Area Covered by 1:7,500 scale

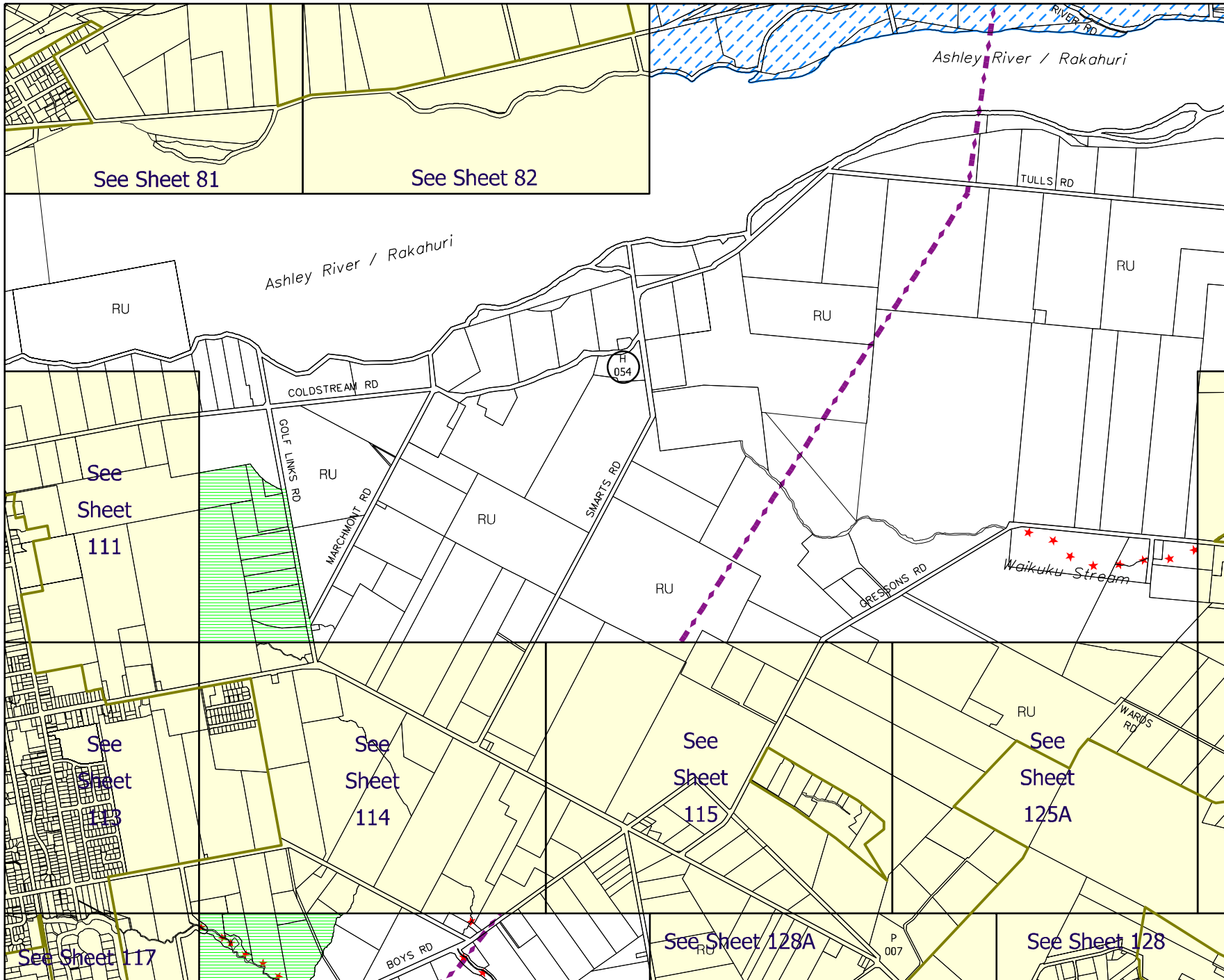
**NOTE:**  
The planning maps are produced in colour and are intended to provide accurate and adequate information as at the date of publication and at the scale at which they are published. The Waimakariri District Council will not accept liability to any person or entity arising out of any reliance in part or full, by such person or entity upon any of the contents of the planning maps for any purpose in circumstances where they are reproduced in a way that alters the scale, and / or colour or any other detail of the maps, and the information contained therein.

URBAN LEGEND (Sheets 77 to 133)

	Zone Business 1		Subdivision Constraint (Rural)		Designations		River Reaches subject to esplanade provisions
	Zone Business 2		Subdivision Constraint Area (Policy 18.1.1.8)		Heritage Sites		Limited Access Road
	Zone Business 3		Pegasus Rural Zone See Map 142		Vegetation and Habitat Sites		Road to be Closed
	Zone Business 4		Localised Flooding Area		Notable Plant Sites		Outline Development Plan Required Boundary
	Zone Residential 1		Goat control area		Archaeological Sites		Coastal Marine Area boundary
	Zone Residential 2				Waahi Tapu / Waahi Taonga		Average Noise Exposure Contours; Christchurch International Airport
	Zone Residential 3		LURP (Land Use Recovery Plan)		NOTE: These notations do not necessarily indicate the precise position of the Site, nor relate to the size of any Site.		Noise Level in dBA Ldn
	Zone Residential 4A		LURP Priority Areas - Business Zoned		NOTE: See Rule 23.1.1.17 for goat control (includes Outstanding Landscape areas)		Transit New Zealand Designation
	Zone Residential 4B		Not Yet Zoned				Tranz Rail Designation
	Zone Residential 5		LURP Priority Areas - Residential				Transpower High Voltage Lines
	Zone Residential 6		Zoned				Waimakariri District boundary
	Zone Residential 6A		Not Yet Zoned				
	Zone Residential 7		Greenfields Areas				
	Zone Rural		Residential Greenfields - Post LURP				
	Zone Mapleham Rural 4B		Business Greenfields - Post LURP				

NOTE:  
The planning maps are produced in colour and are intended to provide accurate and adequate information as at the date of publication and at the scale at which they are published. The Waimakariri District Council will not accept liability to any person or entity arising out of any reliance in part or full, by such person or entity upon any of the contents of the planning maps for any purpose in circumstances where they are reproduced in a way that alters the scale, and / or colour or any other detail of the maps, and the information contained therein.





NOTE:  
Disclaimer - refer to map legend sheet

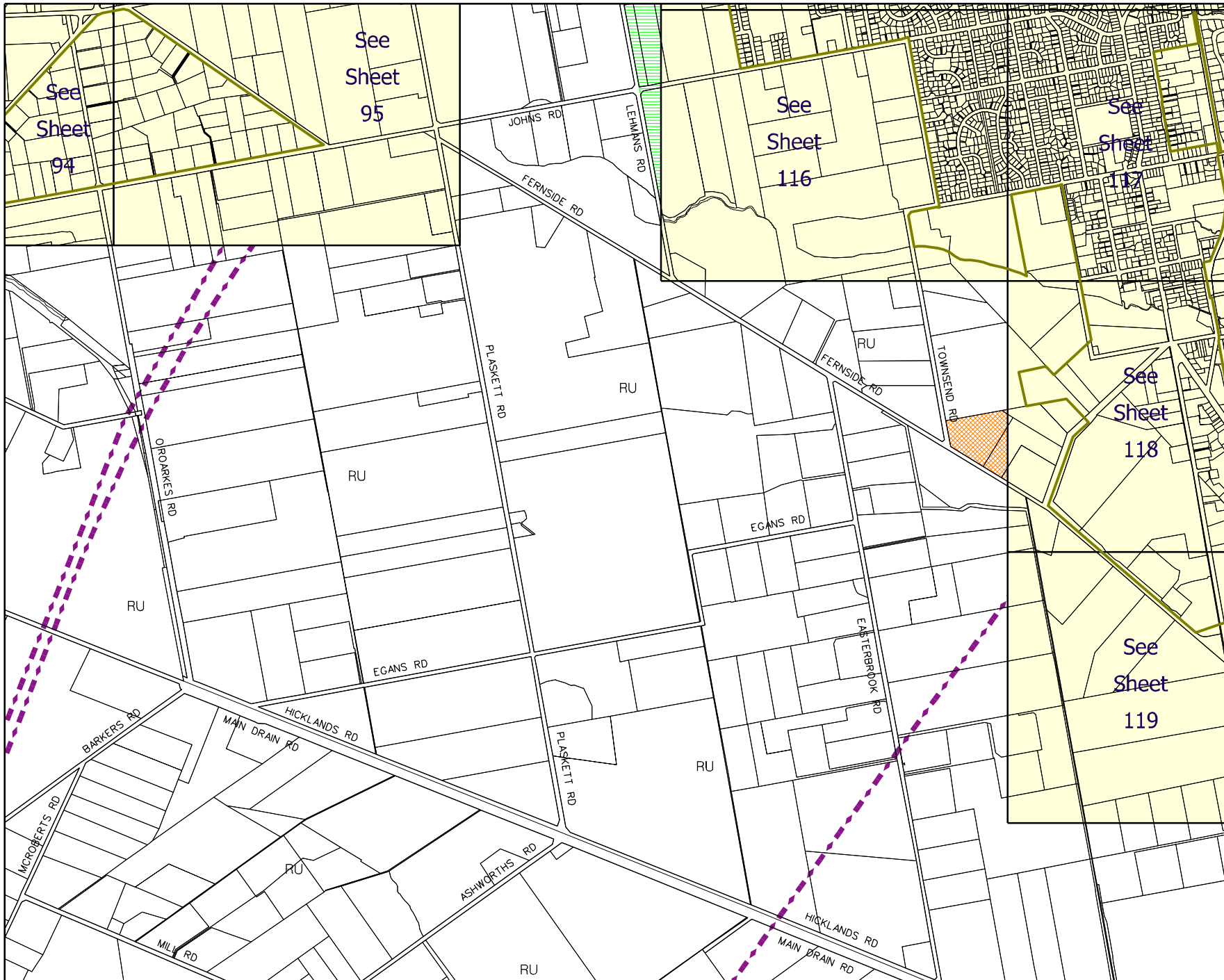


Metres  
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Scale 1:25,000 (A4)

22	23	84	24
78	79	83	82
34	110	111	35
95	112	113	114
46	116	117	47
	118	119	120
		128A	128
		132	131
			133
			48

35





NOTE:  
Disclaimer - refer to map legend sheet

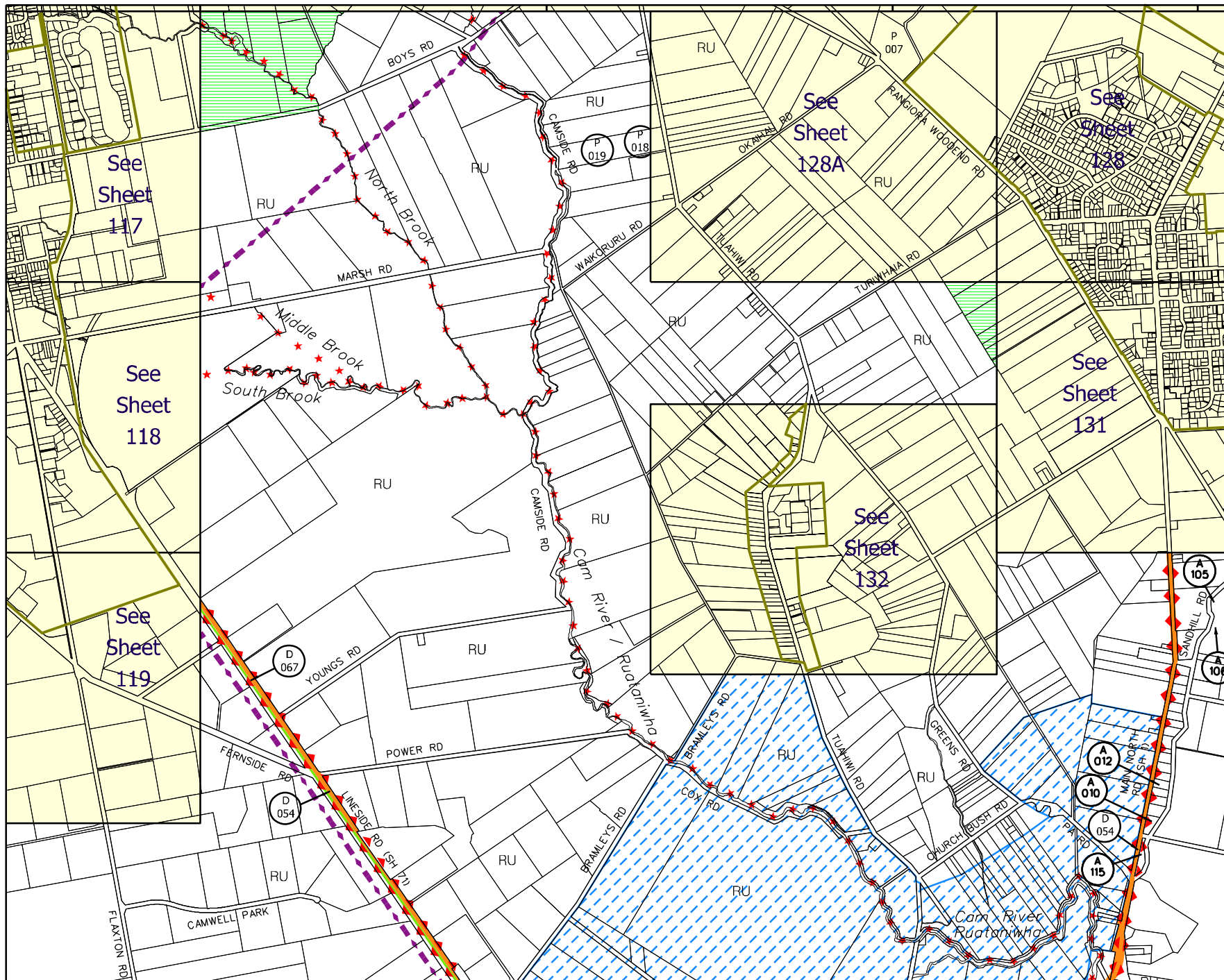


Metres

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Scale 1:25,000 (A4)

		34		
33		110 111	35	
	94 95	112 113 114 115		
45		116 117	47	
		118 119	132	
56		89	58	
90 91		57	102	
92 93			104	

46



NOTE:  
Disclaimer - refer to map legend sheet



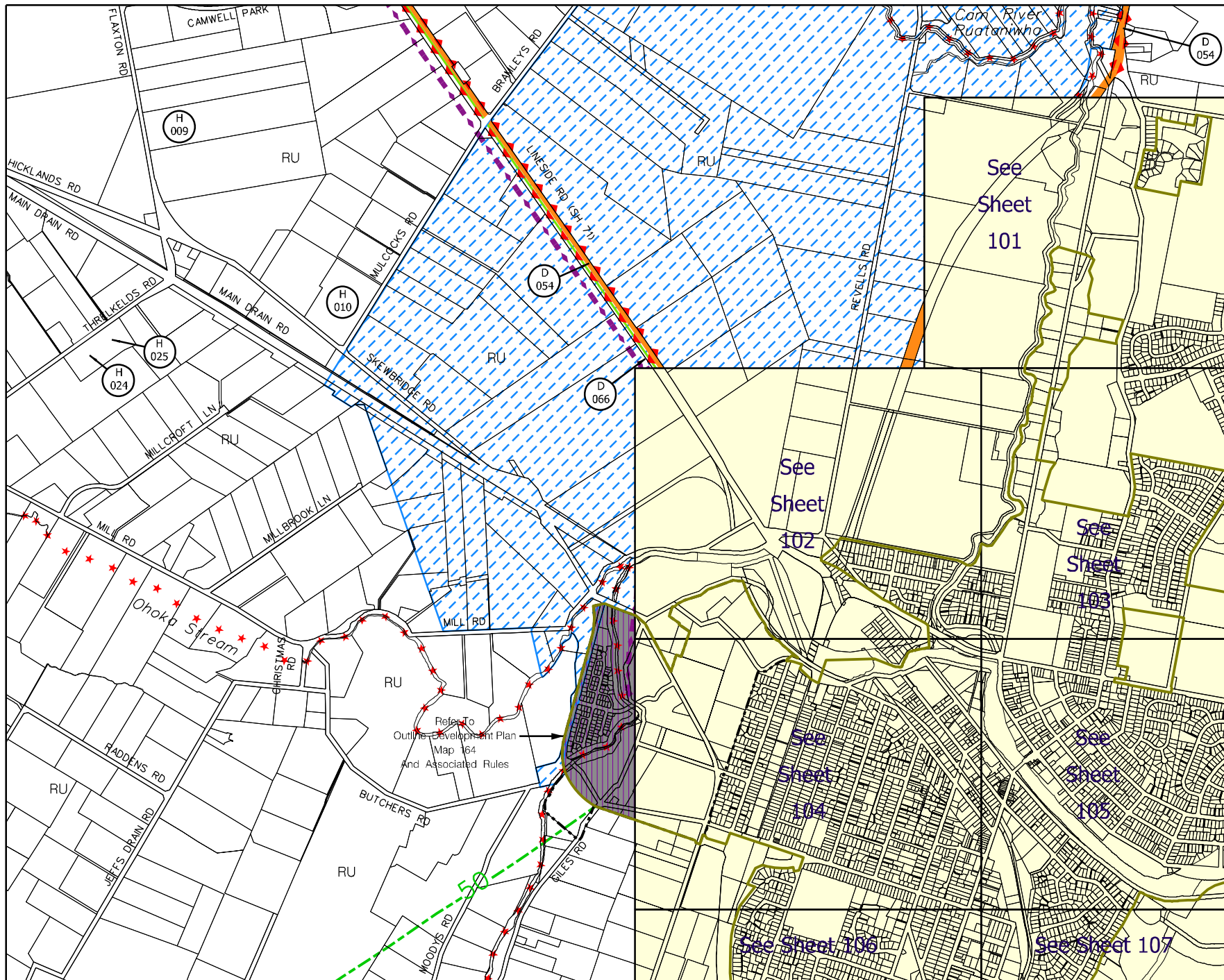
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Scale 1:25,000 (A4)

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	112	113	114	115	122	123
95	116	117	128A	128B	129	130
	46	118	47	131	48	
		119		132	133	
	89			101		
57		58	102	103	108	59
			104	105	109	





NOTE:  
Disclaimer - refer to map legend sheet

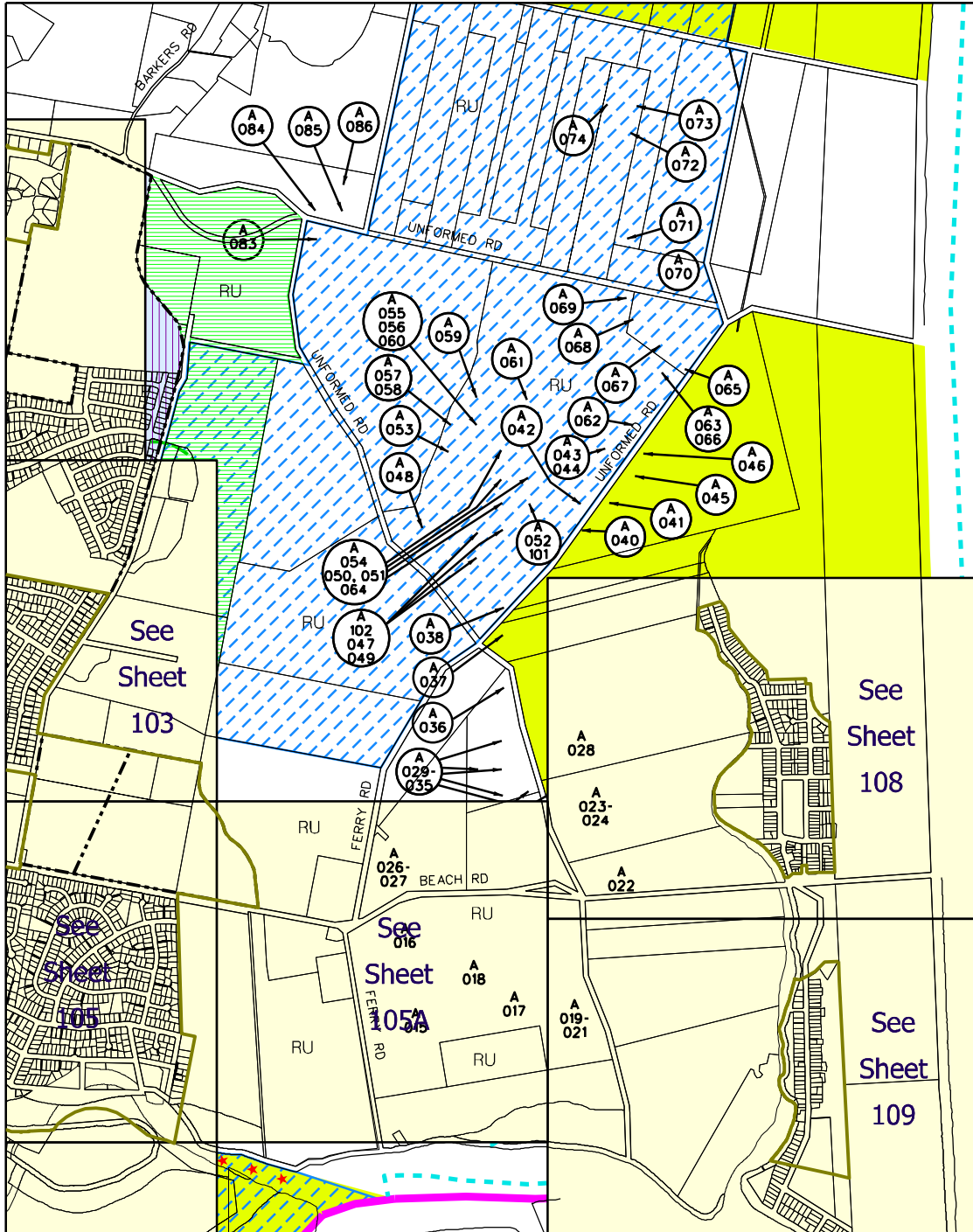



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Scale 1:25,000 (A4)

116	117	47	128	129	130
46	118	132	131	48	
	119				133
89	<b>58</b>	101	59		
57		102	103	108	
		104	105	109	
		106	107		
67	68	69			

58






 For clearer identification of archaeological sites see maps 149 & 150.

COASTAL MARINE AREA

NOTE:  
Disclaimer - refer to map legend sheet

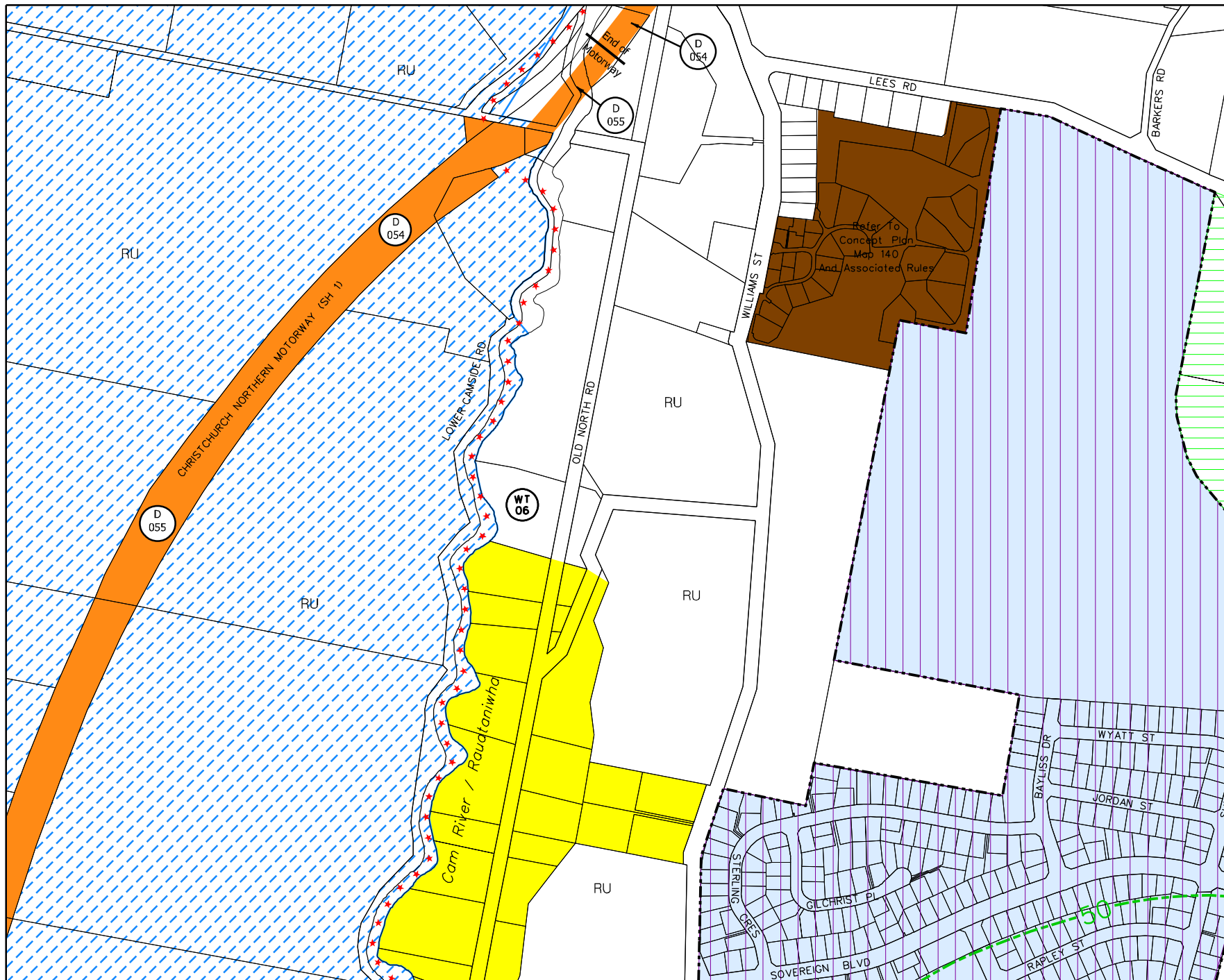


Metres  
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 Scale 1:25,000 (A4)

47	128	129	130
132	131	48	133
58	101	<b>59</b>	
	102	103	108
	104	105	109
68	106	107	
	69		

COASTAL MARINE AREA

See Sheet  
109



NOTE:  
Disclaimer - refer to map legend sheet



Metres

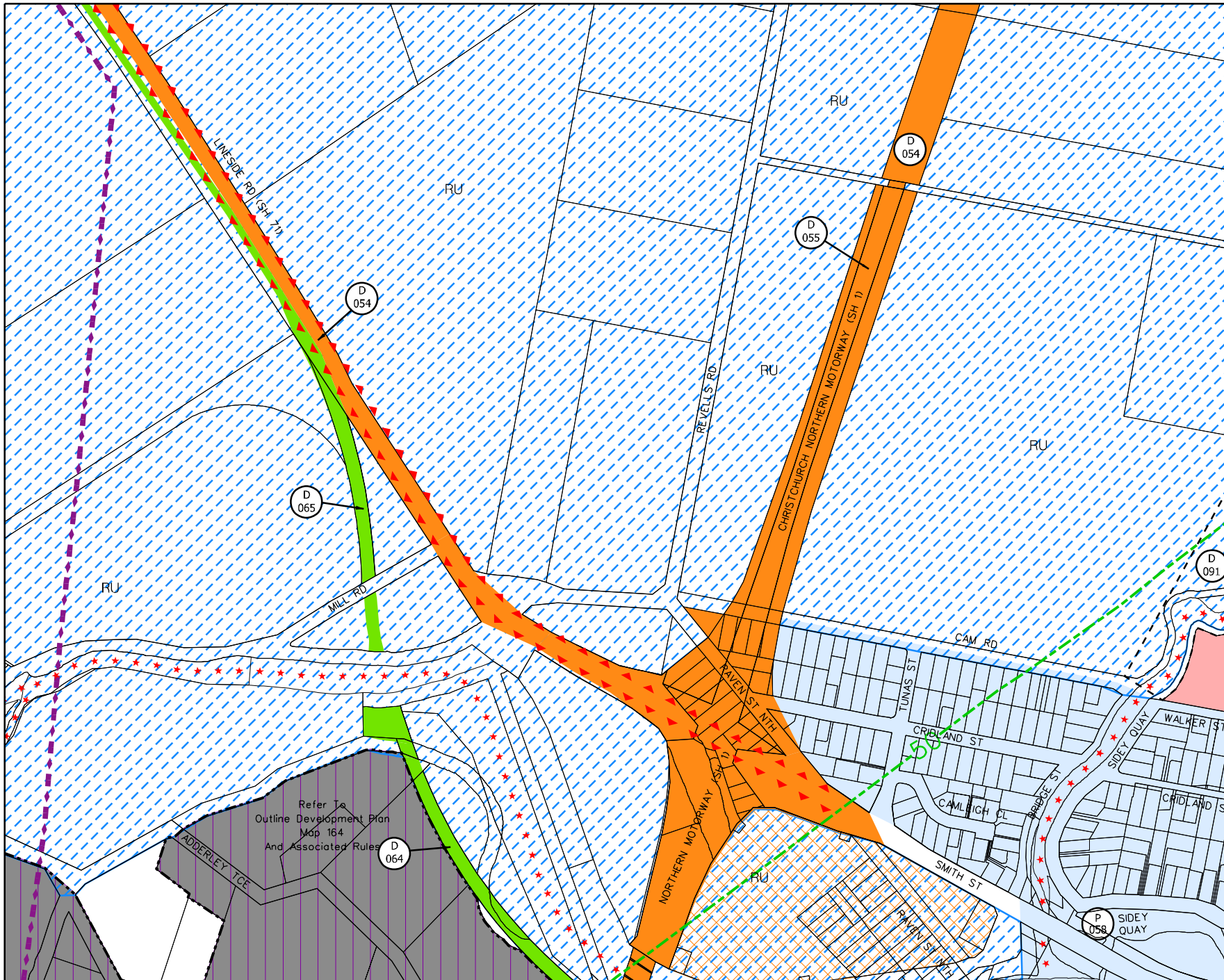
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Scale 1:7,500 (A4)

47	48
58	101
102	103

Kaipoi

101



NOTE:  
Disclaimer - refer to map legend sheet

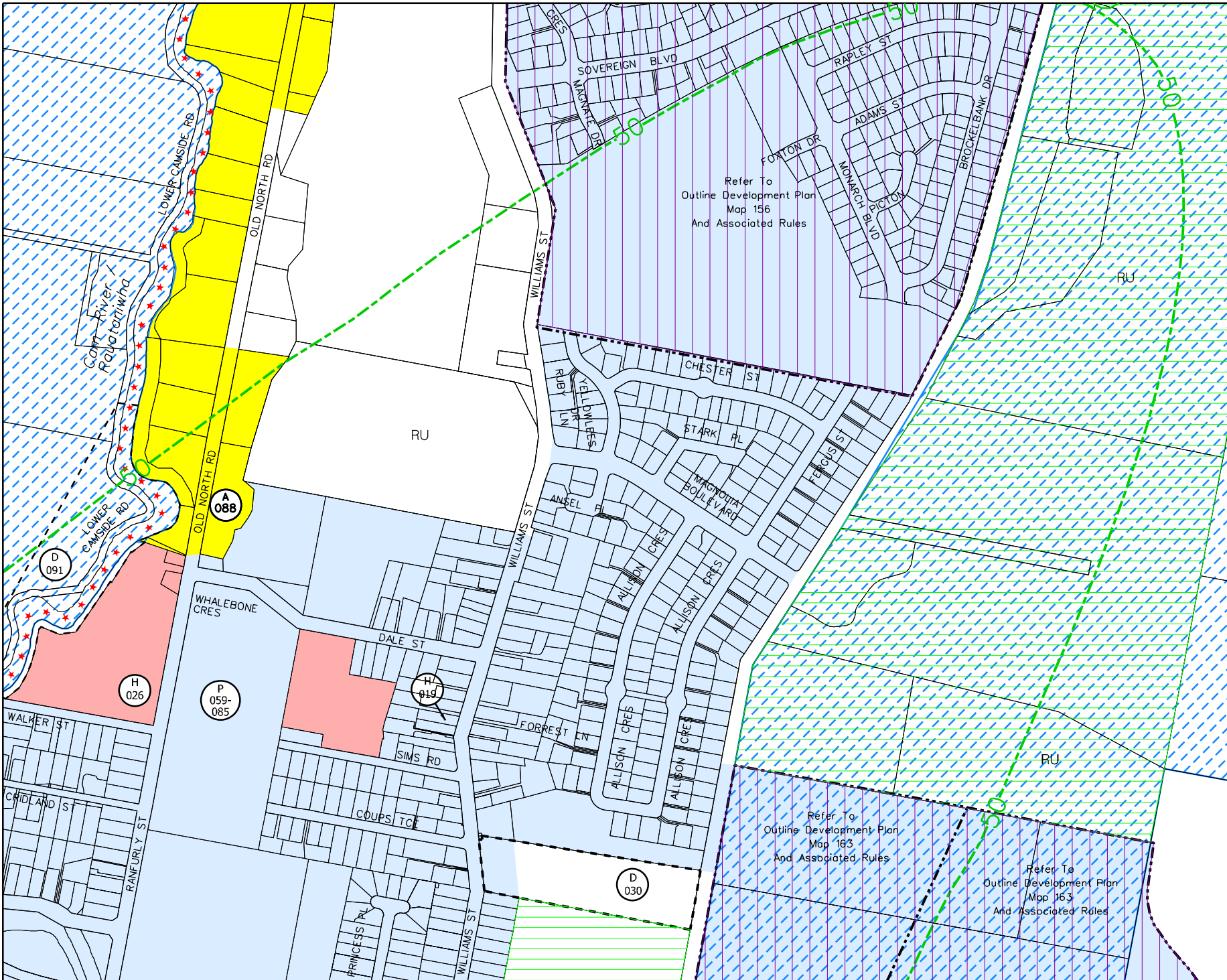


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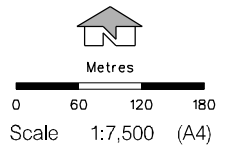
		101
58	102	103
	104	105

Kaipoi  
102



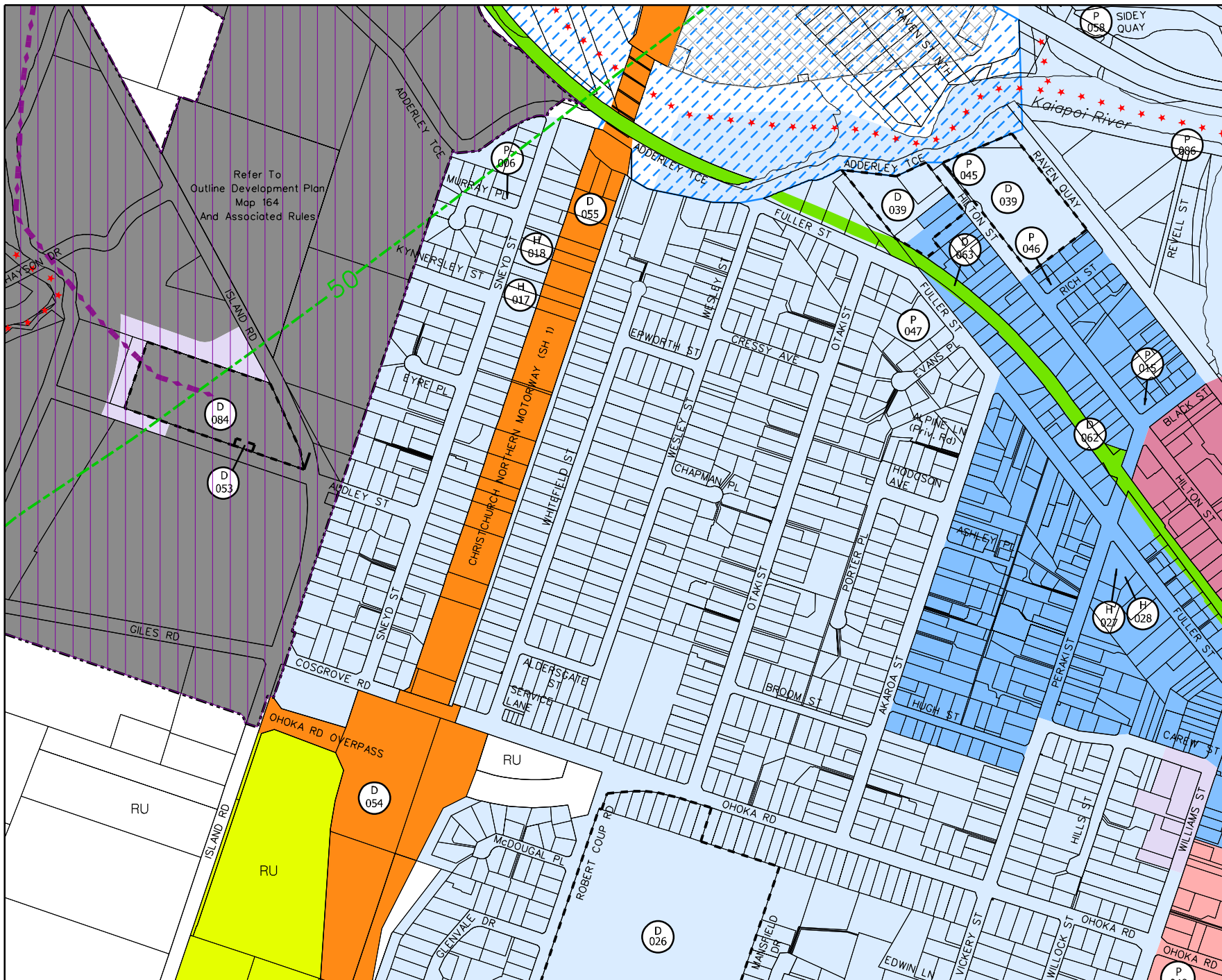


NOTE:  
Disclaimer - refer to map legend sheet



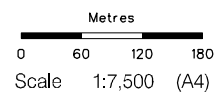
58	101	59
102	<b>103</b>	
104	105	105A

Kaiapoi  
**103**



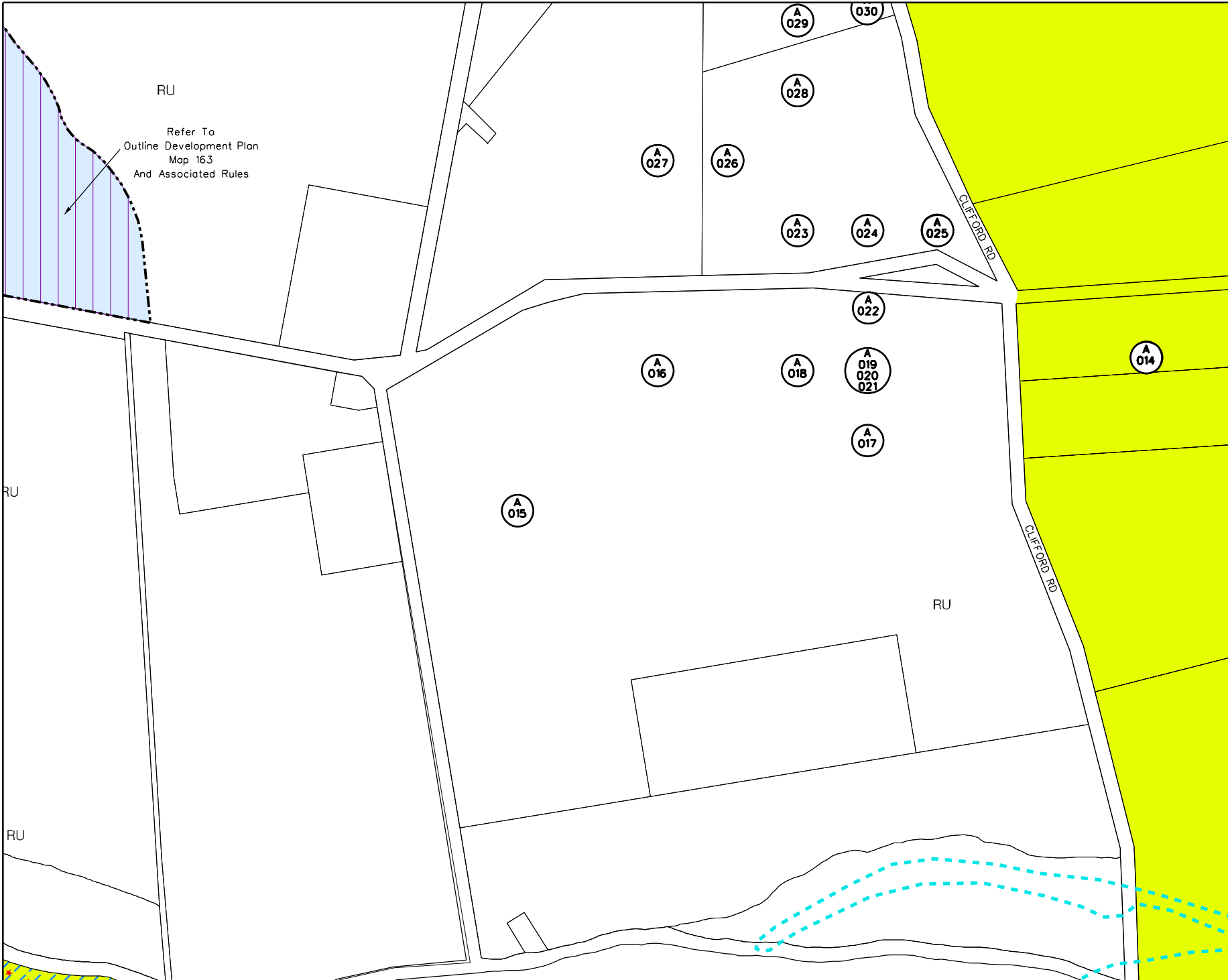
Refer To  
Outline Development Plan  
Map 164  
And Associated Rules

NOTE:  
Disclaimer - refer to map legend sheet

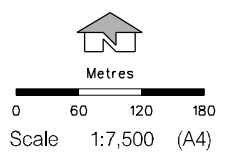


	102	103
58	<b>104</b>	105
68	106	107

Kaipoi  
**104**



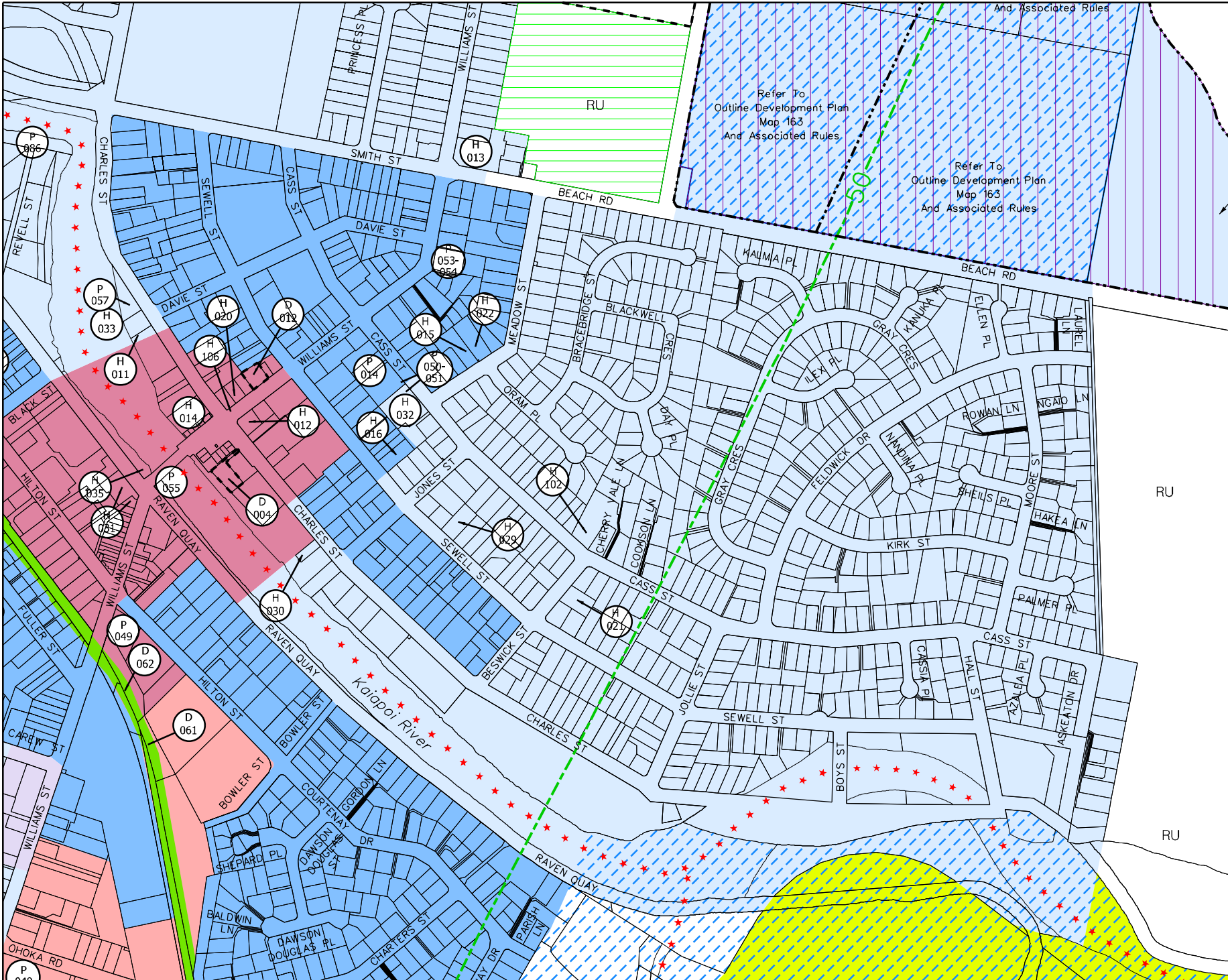
NOTE:  
Disclaimer - refer to map legend sheet



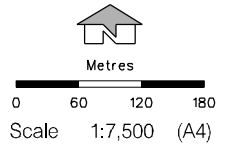
103	59	108
105	<b>105A</b>	109
107	69	

Kaipoi  
**105A**





NOTE:  
Disclaimer - refer to map legend sheet



102	103	59
104	<b>105</b>	105A
106	107	69

Kaiapoi  
**105**



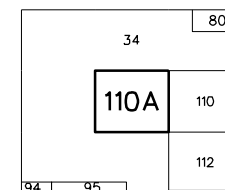
NOTE:  
Disclaimer - refer to map legend sheet



Metres

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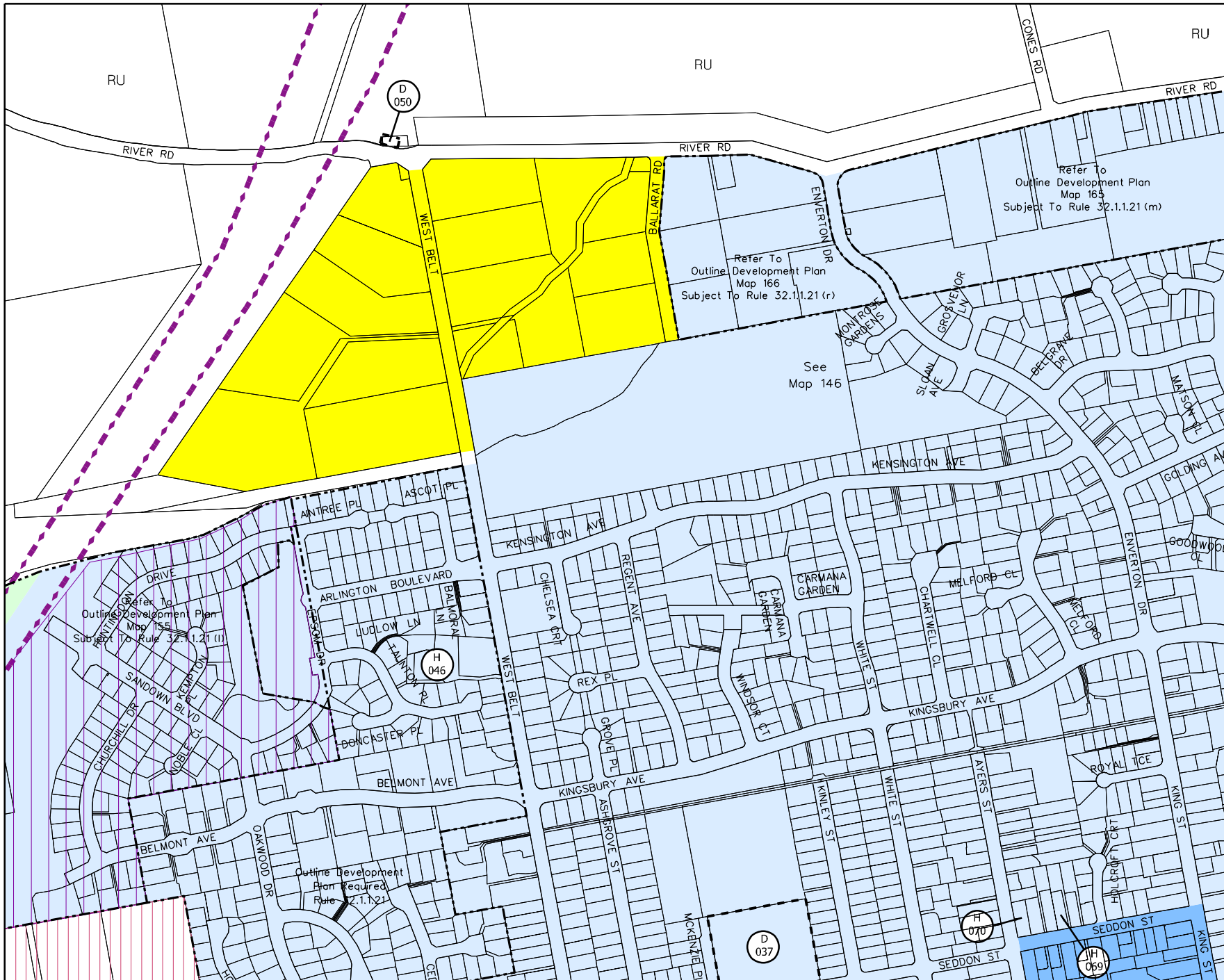
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Rangiora

110A





NOTE:  
Disclaimer - refer to map legend sheet



Metres

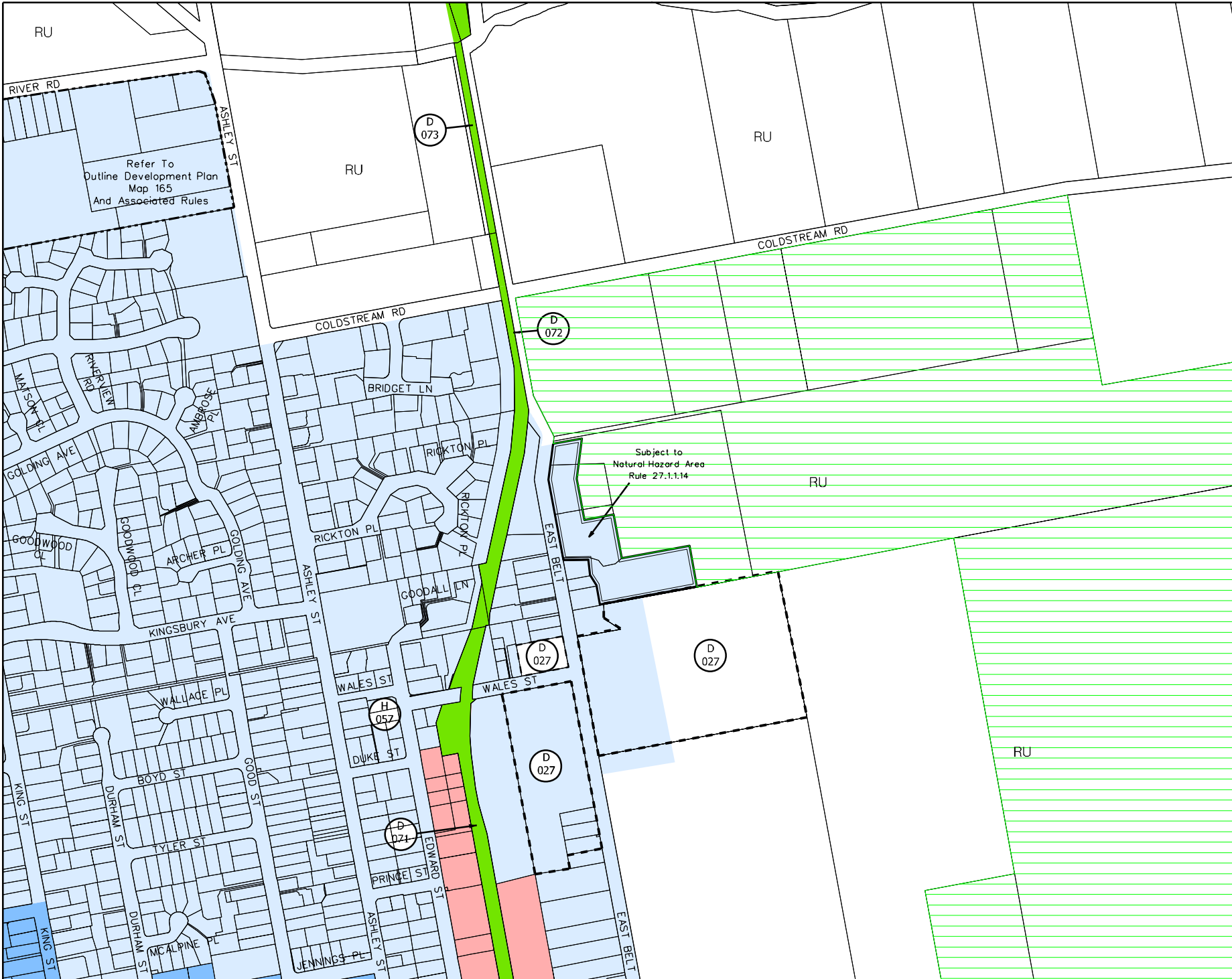
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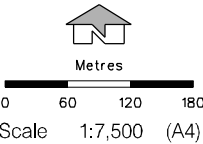
	34	
110A	110	111
34	112	113

Rangiora

110

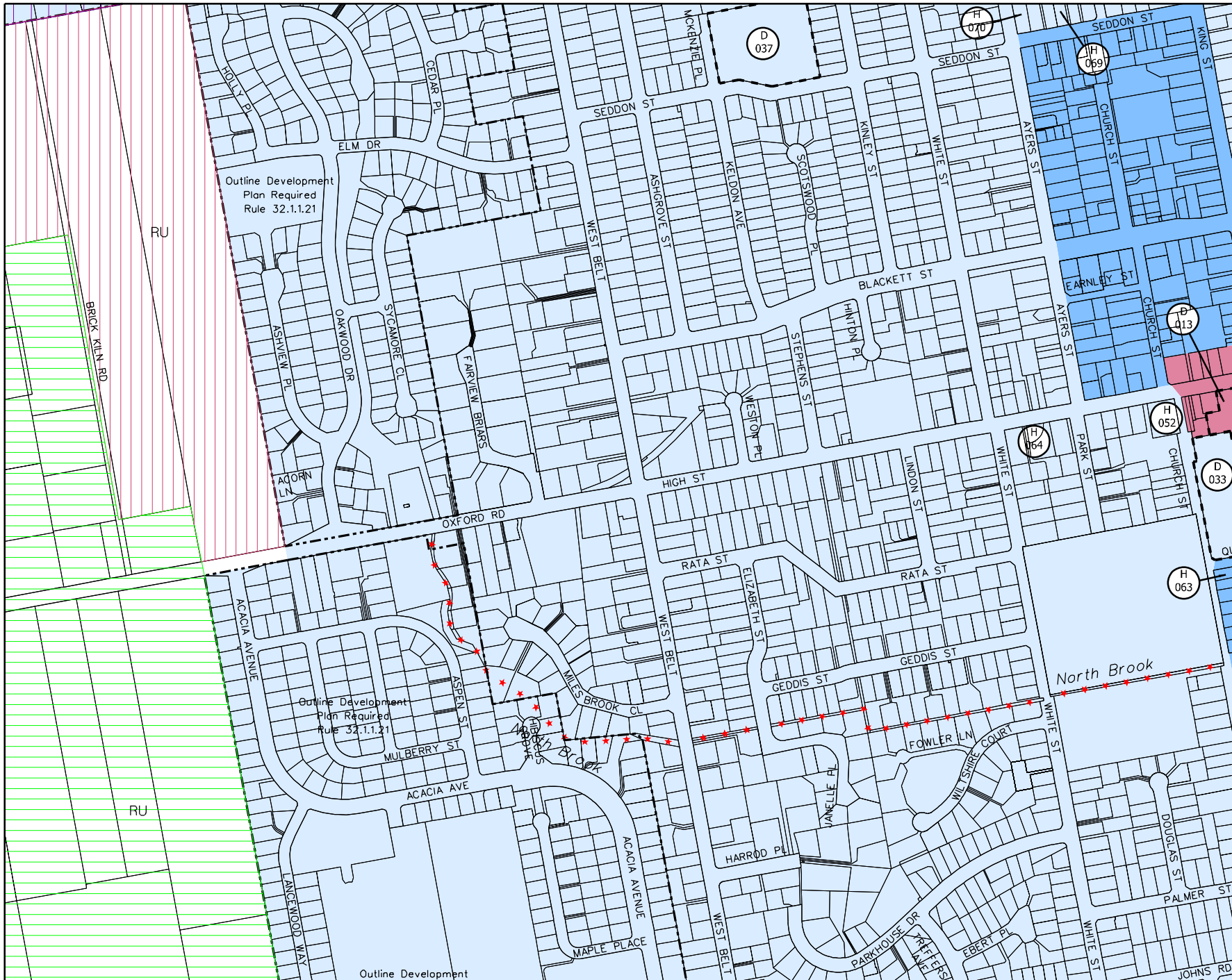


NOTE:  
Disclaimer - refer to map legend sheet



34		35
110	111	
112	113	114

Rangiora  
111

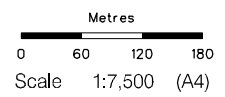


Outline Development  
Plan Required  
Rule 32.1.1.21

Outline Development  
Plan Required  
Rule 32.1.1.21

Outline Development

NOTE:  
Disclaimer - refer to map legend sheet

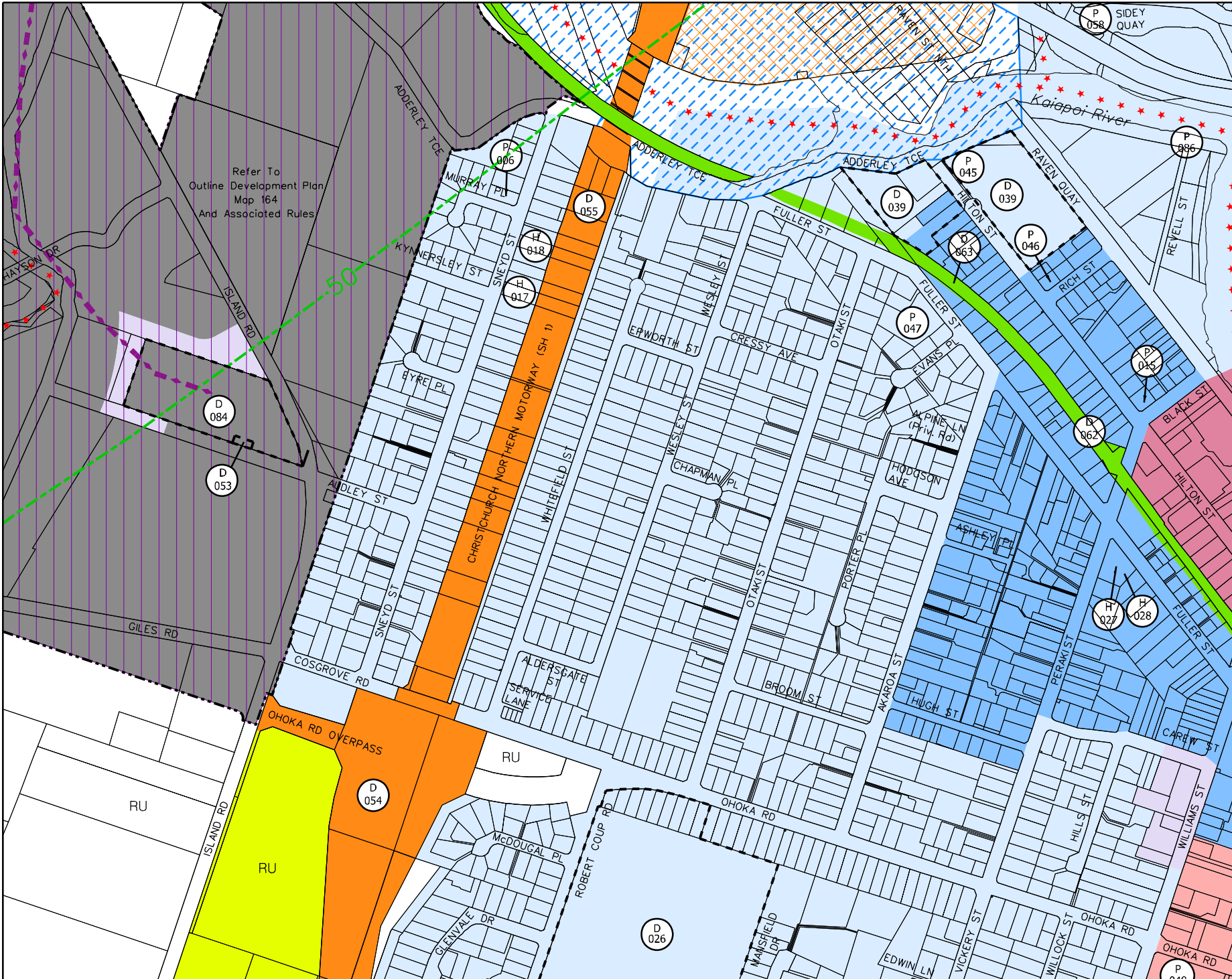


110A	110	111
34	<b>112</b>	113
46	116	117

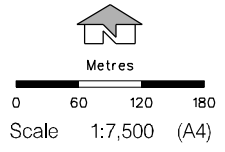
Rangiora  
**112**



Refer To  
Outline Development Plan  
Map 164  
And Associated Rules

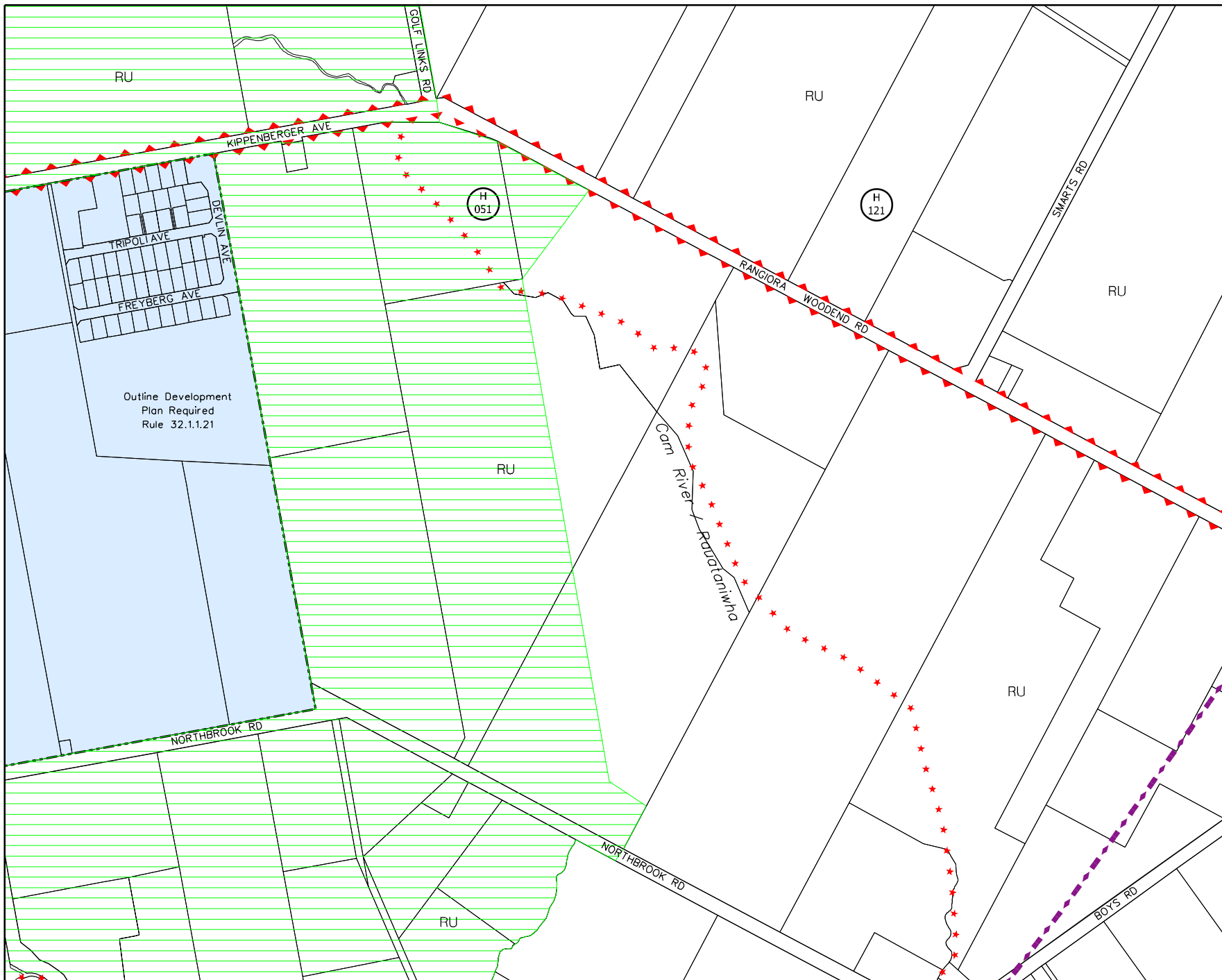


NOTE:  
Disclaimer - refer to map legend sheet

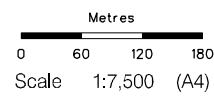


	102	103
58	104	105
68	106	107

Kaipoi  
104

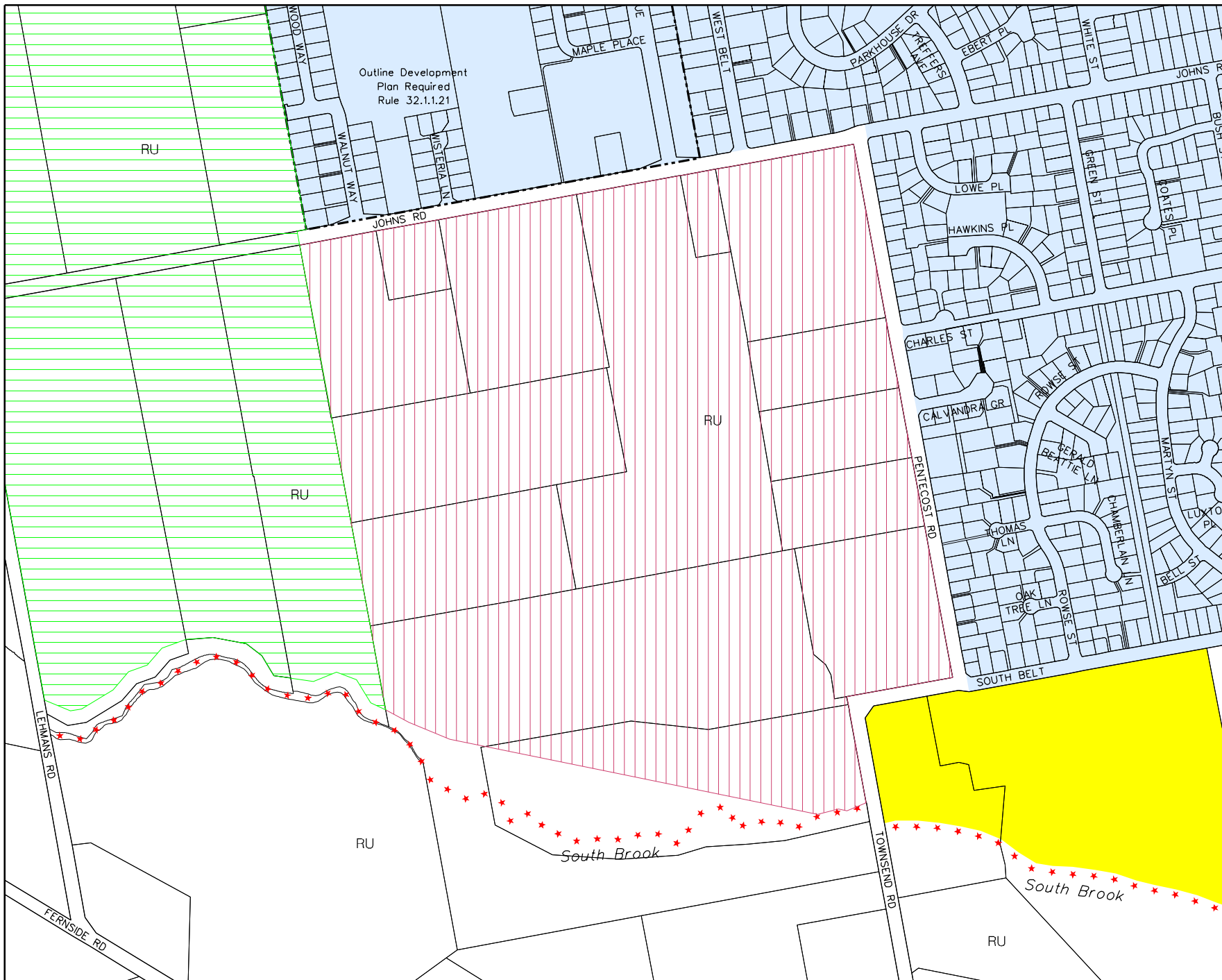


NOTE:  
Disclaimer - refer to map legend sheet

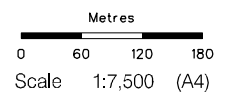


111	35
113	<b>114</b>
117	47
	128A

**Rangiora**  
**114**



NOTE:  
Disclaimer - refer to map legend sheet



34	112	113
	116	117
46		118

Rangiora  
116

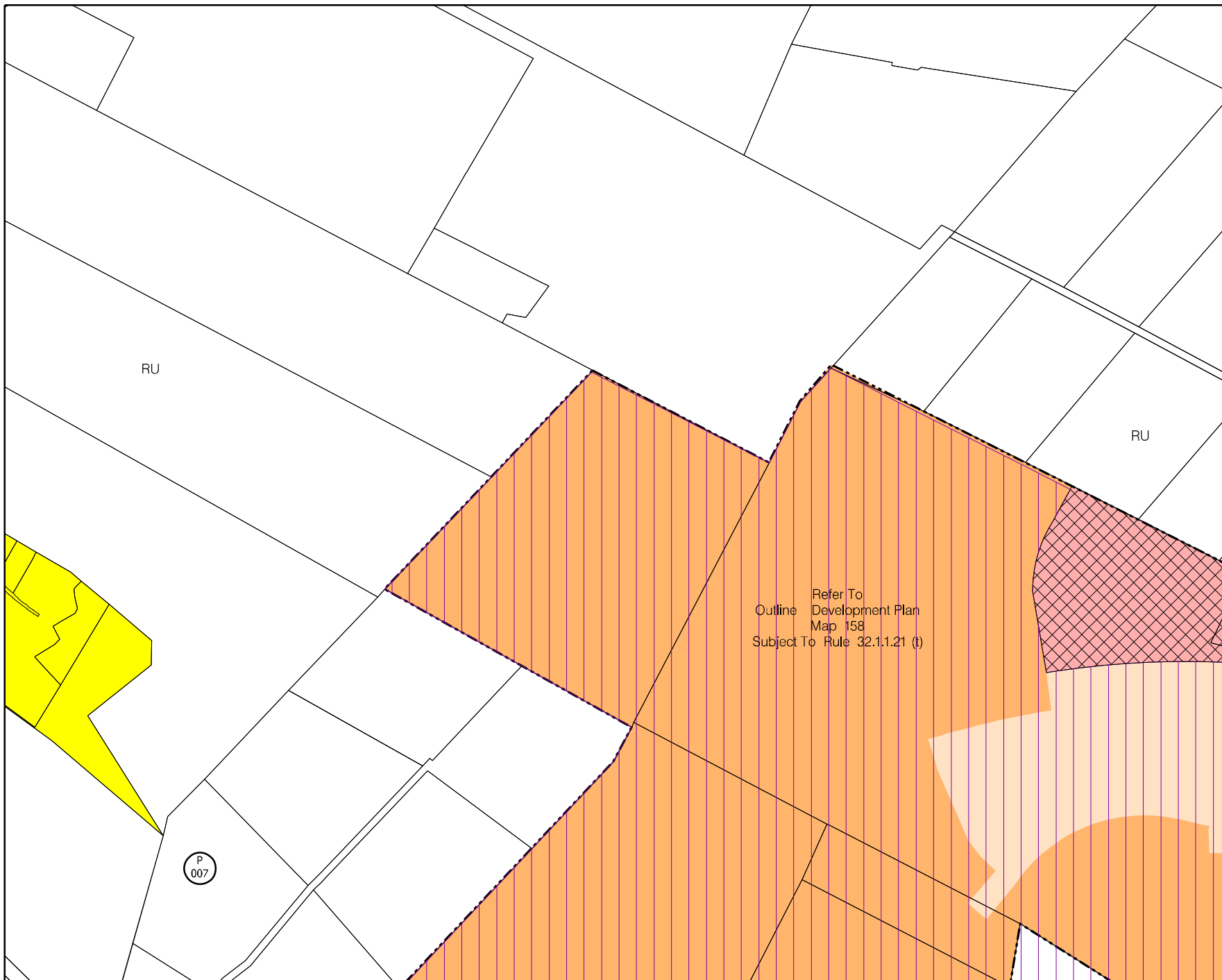












NOTE:  
Disclaimer - refer to map legend sheet



Metres

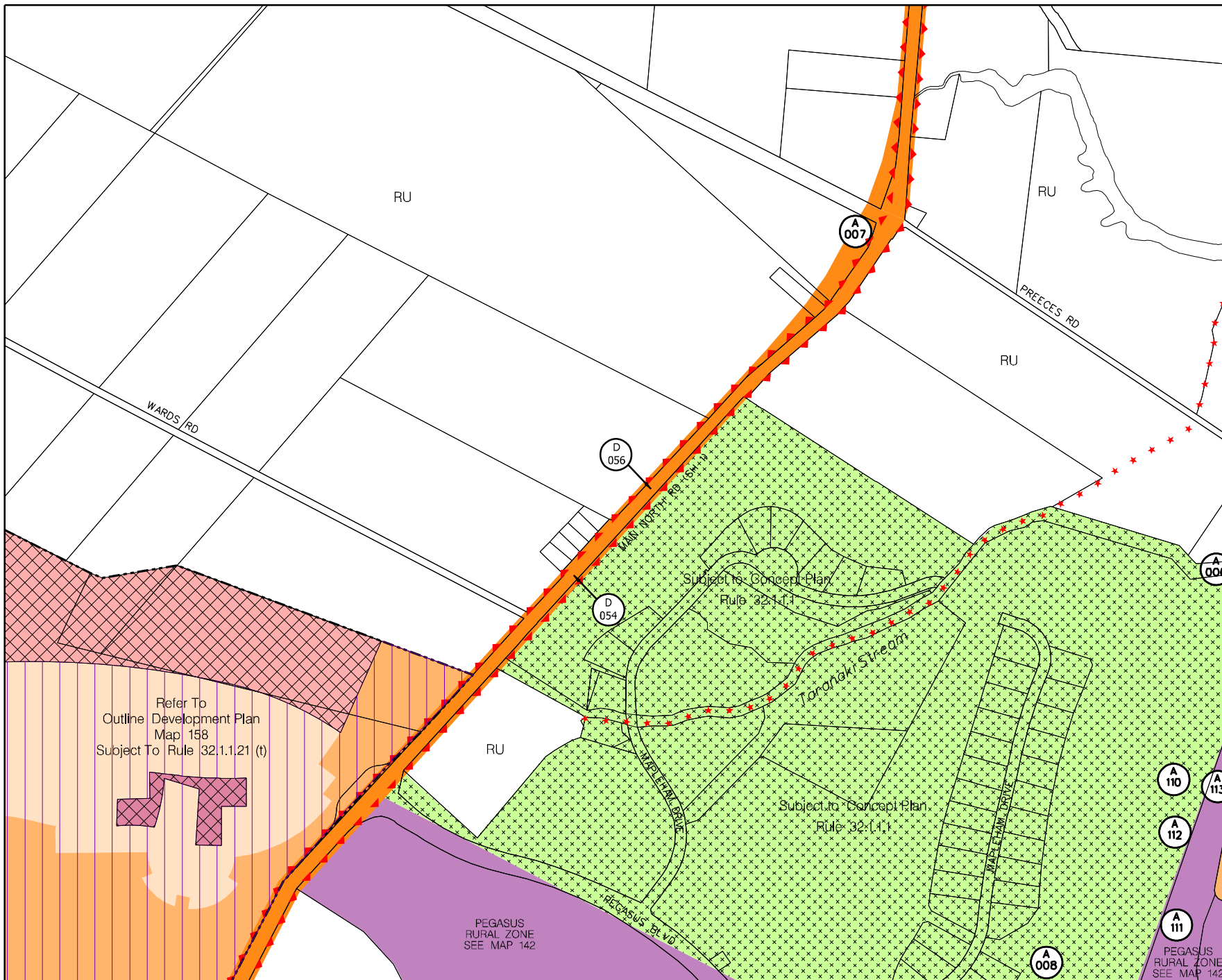
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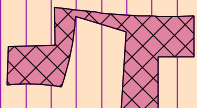
	35		122
115	<b>125A</b>		125
47	128A	128	129

**Mapleham**

**125A**



Refer To  
Outline Development Plan  
Map 158  
Subject To Rule 32.1.1.21 (t)



PEGASUS RURAL ZONE  
SEE MAP 142

PEGASUS RURAL ZONE  
SEE MAP 142

NOTE:  
Disclaimer - refer to map legend sheet

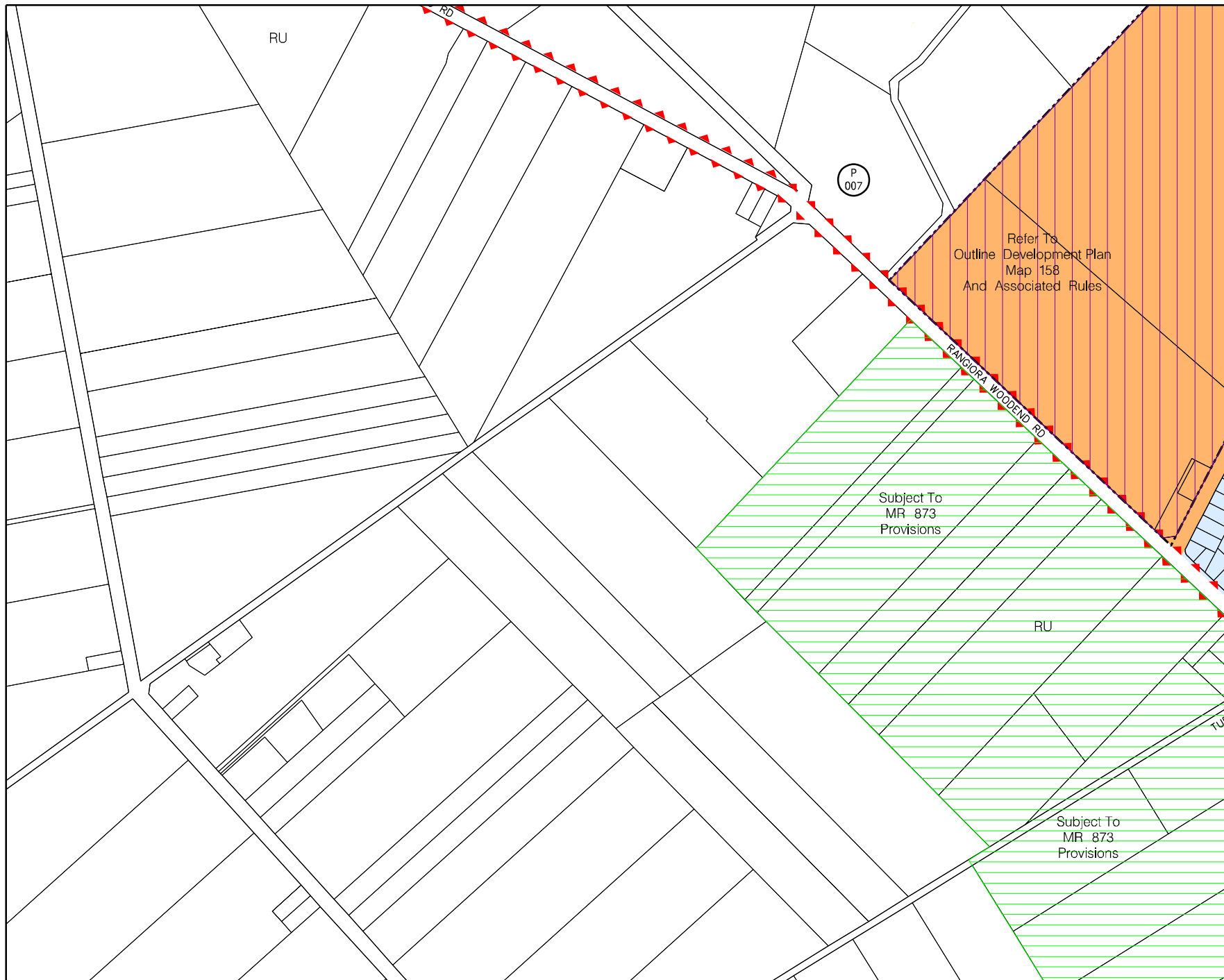


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35	122	123
125A	125	126
128A	128	129

Mapleham

125



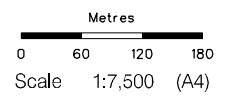
Refer To  
Outline Development Plan  
Map 158  
And Associated Rules

Subject To  
MR 873  
Provisions

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Subject To  
MR 873  
Provisions

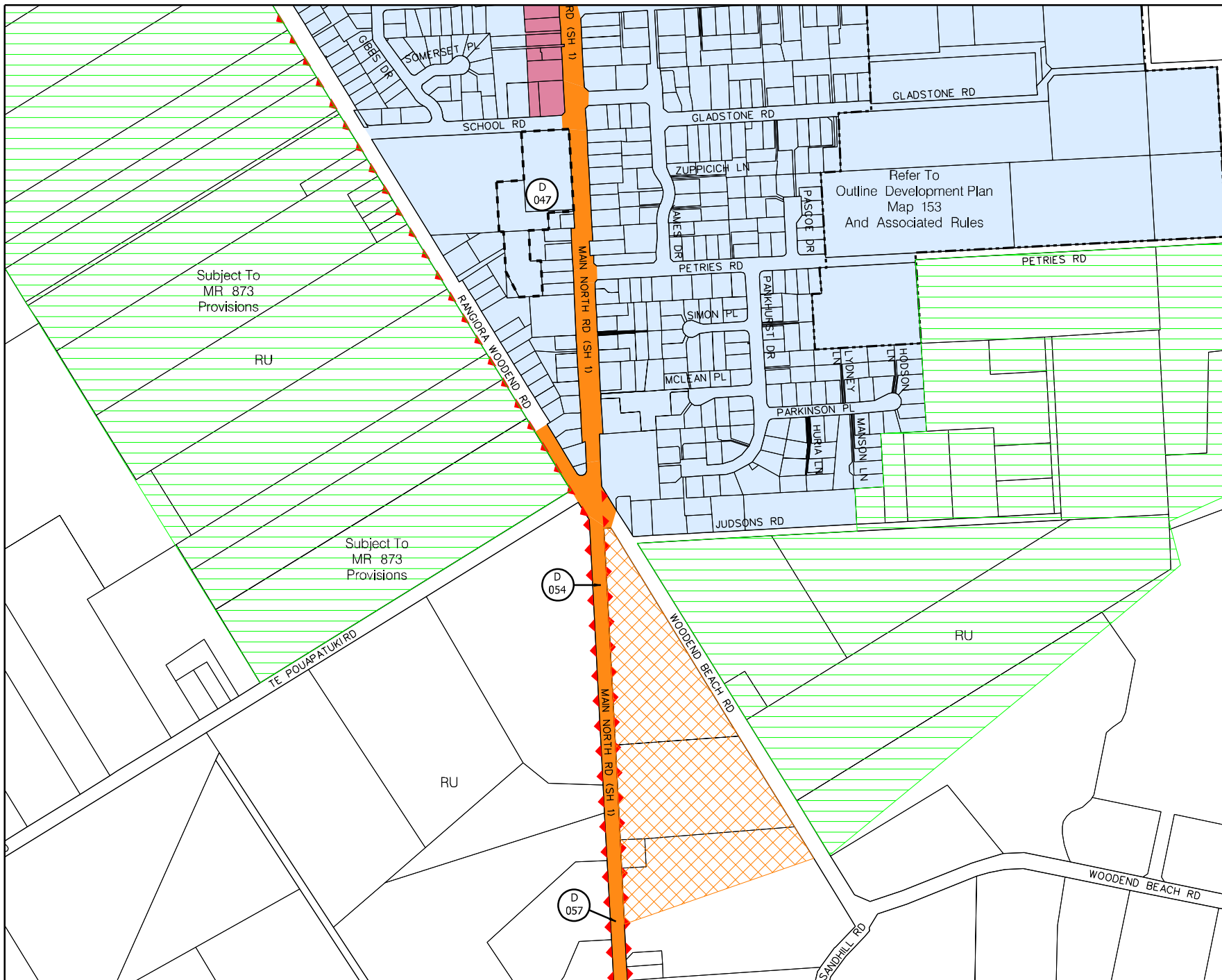
NOTE:  
Disclaimer - refer to map legend sheet



114	115	125A	125
		<b>128A</b>	128
47		132	131

Woodend  
**128A**





NOTE:  
Disclaimer - refer to map legend sheet



Metres  
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Scale 1:7,500 (A4)

128A	128	129
47	131	
132		48
47		

Woodend  
131





## Appendix 5: Amendments to the Selwyn District Plan

The proposed changes to the Selwyn District Plan set out in this Appendix cover the following:

- Amendments to changes that have been made to the Selwyn District Plan (via Plan Change 7) which relate to the Rolleston and Lincoln priority areas, including:
  - Amending provisions to avoid adverse effects from erecting buildings or structures on land prone to liquefaction;
  - Removal of staging requirements for Living Z zones in Rolleston and Lincoln relating to infrastructure;
  - Amending Rules relating to Site Coverage in Medium Density housing, Fencing and the Allotment sizes contained in the Living Z zone areas in Rolleston and Lincoln; and
  - Amending the existing Outline Development Plan for ODP Area 6 in Rolleston (Appendix 38) and clarify the density targets contained in ODPs for Rolleston and Lincoln (as shown in Appendix 37 and 38);
- Re-zoning of land for Prebbleton priority areas including:
- Rezoning 72 hectares of land within Prebbleton to Living Z or Living 1A;
  - Inclusion of ODPs and Policy support for each of the existing zoned land or priority areas rezoned;
  - Inclusion of a new Living Z zone in Prebbleton; and
  - Changing the relevant planning maps to illustrate these areas; and
- Amendments to other planning maps to identify the priority areas set out in Map A of Chapter 6 of the RPS (Appendix 2 of the Land Use Recovery Plan). These areas identify where rebuilding and development should and should not occur before 2028.



## **The Need for Change for Recovery Purposes**

### **Prebbleton**

The Prebbleton priority areas identify and re-zone land for residential development within the township of Prebbleton. The re-zonings provide for additional households to enable development as part of the earthquake recovery response. Provisions within the District Plan intend to manage the extent and layout of development via the Outline Development Plans included for each of the areas. Re-zoning of the Prebbleton priority areas applies existing and new District Plan provisions taking into account previous infrastructure and planning assessments.

### **Amendments to District Plan provisions included via Plan Change 7 to the Selwyn District Plan**

In October 2011, the Selwyn District Council rezoned 810 hectares of land to a new Living Z zone within land now identified as priority areas in Rolleston and Lincoln. These areas provide for over 8,800 households which enable new areas of residential development as part of the earthquakes recovery response. As a result of Chapter 6 being inserted into the Canterbury Regional Policy Statement, some minor amendments need to be made to provisions included in the Selwyn District Plan which were inserted by Plan Change 7.

In addition, as development has been occurring within these priority areas over the last 18 months, minor amendments need to be made to certain provision insert by Plan Change 7 to clarify and resolve issues that have been identified by developers and Council.

## Changes to the Selwyn District Plan Township Volume

Delete A4.5 Townships and Zones – Table A4.4 – Descriptions of Townships Zones (applies to Living Z only) and add new A4.5 Townships and Zones – Table A4.4 – Descriptions of Townships Zones (applies to Living Z only) to read as follows:

### Living Z

**New urban growth areas within or adjacent to the edge of existing townships. These areas are to be subject to an Outline Development Plan to ensure that good standards of urban design and connectivity with existing townships are achieved. The Living Z zone provides for a range of site sizes and living options, including lower density suburban areas and medium density small sections and townhouses. The Living Z zones, where an ODP is not operative in the Plan, are deferred until such time as an Outline Development Plan for the area is made operative in the District Plan. Where the inclusion of an operative ODP is the only reason for the deferral, the deferral will be considered to be lifted upon an ODP becoming operative within the Plan.**

**Medium Density areas shown on an Outline Development Plan are subject to the Medium Density rules, and any business areas shown on an Outline Development Plan are subject to the Business 1 rules.**

Delete B1 Natural Resources Policy B1.1.7 and add new B1 Natural Resources, Policy B1.1.7 to read as follows:

### Policy B1.1.7

**Avoid adverse effects from erecting buildings or structures on unstable land or land that is prone to liquefaction.**

Delete B4 Growth of Townships, Policy B4.1.1, Objective B4.3.3, B4.3.5 and B4.3.6, Policy B4.3.1, Policy B4.3.7 – B4.3.9, Policy B4.3.56 and Policy B4.3.76 and add new B4 Growth of Townships, Policy B4.1.1, Objective B4.3.3, B4.3.5 and B4.3.6, Policy B4.3.1, Policy B4.3.7 – B4.3.9, Policy B4.3.56, Policy B4.3.68 and Policy B4.3.76 to read as follows:

### Policy B4.1.1

- A) **Provide for a variety of allotment sizes for erecting dwellings in the Living 1 Zones, while maintaining average section size similar to that for existing residential areas in townships, except within the Living Z Zone, including any Medium Density area identified in an Outline Development Plan where a higher density of development is anticipated.**
- B) **Facilitate development in the Living 1A6 Zone in Prebbleton where it is consistent with the density provisions of the Regional Policy Statement, and is compatible with the form and character of development in adjacent living zones, with a particular emphasis on maintaining**

**residential lots of not less than 1000m<sup>2</sup> along the common boundary of the Kingcraft Drive Existing Development Area.**

### **Objective B4.3.3**

**For townships within the Greater Christchurch area, new residential or business development is to be provided within existing zoned land or priority areas identified in the Regional Policy Statement and such development is to occur in general accordance with an operative Outline Development Plan.**

### **Objective B4.3.5**

**Ensure that sufficient land is made available in the District Plan to accommodate additional households in the Selwyn District portion of the Greater Christchurch area between 2013-2028 through both Greenfield growth areas and consolidation within existing townships.**

### **Objective B4.3.6**

**Ensure that subdivision and development in Living Z zoned areas achieves an average net density over an Outline Development Plan area of at least ten household units per hectare.**

### **Policy B4.3.1**

**Ensure new residential, rural residential or business development either:**

- **Complies with the Plan policies for the Rural Zone; or**
- **The land is rezoned to an appropriate Living Zone that provides for rural-residential development (as defined within the Regional Policy Statement) in accordance with an Outline Development Plan incorporated into the District Plan; or**
- **The land is rezoned to an appropriate Living or Business zone and, where within the Greater Christchurch area, is contained within existing zoned land and priority areas identified in the Regional Policy Statement and developed in accordance with an Outline Development Plan incorporated into the District Plan.**

### **Policy B4.3.7**

**Living Z urban growth areas identified in the District Plan shall not be developed for urban purposes until an operative Outline Development Plan for that area has been included within the District Plan. Each Outline Development Plan shall:**

- **Be prepared as a single plan for any identified Outline Development Plan area identified on the Planning Maps and Appendices;**
- **Be prepared in accordance with the matters set out in Policy B4.3.8;**
- **Take account of the Medium Density and Subdivision Design Guides.**

## Policy B4.3.8

### Each Outline Development Plan shall include:

- Principal through roads, connection and integration with the surrounding road networks, relevant infrastructure services and areas for possible future development ;
- Any land to be set aside for
  - community facilities or schools;
  - parks and land required for recreation or reserves;
  - any land to be set aside for business activities;
  - the distribution of different residential densities;
  - land required for the integrated management of water systems, including stormwater treatment, secondary flow paths, retention and drainage paths;
  - Land reserve or otherwise set aside from development for environmental or landscape protection or enhancement and;
  - land reserved or otherwise set aside from development for any other reason, and the reasons for its protection.
- Demonstrate how each ODP area will achieve a minimum net density of at least 10 lots or household units per hectare ;
- Identify any cultural (including Te Taumutu Rununga values), natural, and historic or heritage features and values and show how they are to be enhanced or maintained;
- Indicate how required infrastructure will be provided and how it will be funded;
- Set out the phasing and co-ordination of subdivision and development in line with the phasing shown on the Planning Maps and Appendices;
- Demonstrate how effective provision is made for a range of transport options, including public transport systems, pedestrian walkways and cycleways, both within and adjoining the ODP area;
- Show how other potential adverse effects on and/or from nearby existing or designated strategic infrastructure (including requirements for designations, or planned infrastructure) will be avoided, remedied or appropriately mitigated;
- Show how other potential adverse effects on the environment, the protection and enhancement of surface and groundwater quality, are to be avoided, remedied or mitigated;

- Include any other information which is relevant to an understanding of the development and its proposed zoning;
- Demonstrate that the design will minimise any reverse sensitivity effects.

### Policy B4.3.9

The phasing of any Living Z Zone shown on the Planning Maps and Appendices occurs as follows:

- In Rolleston 11 Living Z areas have been identified, and an Outline Development Plan for six of these areas have been incorporated into the District Plan. The remaining five Living Z ODP Areas have been deferred. The deferment for these areas shall be lifted once an operative Outline Development Plan for that area has been incorporated into the District Plan.
- In Lincoln six Living Z areas have been identified, and an Outline Development Plan for each area incorporated into the District Plan. A Business 2 Zone has also been identified with an operative ODP included in the District Plan.

### Policy B4.3.56

Ensure that new Greenfield urban growth only occurs within the Outline Development Plan areas identified on the Planning Maps and Appendices, and in accordance with the phasing set out in Policy B4.3.9.

### Page B4-076

#### Prebbleton

#### Preferred Growth Option

The first preferred areas for expansion of Prebbleton are east and west of Springs Road, between the north and south limits of the existing Living and Business zones as identified in Appendix 31, inclusive of the additional 'Greenfield' development sites that are zoned Living Z and identified in the Outline Development Plans contained in Appendix 41.

### Policy B4.3.68

Ensure that development within each of the Living Z zone Outline Development Plan areas identified on the Planning Maps and Appendices within Prebbleton address the specific matters relevant to each ODP Area number listed below:

#### Prebbleton

#### Outline Development Area 1

- Provision of a primary loop road linking up to two access points off Blakes Road;

- Provision of a connection to The Paddocks subdivision to the north (Living 2A zone);
- Provision of pedestrian and cycle links through the ODP area to connect with the adjoining urban area to the west and east via Blakes Road;
- Provision of a comprehensive stormwater system that has sufficient capacity for the ODP area;
- Provision of reticulated water supply and wastewater systems that have sufficient capacity for the ODP area;
- Provision of a local neighbourhood park;
- Houses developed along Blakes Road must front this road;
- Provision of a minimum net density of 10 households per hectare averaged over the ODP area;

#### Outline Development Area 2

- Provision of a primary north-south road connection through the area linking from Trents Road to the Cairnbrae subdivision (Living 1A6 Zone);
- Provision of pedestrian and cycle link through the ODP area to connect with the adjoining urban area in the Cairnbrae subdivision (Living 1A6 Zone) to the north and to secure through connections to any future residential development that may establish on the opposite southern side of Trents Road;
- Provision of a comprehensive stormwater system that has sufficient capacity for the ODP area;
- Provision of reticulated water supply and wastewater systems that have sufficient capacity for the ODP area;
- Provision of local neighbourhood park;
- Houses developed along Trents Road must front this road;
- Houses developed along the north-western boundary with the Kingcraft Drive Existing Development Area to be of lower residential densities with appropriate fencing and boundary treatments at the rural residential/urban interface;
- Provision of a minimum net density of 10 households per hectare averaged over the ODP area;

#### Outline Development Area 3

- Provision of a primary north-south road connection through the area linking from Springs Road to Trents Road;
- Provision of a secondary east-west road connection through the area linking from the adjacent Sterling Park subdivision (Living X Zone) to the main north-south primary road connection within this ODP;

- Provision of pedestrian and cycle links within and through the ODP area to connect with the adjoining urban areas, including specifically a connection from Trents Road to the Prebbleton Nature Park;
- Provision of a comprehensive stormwater system that has sufficient capacity for the ODP area;
- Provision of reticulated water supply and wastewater systems that have sufficient capacity for the ODP area;
- Provision of a local neighbourhood park and green links;
- Houses developed along Springs Road and Trents Road must front these roads;
- Provision of a minimum net density of 10 households per hectare averaged over the ODP area;
- Houses developed along the north-western boundary with the Rural (Inner Plains) Zone to contain appropriate fencing and boundary treatments at the rural/urban interface;

#### Outline Development Area 4

- Provision of a primary road connection through the area linking from Station Masters Way to Tosswill Road;
- Provision of a secondary east-west road connection through the area to Prebbleton Central;
- Provision of a local east-west secondary connection through the area to Conductors Road;
- Provision of pedestrian and cycle links within and through the ODP area to connect with the adjoining urban areas;
- Provision of an integrated stormwater management scheme to service the wider catchment, which shall incorporate wetland and riparian margins that form part of an environmentally sustainable solution that enhances ecological, cultural and Te Taumutu Rununga values;
- Interim stormwater management solutions established in advance of the integrated scheme are to have sufficient capacity for the development area;
- Provision of reticulated water supply and wastewater systems that have sufficient capacity for the ODP area;
- Provision of an 'open space corridor' through to area to link up with the Prebbleton Central subdivision and Business 1 Zone, including capacity to support stormwater and pedestrian/cycling links;
- Provision of a local neighbourhood park;
- Houses developed along Tosswill Road must front this road;
- Provision of larger lots of at least 1,000m<sup>2</sup> around the northern and eastern boundaries of the ODP Area, with appropriate perimeter fencing and planting to preserve the rural outlook and Port Hills views;

- Provision of medium density lots along the 'open space corridor', to incorporate appropriate permeable fencing treatments and set backs to promote passive surveillance and integration of housing with the wider reserve network;
- Provision of a minimum net density of 10 households per hectare averaged over the ODP area;

### Policy B4.3.76

Ensure that development within each of the Outline Development Plan areas identified on the Planning Maps and Appendices within Rolleston addresses the specific matters relevant to each ODP Area number listed below:

#### Outline Development Plan Area 1

- Provision of local secondary road connections through the area to Dunns Crossing Road, Brookside Road, and Stonebrook Drive;
- Provision of pedestrian and cycle links within and through the ODP area to connect with the adjoining urban area to the east and south;
- Provision of a reticulated wastewater system with capacity to accommodate necessary flows;
- Potential provision of primary school subject to agreement with Ministry of Education;
- Provision of one Local Centre;
- Provision of a 40m noise abatement zone from State Highway 1;
- Provision of local neighbourhood parks and a key open space corridor along the alignment of the unformed East Maddisons Road. Green pedestrian and cycle linkages are to be provided from the balance of the area to this open space corridor;
- Provision of a mix of housing densities within the area, with medium density housing to be located in close proximity to local parks and the central open space corridor;
- Provision of a minimum net density of 11 households per hectare averaged over the ODP area.

#### Outline Development Plan Area 2

- Vehicle access to be via a secondary road connection from the Markham Way cul-de-sac with no direct vehicle access onto Norman Kirk Drive or Rolleston Drive;
- Provision of wells and water pumping facilities to provide sufficient capacity for all future growth in this area, including main trunk connections where necessary;
- Provision of a reticulated wastewater system with capacity to accommodate necessary flows;



- The stopping of an unformed portion of road ‘Dick Roberts Place’ along the area’s western boundary will need to be addressed through a separate road stopping procedure;
- Identification of the whole of this ODP area as a medium density housing area, apart from a strip adjacent to the existing dwellings to the west where lower density lots are to be located to provide an area of transition between the existing dwellings and higher density units;
- Provision of pedestrian and cycle link between Rolleston School and Markham Way;
- Provision of a minimum net density of 20 households per hectare averaged over the ODP area.

### Outline Development Plan Area 3

- Provision of local east-west secondary road connections through the ODP area to the existing “Park Lane” subdivision;
- No direct vehicle access from State Highway 1 and restricted vehicle access from Levi Road, to maintain the operational status of these primary roads, with lots fronting Levi Road to have an alternative access such as slip lanes;
- Provision of pedestrian and cycle links within and through the ODP area to connect with the adjoining urban area to the west and the rural area to the east;
- Provision of a comprehensive stormwater system that has sufficient capacity for the ODP area;
- Provision of reticulated water supply and wastewater systems that have sufficient capacity for the ODP area;
- Provision of local neighbourhood parks, including a gateway reserve on the southeastern corner of the ODP area adjacent to Levi Road;
- Provision of larger lots of at least 1,000m<sup>2</sup> around the northern, eastern, and southern boundaries of the ODP area;
- Provision of a minimum net density of 10 households per hectare averaged over the ODP area.

### Outline Development Plan Area 4

- Provision of a local secondary road connection to Levi Road and Lincoln Rolleston Road;
- Provision of pedestrian and cycle links within and through the ODP area to connect with the adjoining urban area to the west and the rural area to the east;
- Provision of a comprehensive stormwater system that has sufficient capacity for the ODP area;
- Provision of reticulated water supply and wastewater systems that have sufficient capacity for the ODP area;

- Provision of a minimum net density of 15 households per hectare averaged over the ODP area.

#### Outline Development Plan Area 5

- Provision of District-function recreation and open space facilities;
- Provision of pedestrian and cycle links within and through the ODP area to connect with the adjoining urban area and ODP area 6.

#### Outline Development Plan Area 6

- Provision of a main east-west primary road connection through the area from near the intersection of Goulds Road and East Madison Road to Springston Rolleston Road;
- Provision of two local north-south secondary road connections through the area, to provide connections between ODP area 5 to the north and the primary road noted above and future urban areas to the south;
- Provision of pedestrian and cycle links within and through the ODP area to connect with the adjoining urban area and ODP area 5;
- Provision of a comprehensive stormwater system that has sufficient capacity for the ODP area;
- Provision of reticulated water supply and wastewater systems that have sufficient capacity for the ODP area;
- Provision of one neighbourhood centre;
- Provision of local neighbourhood parks;
- Provision of a mix of low and medium density housing areas with a minimum net density of 12-households per hectare averaged over the ODP area.

#### Outline Development Plan Area 7 (Park Grove)

- Provision of a main east-west primary road connection through the area linking up to two access points off Overbury Crescent;
- Provision of a 40m noise abatement zone from State Highway 1;
- Provision of pedestrian and cycle links within and through the ODP area to connect to adjoining urban areas;
- Provision of a comprehensive stormwater system that has sufficient capacity for the ODP area;
- Provision of reticulated water supply and wastewater systems that have sufficient capacity for the ODP area;
- Provision of a minimum net density of 19 households per hectare averaged over the ODP area.

#### Outline Development Plan Area 8 (Park Lane)

- ODP Area 8 to align with ODP Area 3;
- Provision of main north-south primary road connection from Marlowe Place to Beaumont Drive;
- Provision of three local east-west secondary road connections to ODP Area 3 (including one connect from Shadbolt Drive to ODP Area 3);
- Provision of a 40m building setback from the State Highway 1;
- No direct access off State Highway 1;
- Provision of pedestrian and cycle links within and through the ODP area to connect to adjoining urban areas;
- Provision of a comprehensive stormwater system that has sufficient capacity for the ODP area;
- Provision of reticulated water supply and wastewater systems that have sufficient capacity for the ODP area;
- Provision of local neighbourhood parks;
- Provision of a minimum net density of 10 households per hectare averaged over the ODP area.

#### Outline Development Plan Area 9 (Helpet Park)

- ODP Area 9 to align with ODP Area 11;
- Provision of a local north-south secondary road connection from Lowes Road to ODP Area 11;
- Provision of a local east-west secondary road connection from Springston Rolleston Road to Lincoln Rolleston Road;
- Provision of pedestrian and cycle links within and through the ODP area to connect to adjoining urban areas;
- Provision of a comprehensive stormwater system that has sufficient capacity for the ODP area;
- Provision of reticulated water supply and wastewater systems that have sufficient capacity for the ODP area;
- Provision of a minimum net density of 10 households per hectare averaged over the ODP Area.

#### Outline Development Plan Area 10 (East Maddisons / Goulds Road)

- ODP Area 10 to align with ODP Area 6;

- Provision of main east-west primary road connection from East Maddisons Road to Goulds Road;
- Provision of local east-west secondary road connections from East Maddisons Road to Goulds Road;
- Provision of pedestrian and cycle links within and through the ODP area to connect to adjoining urban areas;
- Provision of a comprehensive stormwater system that has sufficient capacity for the ODP area;
- Provision of reticulated water supply and wastewater systems that have sufficient capacity for the ODP area;
- Provision of a minimum net density of 10 households per hectare averaged over the ODP Area.

#### Outline Development Plan Area 11 (Branthwaite Drive)

- ODP Area 11 to align with ODP Area 9;
- Provision of main north-south primary road connection from ODP Area 9 linking up with Branthwaite Drive and to future development to the south of ODP Area 11;
- Provision of main east-west primary road connection from Springston Rolleston Road to Lincoln Rolleston Road;
- Provision of two local east-west secondary road connections from Springston Rolleston Road to Lincoln Rolleston Road;
- Provision of pedestrian and cycle links within and through the ODP area to connect to adjoining urban areas;
- Provision of a comprehensive stormwater system that has sufficient capacity for the ODP area;
- Provision of reticulated water supply and wastewater systems that have sufficient capacity for the ODP area;
- Potential provision of two Local Centres;
- Potential provision of one partial Neighbourhood Centre;
- Provision of local neighbourhood parks;
- Provision of a minimum net density of 14 households per hectare averaged over the ODP Area;

Delete C4 Living Zone Rules - Building, Table C4.1 Site Coverage Allowance – (Living Z only) and add new C4 Living Zone Rules - Building, Table C4.1 Site Coverage Allowance – (Living Z only) to read as follows:

## C4 Living Zone Rules – Building, 4.7 – Building and Site Coverage

### Table C4.1 Site Coverage Allowances

<u>Living Z</u>	<u>Including Garage</u>	<u>35%</u>
	<u>Excluding Garage</u>	<u>35% - 36m<sup>2</sup></u>
	<u>Medium Density</u>	<u>Including garage 40%</u> <u>Excluding garage 40% - 18m<sup>2</sup></u> <u>Where a site is located in a Medium Density area and forms part of a comprehensive residential development of four or more adjoining lots less than 350m<sup>2</sup> in size, the maximum site coverage shall be 45% and shall be calculated across the area of the entire comprehensive residential development, excluding any undeveloped balance lot.</u>

Delete Rule 4.7.3.3 and 4.7.4.1 and add new Rule 4.7.3.3 and 4.7.4.1 to read as follows:

### Rule 4.7.3.3 and 4.7.4.1 – Building and Site Coverage

4.7.3.3 The site is located in a Living Z Medium Density area located within an Outline Development Plan and the maximum area of the site occupied by a building(s) is:

- (a) 45% - including a garage; or
- (b) 45% - 18m<sup>2</sup> - excluding a garage; or
- (c) part of a comprehensive residential development of four or more adjoining lots under 350m<sup>2</sup> in size, in which case the maximum site coverage shall be 50% and shall be calculated across the area of the entire comprehensive residential development, excluding any undeveloped balance lot.

4.7.4.1 The number of sites in the street or subdivision where site coverage already exceeds 35%, except in Medium Density areas within Living Z Zones where site coverage already exceeds 40%.

Delete Rule 4.9.23 and add new rule 4.9.23 to read as follows:

### Living Z Medium Density areas located within an Outline Development Plan

**4.9.23 No set back is required for any garage or accessory building from an internal boundary, provided that the total length of garages or accessory buildings adjacent to the internal boundary do not exceed 7m and provided those garages or accessory buildings comply with a 45 degree recession plane measured from 2.5m above ground level at the boundary, except when the site is on the boundary of a low density area or another Living zone, in which case Rule 4.9.2 applies.**

Delete Rule 4.13.1, 4.13.2 4.13.3 and 4.13.5 and add new Rule 4.13.1, 4.13.2 and 4.13.3 to read as follows:

### Rule 4.13 Buildings and Streetscene

#### Permitted Activities — Buildings and Streetscene

**For all residential development located within the Lowes Road Outline Development Plan area (Appendix 34) or a Living Z zone**

**4.13.1 That any fence located between the front façade of the dwelling and the street boundary or Private Right of Way or shared access over which an allotment has legal access which is parallel or generally parallel to that boundary shall be a maximum height of 1 metre. For allotments with frontage to more than one road, any fencing on the secondary road boundary is to be no higher than 1.8m.**

**4.13.2 Any other fence shall be a maximum height of 1 metre if it is located within 3 metres of the street boundary or Private Right of Way or shared access over which allotment has legal access.**

#### Restricted Discretionary Activities - Buildings and Streetscene

**4.13.3 Any activity which does not comply with Rule 4.13.1 shall be a restricted discretionary activity.**

Add new rule 4.17.1, 4.17.2, 4.17.2.1, 4.17.2.2, 4.17.2.3 and 4.17.2.4 to read as follows:

### Rule 4.17 Fences Adjoining Reserves

#### Permitted Activities – Fences Adjoining Reserves

**4.17.1 For all development located within the Living Z zone that shares a boundary with a reserve or walkway, shall be limited to a single fence erected within 5m of any**

Council reserve that is at least 50% visually transparent where it exceeds 1.2m in height (which shall be applied to the whole fence in its entirety).

## **Restricted Discretionary Activities – Fences Adjoining Reserves**

**4.17.2** Any activity which does not comply with Rule 4.17.1 shall be a restricted discretionary activity. Council shall restrict the exercise of its discretion to the following:

**4.17.2.1** The extent to which the proposed fencing promotes passive surveillance to reduce the fear and incidence of crime.

**4.17.2.2** The extent to which the fencing design and materials compliment the open space amenity of the reserve.

**4.17.2.3** The extent to which the orientation of the section and aspect of the outdoor living areas within the section is able to reduce the effects of the non-complying fence on the open space amenity of the adjoining reserve.

**4.17.2.4** The need to avoid adverse cumulative effects arising from the number of non-complying fences being established along a reserve boundary and the extent to which the incremental reduction of the open space amenity of the reserve is mitigated through appropriate fencing design and construction materials and the layout of future dwellings and yard space.

Delete C12 Living Zone Rules – Subdivision, Table C12.1 – Allotment Sizes (Lincoln - Living Z zone only) and add new C12 Living Zone Rules – Subdivision, Table C12.1 – Allotment Sizes (Lincoln - Living Z zone only) to read as follows:

## **C12 Living Zone Rules – Subdivision, Table C12.1 – Allotment Sizes**

<b>Township</b>	<b>Zone</b>	<b>Average Allotment Size Not Less Than</b>
<b><u>Lincoln</u></b>	<b><u>Living Z</u></b>	<b><u>Low Density: Average allotment size of 600m<sup>2</sup> and a minimum individual allotment size of 500m<sup>2</sup></u></b> <b><u>Medium Density: Maximum average allotment size of 450m<sup>2</sup>, with a minimum individual allotment size of 350m<sup>2</sup></u></b>  <b><u>Medium Density (Comprehensive): Maximum average allotment size 350m<sup>2</sup>, with no minimum site size.</u></b> <ul style="list-style-type: none"><li>- <b><u>Comprehensive residential development will be identified by a consent notice on the subdivision consent and will be located within Medium Density areas as identified on the ODPs – Appendix 37</u></b></li></ul>

- Within a comprehensive residential development, a section 224 certificate shall only be issued following the erection (to the extent that the exterior is fully closed in) of the dwellings that are to be subdivided.

Overall development within an ODP plan area shall achieve the net density target contained in the relevant ODP plan shown in Appendix 37 of the Township volume of the District Plan.

Add new C12 Living Zone Rules – Subdivision, Table C12.1 – Allotment Sizes (Prebleton - Living Z and Living 1A zone only) to read as follows:

Township	Zone	Average Allotment Size Not Less Than
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Prebleton	Living Z	<u>Low Density: Average allotment size of 700m<sup>2</sup> with a minimum individual allotment size of 550m<sup>2</sup></u>
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Medium Density: Maximum average allotment size of 450m<sup>2</sup>, with a minimum individual allotment size of 350m<sup>2</sup>

Medium Density (Comprehensive): Maximum average allotment size 350m<sup>2</sup>, with no minimum site size.

- Comprehensive residential development will be identified by a consent notice on the subdivision consent and will be located within Medium Density areas as identified on the ODPs – Appendix 41
- Within a comprehensive residential development, a section 224 certificate shall only be issued following the erection (to the extent that the exterior is fully closed in) of the dwellings that are to be subdivided.

Overall development within an ODP plan area shall achieve the net density target contained in the relevant ODP plan shown in Appendix 41 of the Township volume of the District Plan.

<u>Living 1A</u>	<u>Area A: 1,250m<sup>2</sup>;</u> <u>Area B: 1,000m<sup>2</sup>;</u> <u>Area C: 800m<sup>2</sup></u>
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In all cases development shall proceed in accordance with the ODP contained in Appendix 19 and shall achieve a minimum net density of 8hh/ha once the entire site has been developed.

2,000m<sup>2</sup> shall apply to the balance of the zone



Delete C12 Living Zone Rules – Subdivision, Table C12.1 – Allotment Sizes (Rolleston - Living Z zone only) and add new C12 Living Zone Rules – Subdivision, Table C12.1 – Allotment Sizes (Rolleston - Living Z zone only) to read as follows:

<b>Township</b>	<b>Zone</b>	<b>Average Allotment Size Not Less Than</b>
<b><u>Rolleston</u></b>	<b><u>Living Z</u></b>	<b><u>Low Density: Average allotment size of 650m<sup>2</sup> with a minimum individual allotment size of 550m<sup>2</sup></u></b>  <b><u>Medium Density: Maximum average allotment size of 450m<sup>2</sup>, with a minimum individual allotment size of 350m<sup>2</sup></u></b>  <b><u>Medium Density (Comprehensive): Maximum average allotment size 350m<sup>2</sup>, with no minimum site size.</u></b> <ul style="list-style-type: none"><li>- <b><u>Comprehensive residential development will be identified by a consent notice on the subdivision consent and will be located within Medium Density areas as identified on the ODPs – Appendix 38</u></b></li><li>- <b><u>Within a comprehensive residential development, a section 224 certificate shall only be issued following the erection (to the extent that the exterior is fully closed in) of the dwellings that are to be subdivided.</u></b></li></ul> <b><u>Overall development within an ODP plan area shall achieve the net density target contained in the relevant ODP plan shown in Appendix 38 of the Township volume of the District Plan.</u></b>

Delete Rule 12.1.7.6 and add new Rule 12.1.7.6 to read as follows:

#### **Rule 12.1.7.6 Non Complying Activities – Subdivision – General**

**12.1.7.6 Any subdivision within a Living Z (deferred) Zone shown on the Planning Maps shall be a non-complying activity where it does not comply with the provisions of the Rural (Inner Plains) Zone.**

Within Appendix 19, delete ODPs entitled “Selwyn District Council Outline Development Plan Shaw Appeal 27/04/06” and “Selwyn District Council Outline Development Plan Fowler and Troy Enterprises 27/04/06” and add new ODP Area 5 (as per attachment 1):

Within Appendix 37 Outline Development Plan – Lincoln, for Outline Development Area 2, delete the first sentence under Density Plan and add in the following text to read as follows:

#### **DENSITY PLAN**

**The ODP Area shall achieve a minimum net density of 10 households per hectare.**

Within Appendix 37 Outline Development Plan – Lincoln, for Outline Development Area 3, delete the first sentence under Density Plan and add in the following text to read as follows:

#### **DENSITY PLAN**

**The ODP area shall achieve a minimum net density of 10 households per hectare.**

Within Appendix 37 Outline Development Plan – Lincoln, for Outline Development Area 4, delete the first and second sentence under Density Plan and add in the following text to read as follows:

#### **DENSITY PLAN**

**The Area 4 Outline Development Plan area will be comprised entirely of conventional residential development in accordance with the Lincoln Structure Plan. The ODP area shall achieve a minimum net density of 10 households per hectare.**

Within Appendix 37 Outline Development Plan – Lincoln, for Outline Development Area 6, delete the text under Density Plan and add in the following text to read as follows:

#### **DENSITY PLAN**

**The site is rectangular and is 32m wide by 230m in length. Access to the site will be by a new road off North Belt which will provide access of the western boundary of the site. The layout of the sections will be undertaken to maximise sunlight and warmth. The lot sizes will provide for medium density housing and will be laid out to achieve combined accessways for back sections. The ODP area shall achieve a minimum net density of 15 households per hectare.**

**The higher intensity residential development incorporating smaller lots will be directly opposite the Lincoln Events Centre and Lincoln Domain. The site is also close to the local Primary and High Schools and the Lincoln Shopping Centre. The site backs on to Roblyn Place. A 5m building setback,**

**together with a 12m setback for two-storey buildings, is shown adjacent to this ODP boundary, in order to reduce any privacy, shading and outlook effects on the adjoining Roblyn Place residents**

Within Appendix 38 Outline Development Plan – Rolleston, for Outline Development Area 1,  
Delete the first sentence under Density Plan and add in the following text to read as follows:

#### **DENSITY PLAN**

**The ODP area shall achieve a net density of 11 households per hectare, based on a net area of approximately 57.5 hectares.**

Within Appendix 38 Outline Development Plan – Rolleston, for Outline Development Area 2,  
Delete the second sentence under Residential Density and add in the following text to read as follows:

**The ODP area shall achieve a net density of 20 households per hectare.**

Within Appendix 38 Outline Development Plan – Rolleston, for Outline Development Area 3,  
Delete the text under Density Plan and add in the following text to read as follows:

**The ODP area shall achieve a minimum net density of 10 households per hectare (with a minimum of 484 households). Lots along the rural periphery and Levi Road have an area greater than 1000m<sup>2</sup> and in total, the average lot size shown on the ODP is 750m<sup>2</sup>.**

Within Appendix 38 Outline Development Plan – Rolleston, for Outline Development Area 6,  
Delete the ODP and text and add in the new ODP and text as per attachment 2:

Within Appendix 38 Outline Development Plan – Rolleston, for Outline Development Area 7,  
Delete the text under Residential Plan and add in the following text to read as follows:

**The ODP provides for a Master Planned high density development with individual allotments averaging approximately 400 m<sup>2</sup> per lot.**

**Each individual site will have a house specifically designed which will form part of the Resource Consent process to enable the site to comply with all required setbacks, recession planes and site coverage provided in the District Plan. The ODP area shall achieve a net density of 19.19 households per hectare on a total area of 3.596 ha.**

Within Appendix 38 Outline Development Plan – Rolleston, for Outline Development Area 8,  
Delete the text under Density Plan and add in the following text to read as follows:

**The ODP area shall achieve a minimum of 10 household lots per hectare. The density plan has been calculated taking into account the multiple ownership that currently exists throughout the total ODP area.**

Add new Appendix 41 Outline Development Plan – Prebbleton as per Attachment 3:

### Selwyn District Plan Maps

Delete Sheets 1 and 2 of Planning Maps 014, 125, 126, 127, 128 and 129 and add new Sheets 1 and 2 of Planning Maps 014, 125, 126, 127, 128 and 129 to rezone Areas 1 through to 4 in Prebbleton identified in attachment 4 to a Living Z zone

Delete Sheets 1 and 2 of Planning Maps 113, 114, 116, 119, 120, 121 and 123 and add new Sheets 1 and 2 of Planning Maps 113, 114, 116, 119, 120, 121 and 123 to rezone land from Living Z Deferred to Living Z as an Outline Development Plan has already been included in the District Plan.

Delete Sheet 1 of Planning Maps 008, 009, 013, 014, 095, 096, 097, 098, 099, 101, 102, 103, 104, 105, 107, 108, 109, 110, 111, 113, 114, 115, 116, 117, 118, 119, 120, 121, 122, 123, 125, 126, 127, 128, 129 and 130 and add new Sheet 1 of Planning Maps 008, 009, 013, 014, 095, 096, 097, 098, 099, 101, 102, 103, 104, 105, 107, 108, 109, 110, 111, 113, 114, 115, 116, 117, 118, 119, 120, 121, 122, 123, 125, 126, 127, 128, 129 and 130 to identify where rebuilding and development should and should not occur before 2028.

# **Attachment 1**

## **Living 1A zone ODP**

PART E

## APPENDIX 19

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### **LIVING 1A ZONE OUTLINE DEVELOPMENT PLAN – PREBBLETON**

## LIVING 1A ZONE OUTLINE DEVELOPMENT PLAN

### INTRODUCTION

This Outline Development Plan (ODP) applies to the undeveloped balance of the Living 1A zone in Prebbleton. The ODP covers 15.02ha of land comprising seven separate properties that either front or have legal access to either Toswill Road or Trices Road. The development block is contained by the Prebbleton Domain to the north-west, Rural (Inner Plains) zone to the north-east and south, and the Stonebridge Way development (being the developed portion of the Living 1A zone) to the west.

The ODP provides an overarching urban design framework to guide the future development of the land. The ODP includes Land Use, Movement, Green and Blue Networks and incorporates the wider strategic and community outcomes expressed in the Prebbleton Structure Plan.

### URBAN DESIGN

The design principles that underpin this ODP are in line with the New Zealand Urban Design Protocol and accord with the Selwyn District Council Subdivision Design Guide (September 2009). The following environmental outcomes are to be achieved:

- Development that meets the District Plan policies, realises an overall increase in residential density, applies urban consolidation principles and assists in achieving a compact concentric settlement pattern for Prebbleton.
- Provision for a range of section sizes and housing typologies to respond to the wider needs of the community, whilst achieving the prescribed minimum household densities and minimum average allotment sizes.
- Subdivision layouts that integrate with adjoining neighbourhoods and incorporate existing land uses where appropriate. The wider context of the development area should influence the subdivision layout by protecting and enhancing cultural, ecological, heritage and Te Taumutu values and existing built features, such as amenity trees and water races.
- Layouts and urban design treatments that create a distinguishable sense of place, assist in enhancing the wider character and amenity of Prebbleton and deliver safe, vibrant and healthy living environments. Layouts should apply Crime Prevention through Environmental Design (CPTED) design principles.
- Integrated and legible road hierarchy that supports safe and efficient connections and promotes walking and cycling. Road design and landscape treatments should contribute to the overall character of Prebbleton and assist in connecting residential development with open space reserves and other public assets and services within the township, such as the Domain, Primary School, Nature Park and the town centre.
- Sustainable methods to treat and dispose of stormwater that protect groundwater resources and surface waterways (including springs, water/stock races) from contamination, while integrating with open space and reserves where appropriate.
- Installation of all the necessary infrastructure services within the ODP area, and the cost effective and efficient connection of those services to the wider network.

### LAND USE

The ODP Area shall achieve a minimum net density of 8 households per hectare.

Dwellings must front Trices Road and Toswill Road to enhance passive surveillance and safety, while preserving the semi-rural streetscape. Appropriate design layouts should take into consideration the shape, orientation and aspect of sections, with internal roads supporting access that avoids housing from backing onto Trices Road and Toswill Road. An exception is made for the parcels that are affected by the limited access requirements applied to the Trices Road and Toswill Road intersection, which will need to be accessed from the internal road network.

The ODP supports three different densities, which respond to the context of the site and support a range of sections sizes, housing typologies and land use activities. Lower density sections should be established along the Tosswill Road and Trices Road boundaries to support an appropriate scale of development at the sensitive rural/urban interface. A 10m building setback for dwellings and utilities is necessary along Trices Road to provide separation and to distinguish the residential neighbourhood from the rural land holdings to the south.

Appropriate interface treatments, and methods to protect these treatments in the long term, need to be established along the Trices Road and Tosswill Road boundaries, which form a gateway to the township and transition from rural to urban. These treatments are to ensure the development integrates with the wider area and addresses any amenity conflicts that may arise at this sensitive residential/rural boundary. Treatments could include appropriate bunding, fencing, retention of a portion of the existing macrocarpa hedgerows or landscaping to avoid long lengths of solid fencing.

Residential housing adjacent to Prebbleton Domain must front the reserve. These lots are to be accessed off a Local Minor Road. This will promote passive surveillance, support front yards facing towards the Domain and avoid a streetscape that is comprised of tall fencing or screening that may undermine the amenity afforded by the reserve. Suitable methods, such as fencing controls, landscape treatments and set backs, should be formalised to ensure all future residential development that overlooks Prebbleton Domain optimises the high amenity and open space outlook provided by the reserve.

## **MOVEMENT NETWORK**

The overall aim is to create an integrated transport network that incorporates all modes of transport.

A Local Major Road connects Tosswill Road with Trices Road. Provision should be made for walking and cycling within this Local Major Road to support access to Prebbleton Domain and the wider circular pedestrian and cycling network identified in the Prebbleton Structure Plan.

Local Intermediate Roads provide a secondary connection to Trices Road and support an integrated network to service the south-eastern portion of the development block. There is a limitation on direct access onto Tosswill Road and Trices Road from the intersection of these roads as far west as the Local Intermediate Road and as far north as the exit point of open space corridor onto Tosswill Road. This restriction will assist in achieving a safe and connected transport network, responds to the future upgrades identified for Trices Road and enables a stepped speed reduction as vehicles enter Prebbleton from the south-east. Interface treatments along the portion of Trices Road and Tosswill Road where access is limited will need to be established at subdivision to achieve a high amenity gateway to the township as the land use activities transition from rural to urban. A Local Minor Road supports links to the Prebbleton Domain and adjacent community facilities.

The open space corridor is an important component of the overall movement network within the Township as it provides an off-road walking and cycling connection between the Domain and Trices Road. This corridor secures walking and cycling connections to the existing Domain and the proposed extensions on the opposite side of Tosswill Road, which provides an alternative to the existing alignment of the Christchurch to Little River Rail Trail. Two green link reserves support further connectivity between the Domain and the primary road network within the development block.

The remaining internal roading layout must be able to respond to the possibility that this area may be developed progressively over time. Road alignments must be arranged in such a way that long term inter-connectivity is achieved once the block is fully developed.

## **GREEN NETWORK**

The ODP indicates a 10m wide green space corridor based around the existing water race and walking and cycling network, connecting Prebbleton Domain to Tosswill Road. A further 10m wide corridor along the alignment of the water race between the future Local Intermediate Road and Tosswill Road protects this amenity feature and establishes an additional walking and cycling connection onto Tosswill Road. The future subdivision scheme should utilise the water race as a focal point to optimise the amenity that it provides and the opportunity it presents in achieving a well connected living environment.



A 2,000m<sup>2</sup> reserve is required to accord with the reserve provision calculations and to ensure the development block is served by sufficient and appropriate open space. The location of this reserve is identified as being indicative only pending confirmation at subdivision of the extent and location of any stormwater treatment areas. The future location of the reserve must ensure people living within the development block have access to an open space reserve within a 400m radius of their homes. Two green space links connect the proposed future development with the existing domain and the town centre to facilitate walking and cycling connections.

Further investigations shall be undertaken at subdivision to determine the practicalities of retaining the following existing specimen trees within any future layout:

- Four oak/ *Quercus* trees on the frontage of 102 Tosswill Road (Lot 1 DP 71108).
- Various border plantings on the north-western boundary of 104 Tosswill Road (Lot 2 DP 71108) where the section adjoins Prebbleton Domain, which is identified as an appropriate interface treatment between any future development of the property and the high amenity presented by the domain.

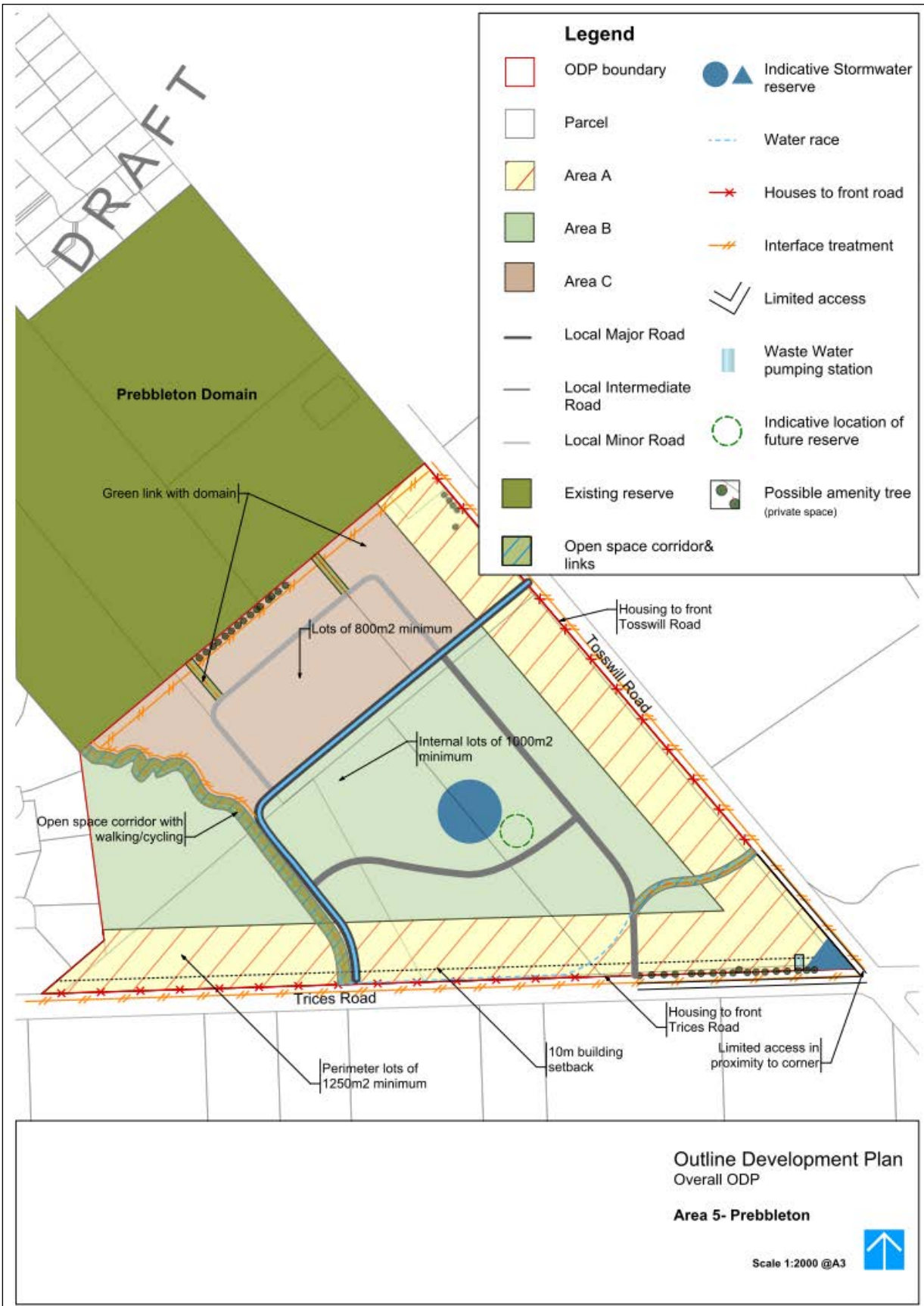
These trees are a link to the historic use of the land, compliment the streetscape and assist in preserving and enhancing the character of Prebbleton. Options to secure the on-going protection of these specimens if retained include consent notices or private covenants to assure the trees longevity.

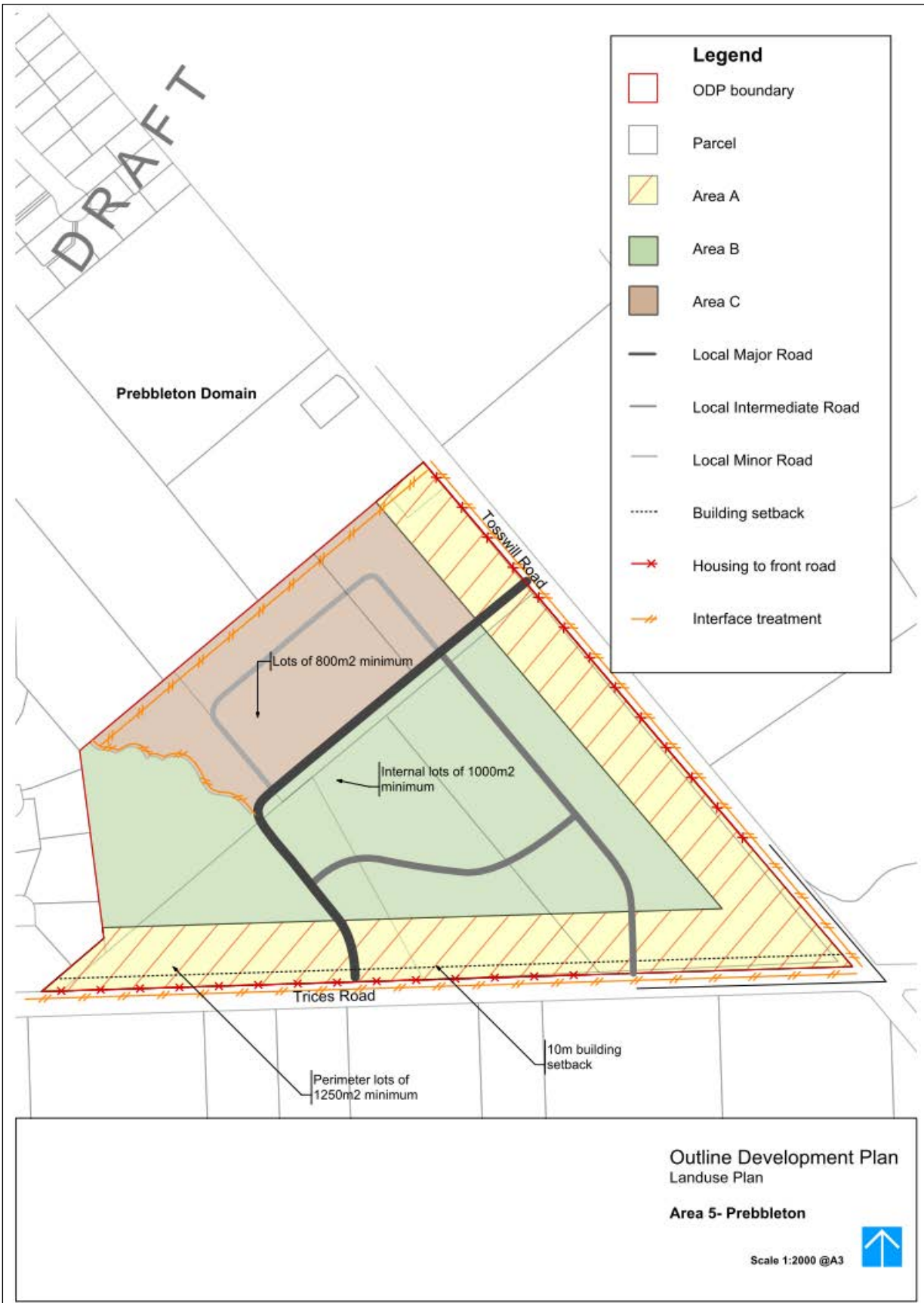
#### BLUE NETWORK

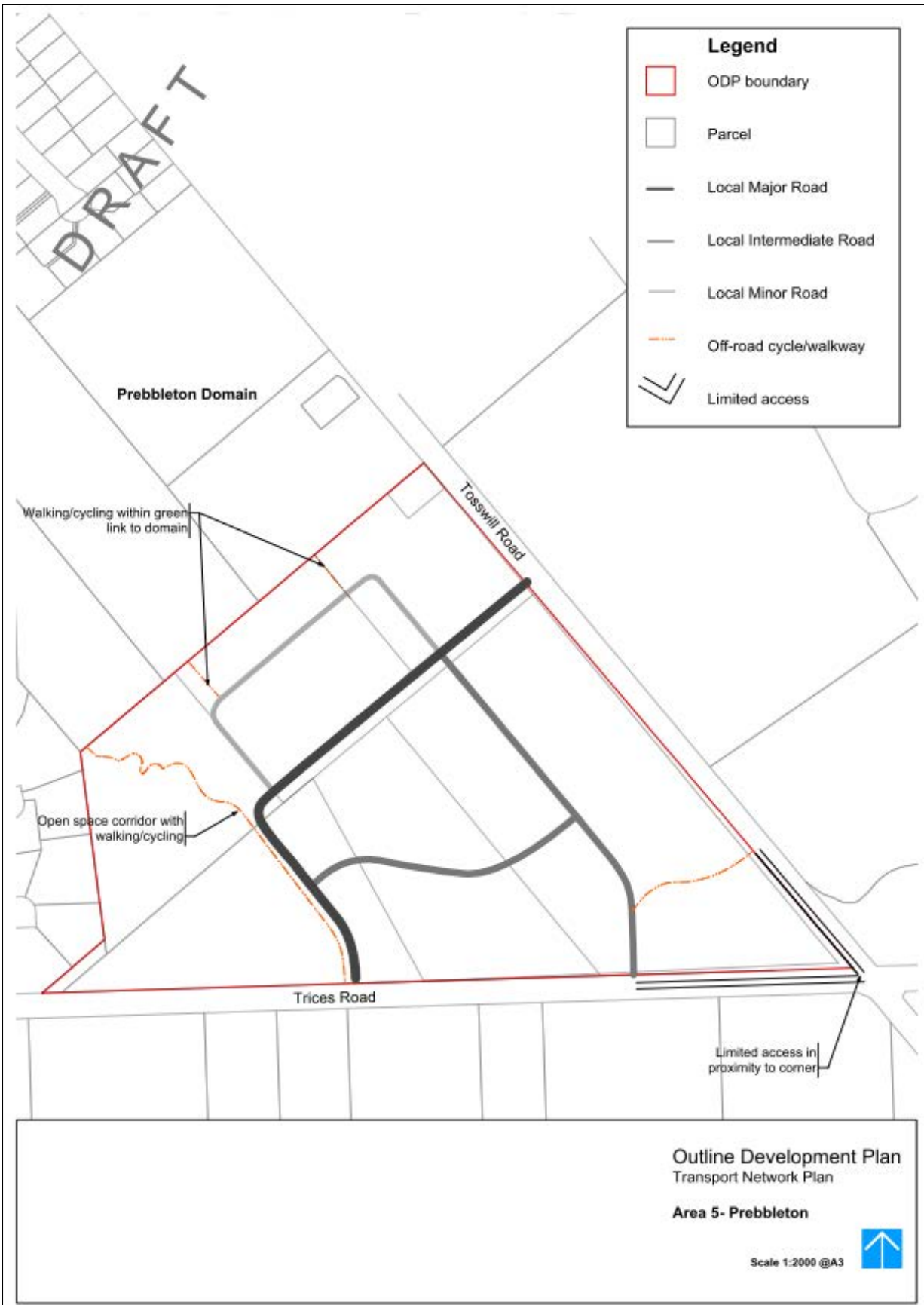
The current 50mm diameter water main that services the general area is identified to be at full capacity. A new 150mm water main will be required to service the development block. There is sufficient capacity in both the Tosswill Road and Trices Road water main and wider network to support the additional water connections required to service the area, with works having to accord with the upgrades identified for the township.

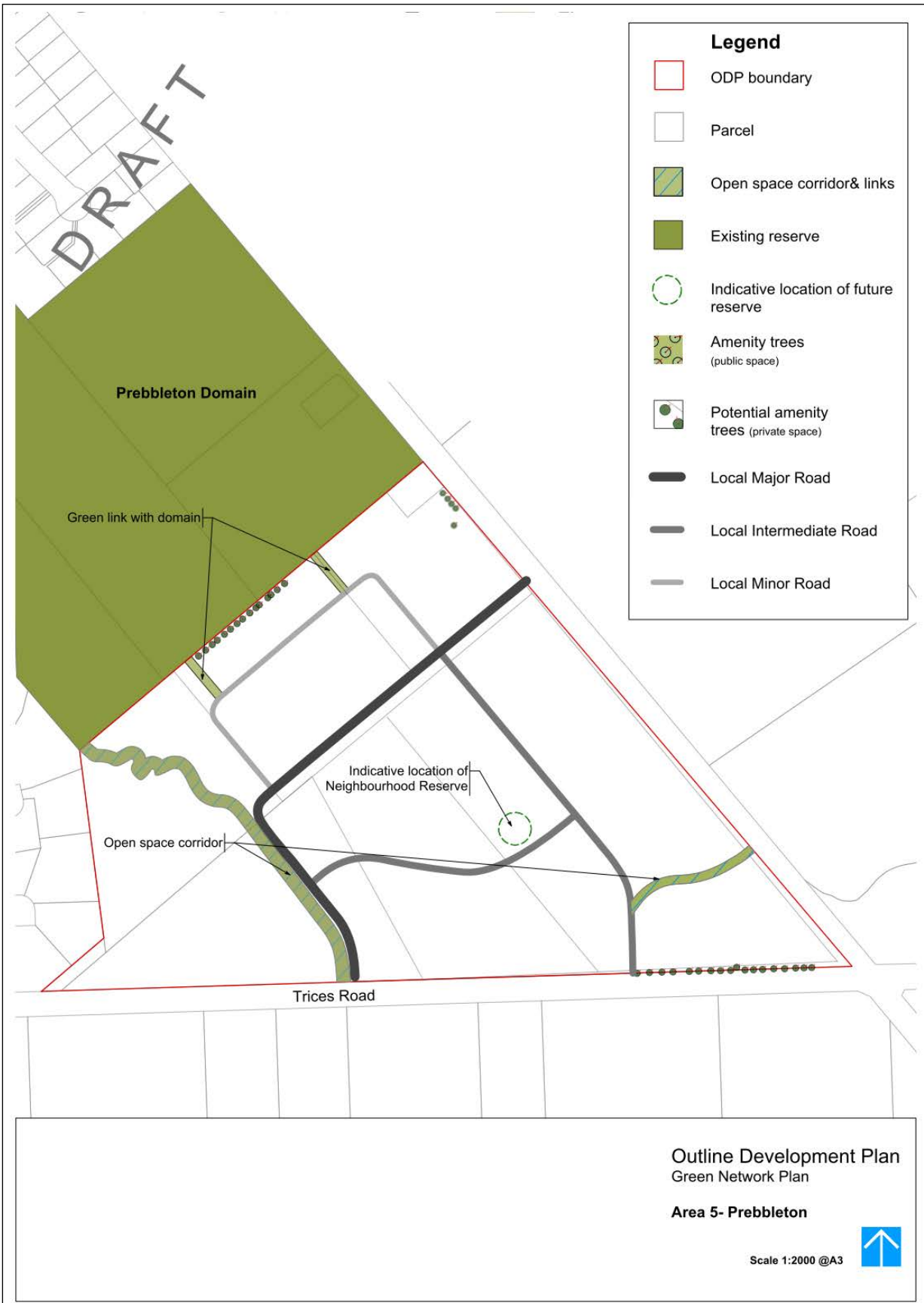
A new pumping station is required to be established at the eastern corner of the development as the Tosswill Road pumping station and sewer main are at full capacity. The developer will need to extend the network to the Springs Road wastewater main.

There are a range of methods available to collect, treat and dispose of stormwater. Options include the discharge of hard surface and roof run-off within residential sections to be disposed on site via soakage pits. Stormwater run-off from the road network is to be directed to basins requiring an estimated area of 1ha. The water race should be developed in an integrated manner that accommodates pedestrian and cycling connections within the site and with Prebbleton Domain. The stormwater reserves referenced on the ODP plan are indicative only. Detailed stormwater solutions are to be determined by the developer in collaboration with Council at subdivision stage and in accordance with Environment Canterbury requirements. Stormwater solutions should integrate into both the road and reserve environments where practicable. The establishment of riparian margins and low impact stormwater management techniques are encouraged where appropriate to establish and enhance ecological corridors, habitats and Te Taumutu values attributed to the water resource.

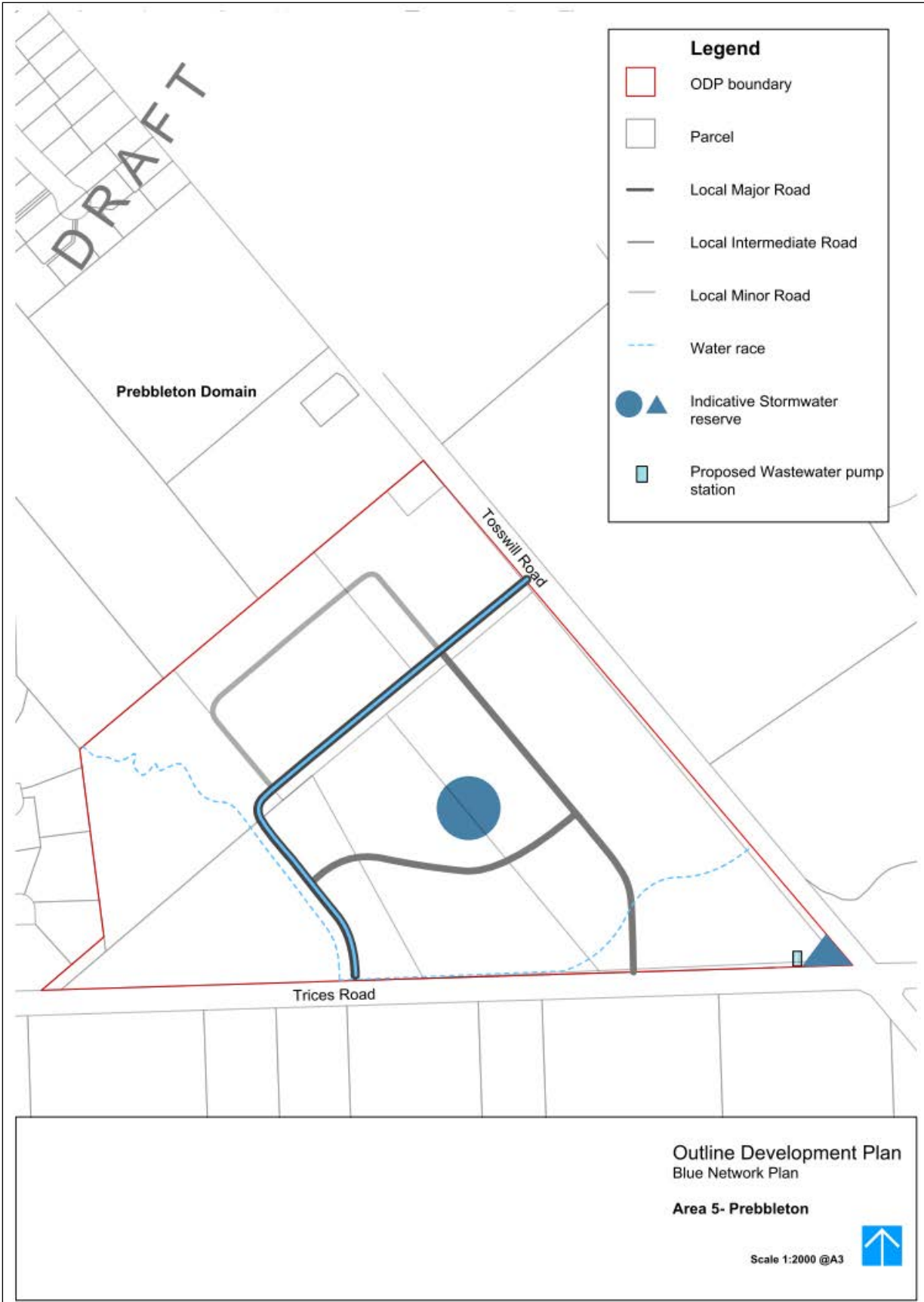












# **Attachment 2**

## **Selwyn District Plan Township Volume - Appendix 38**

### **Rolleston ODP Area 6 Outline Development Plan**

## OUTLINE DEVELOPMENT PLAN AREA 6

### INTRODUCTION

This Outline Development Plan (ODP) is for Development Area 6. Area 6 comprises 83ha and is bound by Dynes Road to the North, Goulds and East Maddisons Roads to the West and areas recognised by Council as future residential growth areas to the south and east.

The ODP embodies a development framework and utilises design concepts that are in accordance with:

- Policy B4.3.7 and B4.3.68 of PC7
- Canterbury Regional Policy Statement
- The Rolleston Structure Plan
- The Greater Christchurch Urban Development Strategy (UDS)
- The Ministry for the Environment's Urban Design Protocol

The ODP has been broken down into four plans. (Density, Movement Network, Green Network and Blue Network).

### DENSITY PLAN

The ODP provides for a variety of allotment sizes from density residential areas of 15 to 20 households/ha through to larger 'standard' residential properties. Generally the density is higher within the central areas of the ODP around key amenity areas and decreases towards the periphery. The highest density housing is to be located in direct proximity to the proposed neighbourhood centre and larger open spaces such as green corridors and neighbourhood parks. The ODP Area shall achieve a minimum net density of 12 households/ per hectare. Given the mix of densities and the likelihood of a staged approach to development, the 12 households per hectare minimum density need not apply to each individual stage. However at the time of subdivision of each stage, assessment and confirmation as to how the minimum density of 12 households per hectare for the overall ODP can be achieved will be required.

A neighbourhood centre will be located in the centre of the ODP adjacent to the central reserve area where the green corridors converge. The Centre will make provision for approximately 1000m<sup>2</sup> gross retail floor area to cater to local weekly and day-to-day retail requirements, though other non-retail uses (such as other business and/or community facilities) may also be appropriately located in the Centre. Additional land is also required for surrounding roads, associated car parking and landscaping to service the eventual activities established in the Centre.

### MOVEMENT NETWORK

The ODP provides for a range of transport options, including:

- vehicular connections linking to Goulds Road, East Maddisons Road, Dynes Road and Springston-Rolleston Road;
- active transport connections at the site boundaries to adjacent areas and internal pedestrian and cycle corridors to encourage viable alternatives to individual motor vehicles; and
- roads which will enable the provision of public transport routes through the site should such services be deemed appropriate;

Roading connections have been designed so as to balance the permeability of the site against the requirements to minimise the number of new intersections and maintain appropriate intersection spacing. The ODP employs a roading hierarchy that allows for



Primary, Secondary and Tertiary Roads; however only the more significant roads (Primary and Secondary) have been shown on the ODP. As the Tertiary Roads' primary function will be to service the residential areas, their eventual layout will respond to the detailed subdivision design of those areas.

Goulds Road and East Maddisons Road provide primary road corridors on the Area's western periphery. Internally, the site contains one primary collector road which links the Neighbourhood Centres envisaged by the Rolleston Structure Plan at East Maddisons and Springston-Rolleston Roads. Though the collector is envisaged to cater for a large portion of through vehicle movements, it is not a high-speed corridor and rather, should provide direct access to adjoining sites. To this end, it is envisaged that the collector will interact with the adjacent neighbourhoods, rather than creating severance between them.

The proposed north-south and east-west secondary road connections perform similar functions to the primary roads, providing ample access throughout the site as well as good external links and connections to the immediate neighbours. Notwithstanding this similarity in function, secondary roads will assume a form which is of a more residential nature, and cater less to through vehicle traffic. In addition to the proposed internal secondary routes, Dynes Road will perform a secondary function along the site's northern boundary.

An integrated network of tertiary roads will facilitate internal distribution of traffic, provide access to properties, connect open spaces within the site and offer future links to the immediate neighbours. The tertiary roads will provide a narrower carriageway to encourage slower speeds and to maintain a residential character.

The overall aim of the pedestrian and cycle network is to encourage active transport use within the site and to enable good connections to the wider Rolleston area. Primary and Secondary Roads will provide footpaths and cycle routes, including designated cycle lanes where appropriate. Tertiary Roads will also provide adequate space for cyclists and convenient pedestrian movements. Shared off-road pedestrian and cycle connections will be provided to achieve safe, attractive active transport corridors and recreational amenity.

#### GREEN NETWORK

The ODP reflects and adds to the green network anticipated in the Rolleston Structure Plan. A range of reserve types and sizes are proposed to create a connected open space network, whereby parks will be linked via green corridors formed along existing stockwater races and along new roading corridors.

Incorporating new structural vegetation, stormwater swales and 'green' streets, four green corridors and green links will contribute generously to the area's sense of place and overall amenity. The green corridors, as well as the proposed neighbourhood parks, are located immediately adjacent to higher density residences to compensate for reduced private open space on individual allotments. In addition, co-locating green spaces alongside residential sites will allow passive surveillance for enhanced safety and security.

Neighbourhood parks will be located to ensure that an open space is within 400m walking distance from any new dwelling. The parks will be of varying sizes to reflect both their purpose and their neighbourhood's character and will include both active and passive functions.

Portions of the green network will also perform a stormwater conveyance and treatment function, particularly for runoff from roads and hardstand in public spaces. Opportunities for

pedestrian and cycle paths will also be realised in the design of future reserves and green links to ensure a high level of connectivity is afforded to residents and visitors, and to maximise the utility of the public space.

Council's open space requirements cited in the LTP and Activity Management Plans should be referred to during subdivision design. Where any additional reserve areas may be proposed at subdivision stage over and above the requirements of Council, alternative arrangements may be made for any additional open space land that the Council does not take towards reserve contributions. These arrangements may include purchase by the Council with a corresponding targeted rate, gifting the land to the Council, or other mechanisms as may be agreed at subdivision stage.

No specific mitigation treatment is required for Area 6 to address potential reverse sensitivity effects. Adjoining land uses to the south and east are not of a nature that would require specific effects management at the rural/residential interface over and above the decreased residential density in this portion of the ODP Area.

Moreover, as these adjoining areas are anticipated by Council strategic planning policy for future urban use, ODP Area 6 makes provision to integrate with them, rather than become isolated from them.

#### BLUE NETWORK

The underlying soils are relatively free-draining (mostly gravels) and infiltration to ground is generally the most appropriate means of stormwater disposal. The public stormwater system will primarily only be required for runoff generated from within the road reserve, as individual buildings will be able to dispose roof water directly to ground within private properties. Where there is potential for the stormwater to be contaminated (e.g. road runoff), treatment will be incorporated in the stormwater system prior to disposal.

There are a range of options available for the collection, treatment and disposal of stormwater. Final treatment solutions will be determined through detailed site investigations during subdivision stages; however, common themes will be achieved throughout the area, including that systems will be designed to integrate into both the roadway and reserve environments. The ability to add amenity value, beyond a pure stormwater function, will also be key component to the overall design.

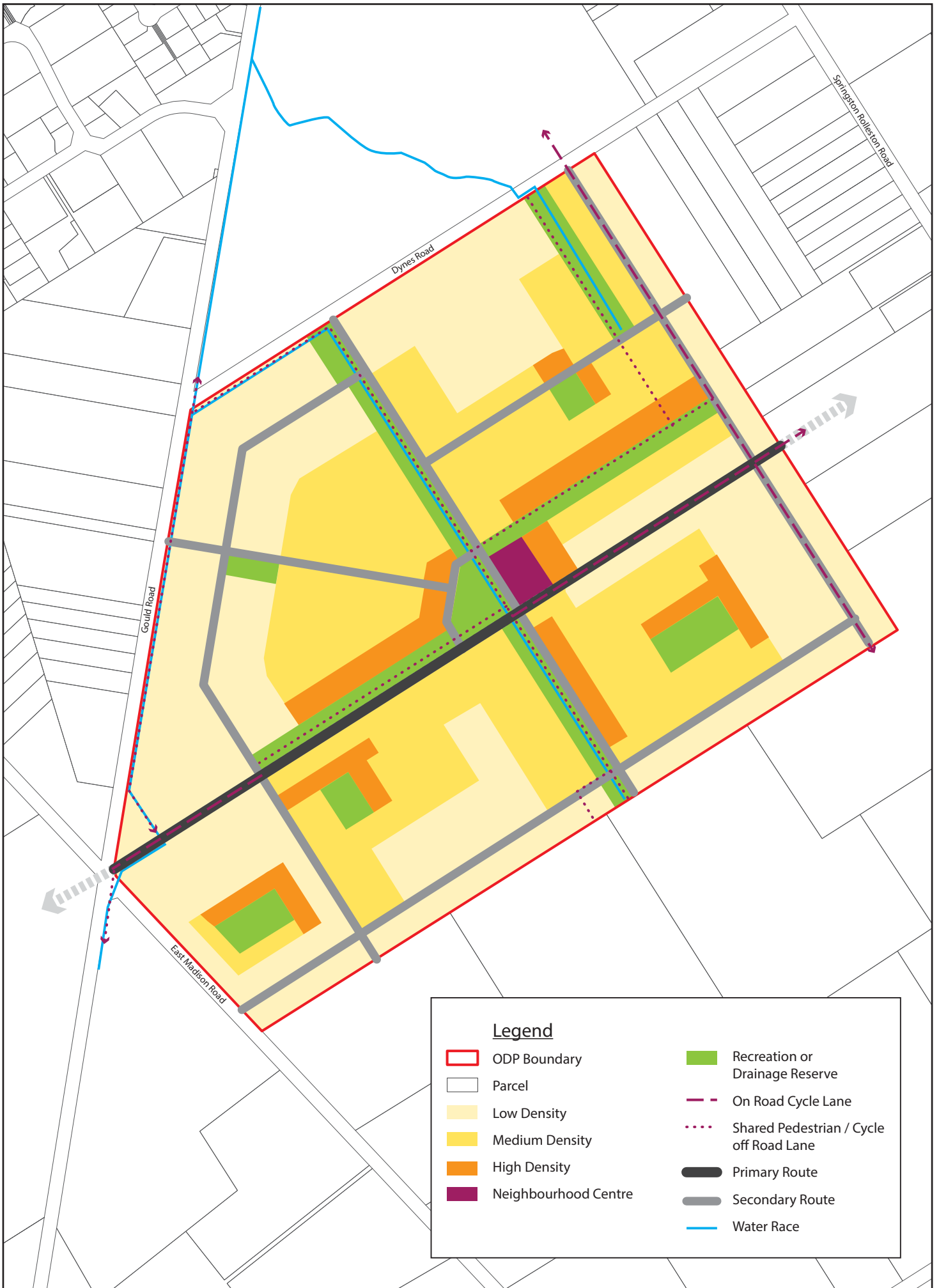
The stormwater conveyance and disposal systems proposed for this site will be consistent with other effective stormwater systems commonly used within Rolleston. The relatively flat nature of the site necessitates the management of stormwater treatment and disposal to be undertaken as a number of discrete catchments, rather than conveying stormwater from the entire site to one central treatment location. However, to minimise on-going maintenance costs and maximise integration potential, the number of these treatment areas will be minimised and main stormwater treatment areas will be located within the major open spaces.

Ground levels slope naturally to the south, making a primarily gravity wastewater network entirely feasible. The area immediately adjacent to Dynes/Goulds Road intersection is currently able to connect, via gravity, to the existing pump station on Goulds Road. However the vast majority of the ODP Area will need to gravitate to the new East Selwyn Sewer Scheme pump station on Selwyn Road. This will be achieved via a route extending south of the ODP area to Selwyn Road and then east to connect with the East Selwyn Sewer Scheme pump station on Selwyn Road.

The water reticulation will be an extension of the existing water supply in Rolleston. Selwyn District Council currently has plans to commission additional water bores, to match the expected increase in water demand from the identified growth areas. If required, an additional bore field could be incorporated within the development of this site to assist servicing requirements for the southern development areas.

# **Attachment 2 Continued**

**Maps**



**Legend**

ODP Boundary	Recreation or Drainage Reserve
Parcel	On Road Cycle Lane
Low Density	Shared Pedestrian / Cycle off Road Lane
Medium Density	Primary Route
High Density	Secondary Route
Neighbourhood Centre	Water Race

20 February 2013





**Legend**

- ODP Boundary
- Parcel
- Recreation or Drainage Reserve
- On Road Cycle Lane
- Shared Pedestrian / Cycle off Road Lane
- Primary Route
- Secondary Route
- Water Race

20 February 2013



Outline Development Plan  
 Area 6 - Rolleston  
 Movement Network



**Legend**

- ODP Boundary
- Parcel
- Recreation or Drainage Reserve
- Roads
- On Road Cycle Lane
- Shared Pedestrian / Cycle off Road Lane
- Water Race
- ↔ Green Link

20 February 2013



Outline Development Plan  
Area 6 - Rolleston  
Green Network



**Legend**

- ODP Boundary
- Parcel
- Recreation or Drainage Reserve
- Roads
- Waterway / Water Race
- Proposed Gravity Wastewater

20 February 2013



Outline Development Plan  
 Area 6 - Rolleston  
 Blue Network



# **Attachment 3**

## **Living Z zone ODP's**

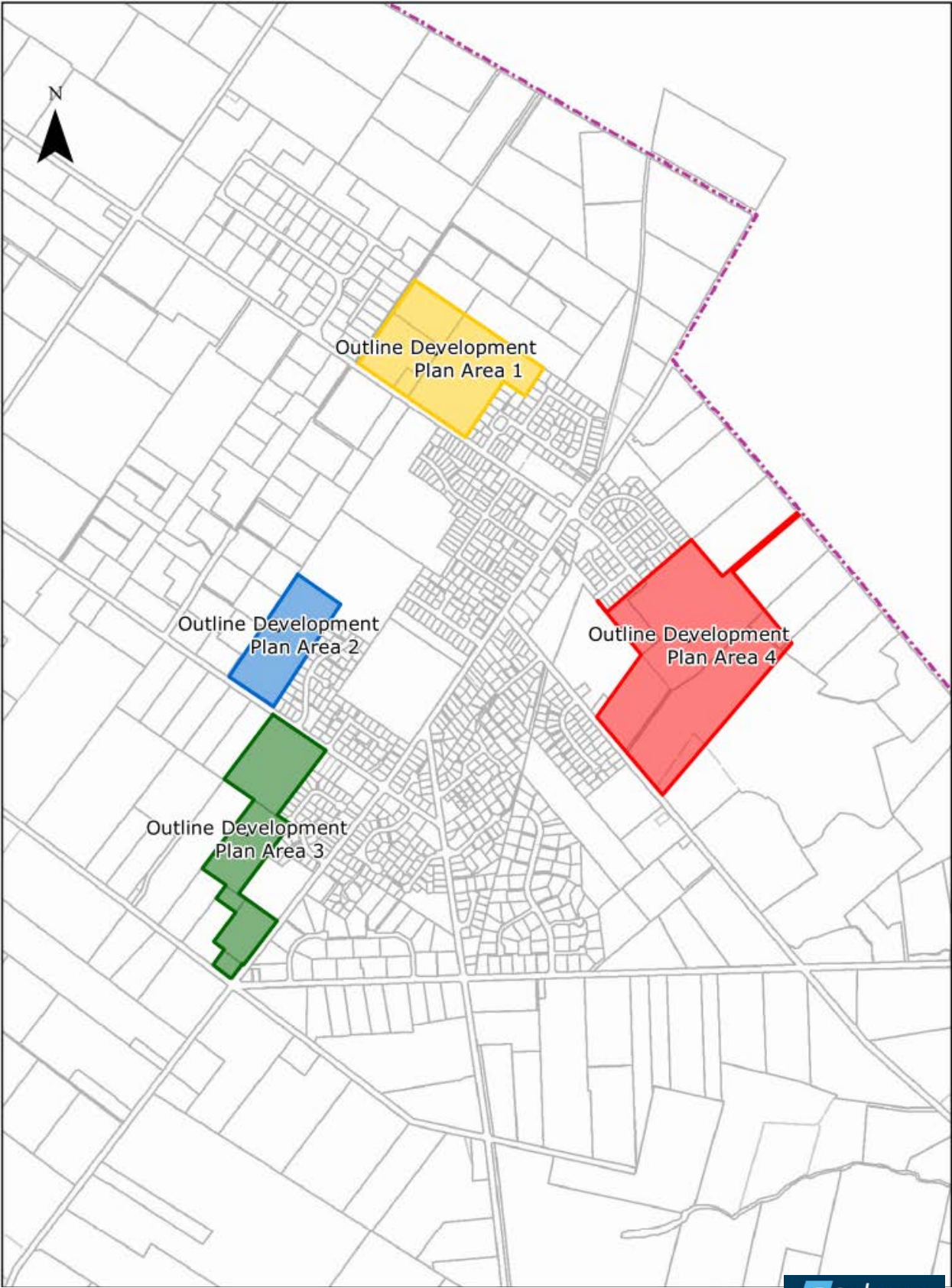
PART E

# APPENDIX 41

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## **LIVING Z ZONE OUTLINE DEVELOPMENT PLANS – PREBBLETON**

- For locations of these ODP's see overleaf
- ODP Area 1
- ODP Area 2
- ODP Area 3
- ODP Area 4



# **OUTLINE DEVELOPMENT PLAN AREA 1**

## **INTRODUCTION**

This Outline Development Plan (ODP) is for Area 1, which is zoned Living Z. Area 1 includes 13.3ha of land, comprising four properties. Three of these properties front onto Blakes Road. The ODP area is contained by The Paddocks development to the north, Aberdeen Drive to the west and Elmwood Drive to the east.

The ODP provides an overarching urban design framework to guide the future development of the land. The ODP includes Land Use, Movement, Green and Blue Networks and incorporates the wider strategic and community outcomes expressed in the Prebbleton Structure Plan.

## **URBAN DESIGN**

The design principles that underpin this ODP are in line with the New Zealand Urban Design Protocol and accord with the Selwyn District Council Subdivision Design Guide (September 2009). The following environmental outcomes are to be achieved:

- Development that meets the District Plan policies, realises an overall increase in residential density, applies urban consolidation principles and assists in achieving a compact concentric settlement pattern for Prebbleton.
- Provision for a range of section sizes and housing typologies to respond to the wider needs of the community, whilst achieving the prescribed minimum household densities and minimum average allotment sizes.
- Subdivision layouts that integrate with adjoining neighbourhoods and incorporate existing land uses where appropriate. The wider context of the development area should influence the subdivision layout by protecting and enhancing cultural, ecological, heritage and Te Taumutu values and existing built features, such as amenity trees and water races.
- Layouts and urban design treatments that create a distinguishable sense of place, assist in enhancing the wider character and amenity of Prebbleton and deliver safe, vibrant and healthy living environments. Layouts should apply Crime Prevention through Environmental Design (CPTED) design principles.
- Integrated and legible road hierarchy that supports safe and efficient connections and promotes walking and cycling. Road design and landscape treatments should contribute to the overall character of Prebbleton and assist in connecting residential development with open space reserves and other public assets and services within the township, such as the Domain, Primary School, Nature Park and the town centre.
- Sustainable methods to treat and dispose of stormwater that protect groundwater resources and surface waterways (including springs, water/stock races) from contamination, while integrating with open space and reserves where appropriate.
- Installation of all the necessary infrastructure services within the ODP area, and the cost effective and efficient connection of those services to the wider network.

## **LAND USE**

The ODP Area shall achieve a minimum net density of 10 households per hectare. Lower density allotments are necessary on the majority of the western boundary and the full length of the northern boundary to integrate the site with the adjoining Living 2A (Blakes Road) and Living 2A (The Paddocks) zones, being the Aberdeen and The Paddocks subdivisions respectively. Smaller sections are best established within the centre of the site to reduce the risk of adverse amenity effects on the adjoining Living 1A4 zone to the east and Living 1A6 zone to the south.

Dwellings must front Blakes Road to enhance passive surveillance and safety, while creating a high amenity streetscape. Appropriate design layouts should take into consideration the shape, orientation and aspect of sections, with internal roads supporting access that avoids housing from backing onto Blakes Road.

Residential housing established along the Local Minor Road to the north-east of the reserve must front the reserve. Appropriate interface treatments must promote passive surveillance, support front yards facing

towards the reserve and avoid a streetscape that is comprised of tall fencing or solid screening. Other suitable methods, such as fencing controls and set backs, should be formalised at subdivision stage to ensure that all future residential development that overlooks the reserve optimises the high amenity and outlook provided by this open space.

## MOVEMENT NETWORK

The proposed roading network consists of a Local Major Road that forms a loop around the development block, supported by Local Intermediate and Local Minor Roads, to achieve an integrated and legible internal road layout. The ODP indicates that the Local Major Road connects to Blakes Road at two points, with the south-eastern connection aligning with the access point being established in the Living 1A6 zone on the opposite side of Blakes Road to form a crossroad.

The Local Minor Road that links between the Local Major Road and Local Intermediate Road shall align with the existing grouping of high quality amenity trees. These trees must be incorporated into the road reserve as an amenity feature. Retaining the trees preserves a link to the previous land use activity and protects high quality specimens. It also contributes to the character of Prebbleton. This grouping contains 7 specimens that are suitable to be retained as street trees within the road reserve, and includes specifically a row of six oak/*Quercus* that follow a north-south alignment and a single oak/*Quercus* to the south-east of this row of oaks.

A Local Minor Road situated north of the proposed reserve will provide access to adjacent residential sites. This will ensure that buildings front the road and overlook the reserve. The ODP also supports the establishment of a connection between the development block and the Living 2A zone to the north to support connectivity and to ensure this property does not become land locked.

Two indicative connections are also identified from the Local Major Road into the northern portion of the development block to achieve an integrated internal roading network. Confirmation will need to be established at subdivision that these through connections avoid conflict with the existing dwelling established within this property.

The remaining internal roading layout must be able to respond to the possibility that the area may be developed progressively over time. Road alignments must be arranged in such a way that long term inter-connectivity is achieved once the block is fully developed.

## GREEN NETWORK

The ODP indicates a single open space reserve approximately 3,000m<sup>2</sup> in size. The reserve is located centrally along the Local Major Road. This location has been identified to optimise a portion of the existing walnut grove to secure a unique amenity feature, while preserving an historic reference to the previous use of the site. The orientation of this reserve supports a high amenity entrance into the site when viewed from Blakes Road.

A portion of the existing walnut/*Juglans* grove is to be retained as part of the reserve. These trees present an opportunity to retain a link to the land holdings past and to create a public space containing a relatively unique amenity feature. This walnut grove is contained within the legal boundaries of 36 Blakes Road (Lot 2 DP 54834).

Further investigations shall be undertaken at subdivision to determine the practicalities of retaining the following existing specimen trees within any future layout:

- Two sycamore/*Acer pseudoplatanus* and an established walnut/*Juglans* tree on the frontage of Blakes Road located within the gardens of the existing cottage at 36 Blakes Road (Lot 2 DP 54834).
- Three oak/*Quercus* trees within the yard at the rear of the Prebbleton Veterinary surgery at 56 Blakes Road (Lot 1 DP 58405).
- Nine oak/*Quercus* that follow an east-west alignment could also be incorporated into the road alignment along the northern side of the identified Local Major Road and future residential sections to the west of this road. These amenity trees are all currently contained within the legal boundaries of 60 Blakes Road (Lot 1 DP 71538).
- A copse containing two beech/*Nothofagus* and seven oak/*Quercus* trees to the north of the Local Major Road. The appropriateness of retaining this grouping of trees within any future connection to

the land holding to the north should be investigated at subdivision. These trees are wholly contained within the legal boundary of 60 Blakes Road (Lot 1 DP DP 71538).

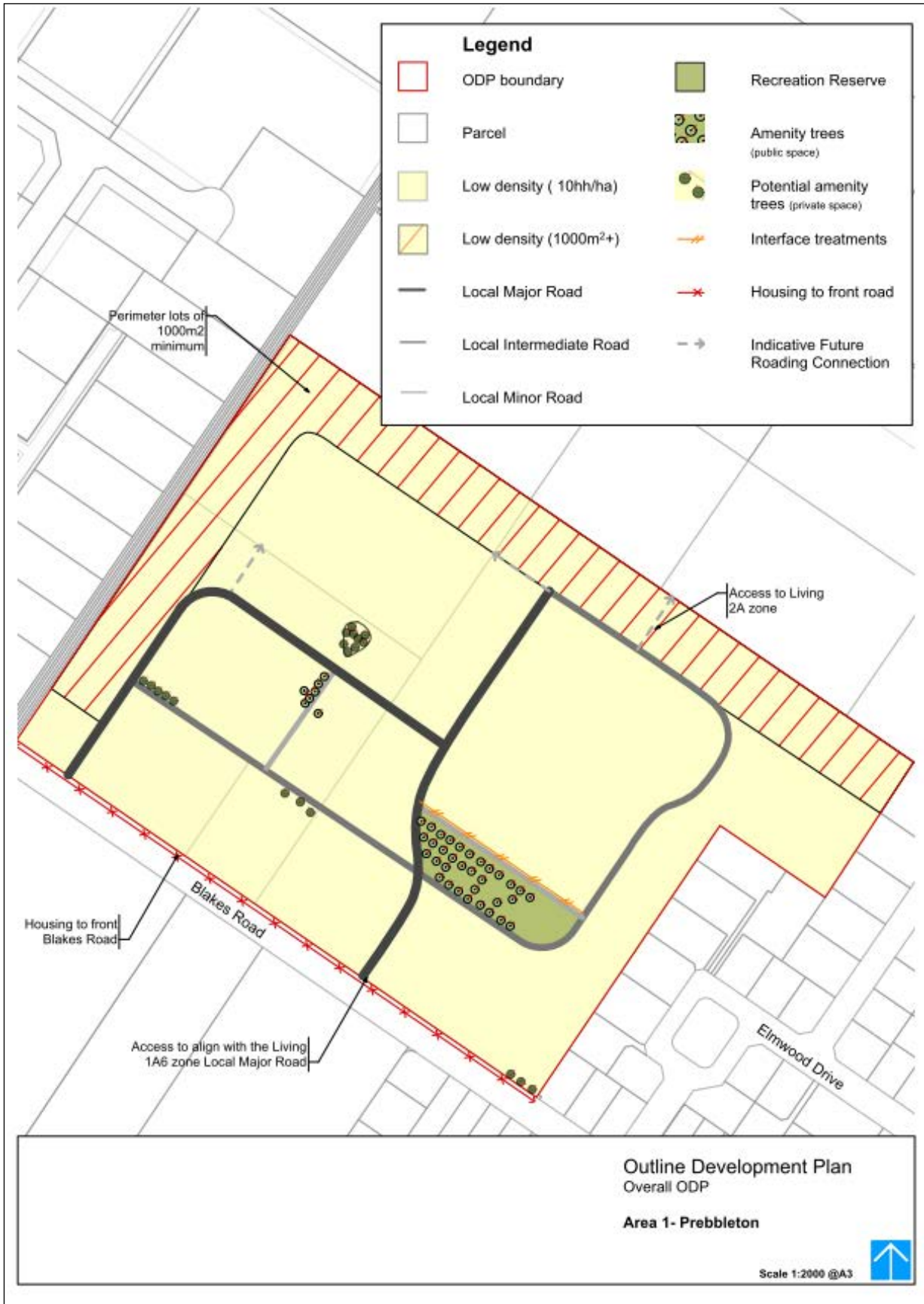
These trees provide amenity to the current and future residents and the community. They are a link to the historic use of the land, compliment the streetscape and assist in preserving and enhancing the character of Prebbleton. Options to secure the on-going protection of these specimens if retained include consent notices or private covenants to assure the trees longevity.

#### BLUE NETWORK

There is sufficient capacity in the Blakes Road water main and wider network to support the additional water connections required to service the area. Any works shall accord with the upgrades identified for the township.

The anticipated wastewater solution is to establish a southern connection to the Living 1A6 zone on the opposite side of Blakes Road to enable access to the sewer gravity main.

There are a range of methods available to collect, treat and dispose of stormwater. Options include the discharge of hard surface and roof run-off within residential sections to on-site soakage pits and for run-off to be collected in roadside swales and then discharged to ground within the proposed development area. The roadside swales referenced on the ODP Plan are indicative only. Detailed stormwater solutions are to be determined by the developer in collaboration with Council at subdivision stage and in accordance with Environment Canterbury requirements. Stormwater solutions should integrate into both the road and reserve environments where practicable. The establishment of riparian margins and low impact stormwater management techniques are encouraged where appropriate to establish and enhance ecological corridors, habitats and Te Taumutu values attributed to the water resource.



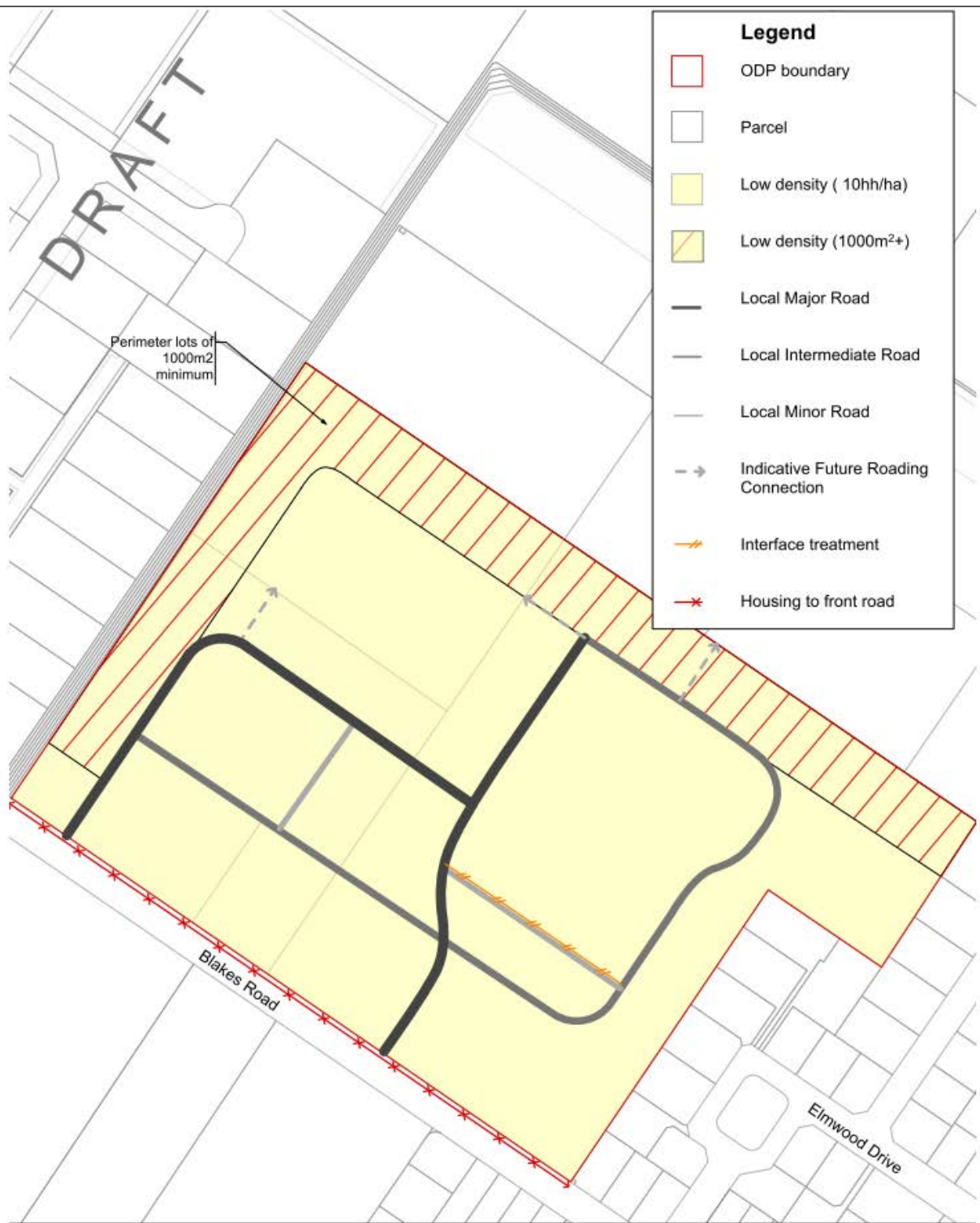


DRAFT

Perimeter lots of 1000m<sup>2</sup> minimum

**Legend**

- ODP boundary
- Parcel
- Low density ( 10hh/ha)
- Low density (1000m<sup>2</sup>+) Interface treatment
- Local Major Road
- Local Intermediate Road
- Local Minor Road
- Indicative Future Roading Connection
- Housing to front road



Outline Development Plan  
Landuse Plan  
Area 1- Prebbleton

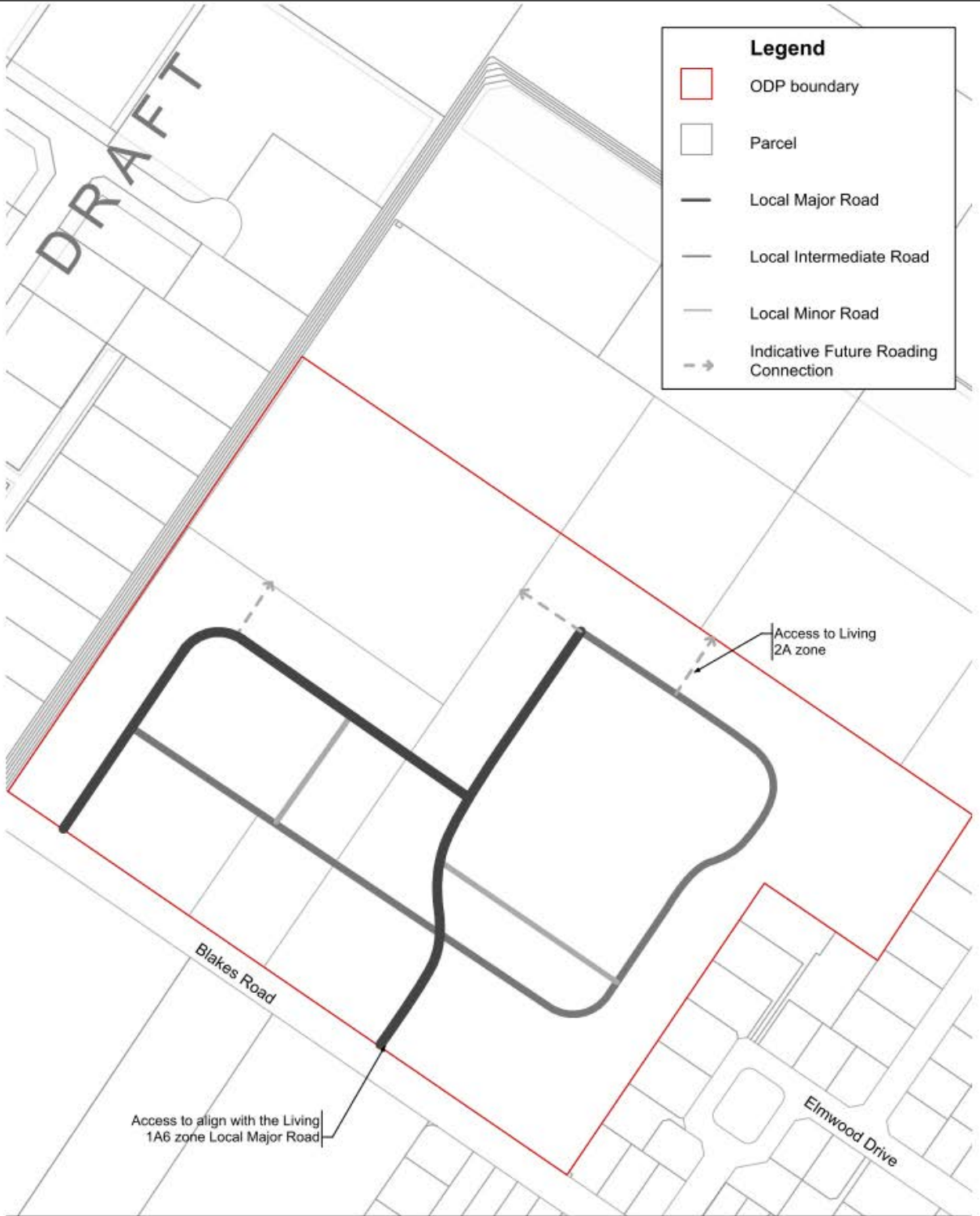
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**Legend**










- ODP boundary
- Parcel
- Local Major Road
- Local Intermediate Road
- Local Minor Road
- Indicative Future Roding Connection



Outline Development Plan  
Transport Network Plan  
**Area 1 - Prebbleton**

Scale 1:2000 @A3 

DRAFT

Legend	
	ODP boundary
	Parcel
	Local Major Road
	Local Intermediate Road
	Local Minor Road
	Recreation Reserve
	Amenity trees (public space)
	Potential amenity trees (private space)
	Indicative Future Roading Connection

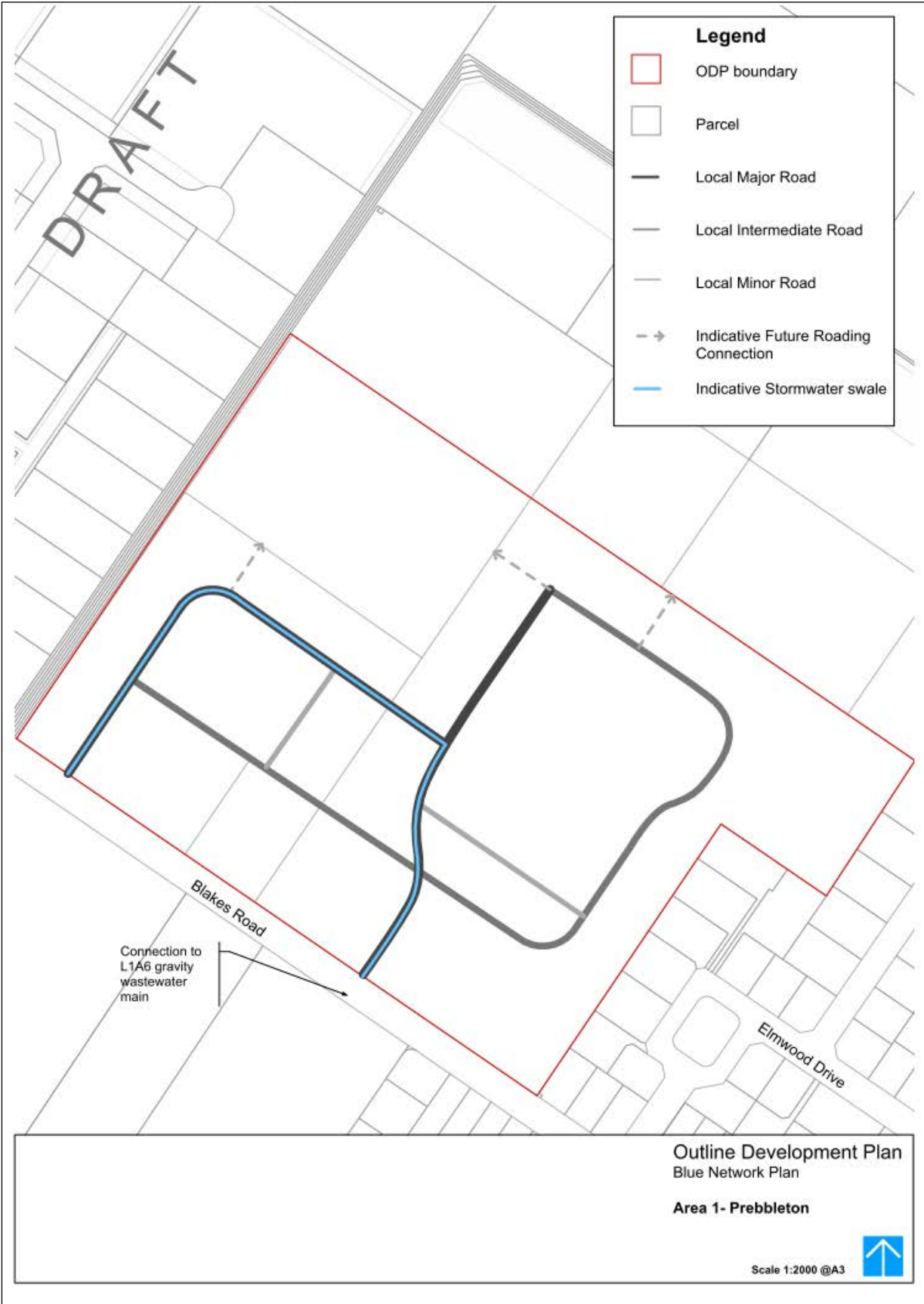


Outline Development Plan  
Green Network Plan

Area 1- Prebbleton

Scale 1:2000 @A3





# **OUTLINE DEVELOPMENT PLAN AREA 2**

## **INTRODUCTION**

This Outline Development Plan (ODP) is for Area 2, which is zoned Living Z. Area 2 is comprised of a single 6.4ha property that fronts Trents Road. The development block is generally bound by the Kingcraft Drive Existing Development Area zone to the west, Cairnbrae development to the north (Living 1A6 zone) and Waratah Park development to the east (Living X zone).

The ODP provides an overarching urban design framework to guide the future development of the land. The ODP includes Land Use, Movement, Green and Blue Networks and incorporates the wider strategic and community outcomes expressed in the Prebbleton Structure Plan.

## **URBAN DESIGN**

The design principles that underpin this ODP are in line with the New Zealand Urban Design Protocol and accord with the Selwyn District Council Subdivision Design Guide (September 2009). The following environmental outcomes are to be achieved:

- Development that meets the District Plan policies, realises an overall increase in residential density, applies urban consolidation principles and assists in achieving a compact concentric settlement pattern for Prebbleton.
- Provision for a range of section sizes and housing typologies to respond to the wider needs of the community, whilst achieving the prescribed minimum household densities and minimum average allotment sizes.
- Subdivision layouts that integrate with adjoining neighbourhoods and incorporate existing land uses where appropriate. The wider context of the development area should influence the subdivision layout by protecting and enhancing cultural, ecological, heritage and Te Taumutu values and existing built features, such as amenity trees and water races.
- Layouts and urban design treatments that create a distinguishable sense of place, assist in enhancing the wider character and amenity of Prebbleton and deliver safe, vibrant and healthy living environments. Layouts should apply Crime Prevention through Environmental Design (CPTED) design principles.
- Integrated and legible road hierarchy that supports safe and efficient connections and promotes walking and cycling. Road design and landscape treatments should contribute to the overall character of Prebbleton and assist in connecting residential development with open space reserves and other public assets and services within the township, such as the Domain, Primary School, Nature Park and the town centre.
- Sustainable methods to treat and dispose of stormwater that protect groundwater resources and surface waterways (including springs, water/stock races) from contamination, while integrating with open space and reserves where appropriate.
- Installation of all the necessary infrastructure services within the ODP area, and the cost effective and efficient connection of those services to the wider network.

## **LAND USE**

The ODP Area shall achieve a minimum net density of 10 households per hectare. Lower density allotments are necessary on the western boundary to integrate the site with the adjoining lifestyle properties established within the Kingcraft Drive Existing Development Area zone. Appropriate interface treatments at the boundary between residential and rural residential activities, and methods to protect these treatments in the long term, shall be established. Treatments could include appropriate fencing, landscaping and minimum building set backs.

Smaller sections are best established within the centre of the site to reduce the risk of adverse amenity effects on the adjoining Living X zone to the east and Living 1A6 zone to the north. The proposed reserve forms a focal point for residential housing, with layouts and interface treatments optimising the open space amenity and outlook provided by the reserve.

Dwellings must front Trents Road to enhance passive surveillance and safety, while contributing to a high amenity streetscape. Appropriate design layouts should take into consideration the shape, orientation and aspect of sections, with internal roads and access arrangements that support housing that fronts onto Trents Road.

Residential housing established adjacent to any future stormwater basins and the reserve must front these open space areas. Appropriate interface treatments promote passive surveillance and support front yards facing towards the reserve. Streetscapes that are comprised of tall fencing or screening that may undermine the amenity afforded by the reserve are to be avoided. Suitable methods, such as fencing controls and set backs, should be formalised at subdivision stage to ensure all future residential development overlooking these reserves benefit from the high amenity and outlook.

#### MOVEMENT NETWORK

The proposed roading network consists of one Local Intermediate Road that links Trents Road with the Cairnbrae development to the north (Living 1A6 zone) to achieve an integrated transport network for the wider area. On-road cycling is provided for within the Local Intermediate Road to support the wider circular walking and cycling network identified within the Prebbleton Structure Plan.

The remaining internal roading layout must be arranged in such a way that long term inter-connectivity is achieved once the block is fully developed.

#### GREEN NETWORK

The ODP indicates a single open space reserve that is approximately 2,000m<sup>2</sup> in size to accord with the reserve provision calculations for the township and the population base it is serving. The location has been identified to ensure people living within the development block have access to open space within a 400m walking radius of their homes.

#### BLUE NETWORK

There is sufficient capacity in the Trents Road water main and wider network to support the additional water connections required to service the area, with works having to accord with the upgrades identified for the township.

The anticipated wastewater solution is to extend the wastewater network to the pumping station established at the intersection of Trents Road and Lindsay Drive.

There are a range of methods available to collect, treat and dispose of stormwater. Options include the discharge of hard surface and roof run-off within residential sections to be disposed on site via soakage pits and for run-off to be directed to two stormwater basins within the proposed development area. The stormwater basins and swales referenced in the ODP plan are indicative only. Detailed stormwater solutions are to be determined by the developer in collaboration with Council at subdivision stage and in accordance with Environment Canterbury requirements. Stormwater solutions should integrate into both the road and reserve environments where practicable. The establishment of riparian margins and low impact stormwater management techniques are encouraged where appropriate to establish and enhance ecological corridors, habitats and Te Taumutu values attributed to the water resource.



**Legend**

- |   |   |   |                       |   |                               |
|---|---|---|-----------------------|---|-------------------------------|
|  | ODP boundary  |  | Local Minor Road      |  | Indicative Stormwater reserve |
|  | Parcel  |  | Interface treatment   |  | Indicative Stormwater swale   |
|  | Low density ( 10hh/ha)  |  | Housing to front road |  | On-road cycling               |
|  | Low density (1000m <sup>2</sup> +) Perimeter lots of 1000m <sup>2</sup> minimum |  | Recreation Reserve    |   |                               |
|  | Local Intermediate Road   |   |                       |   |                               |

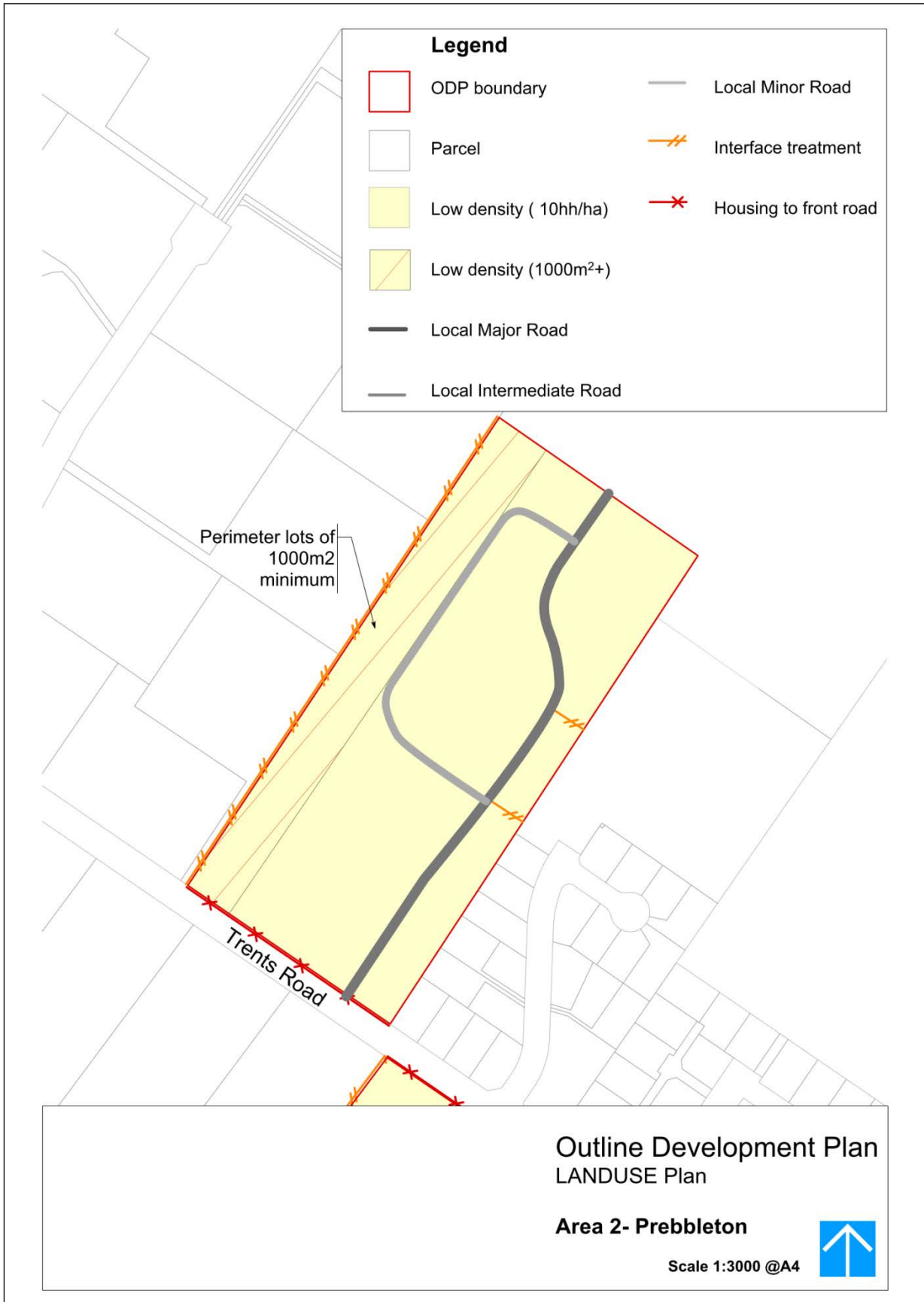


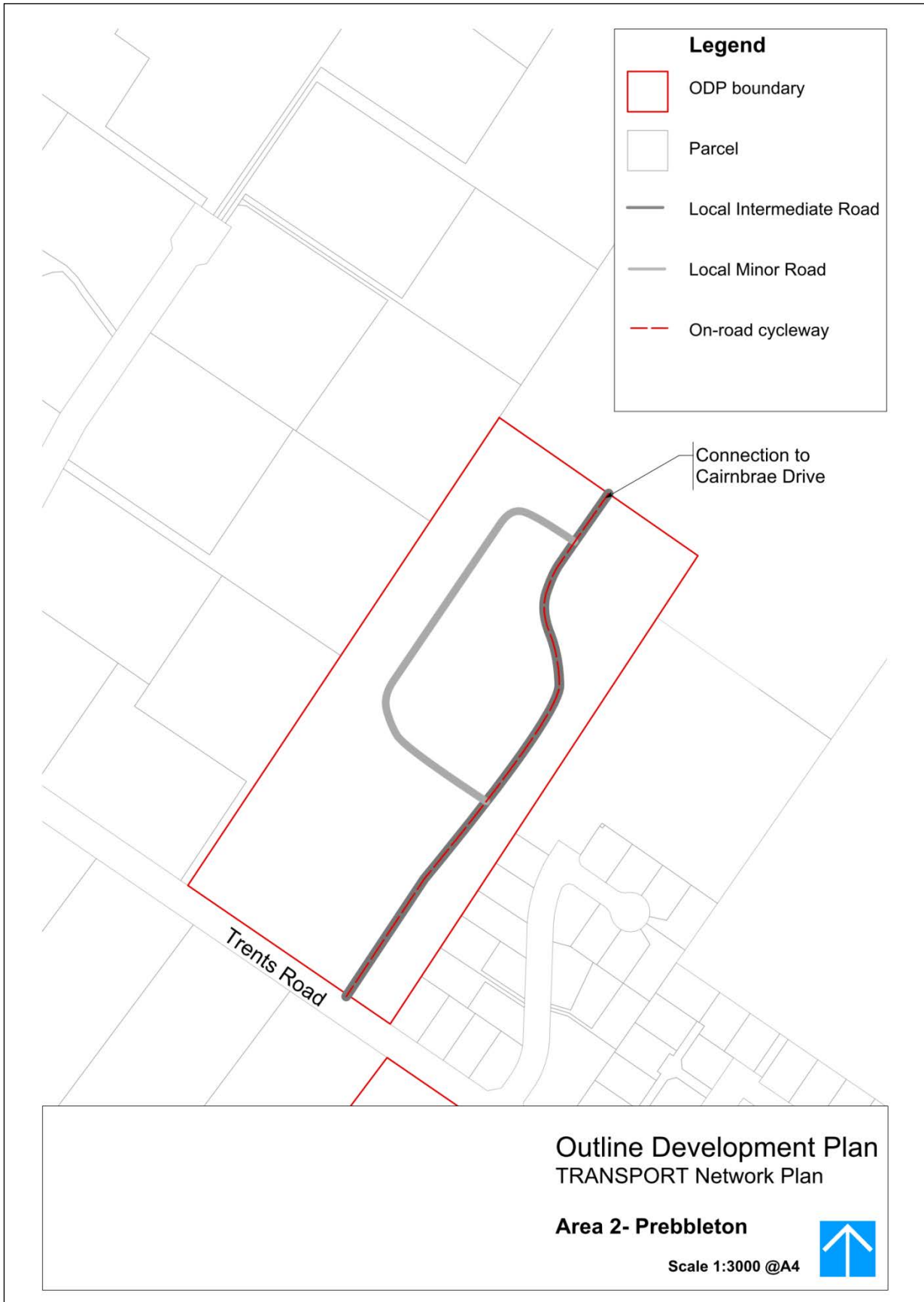
**Outline Development Plan  
OVERALL ODP**

**Area 2- Prebbleton**

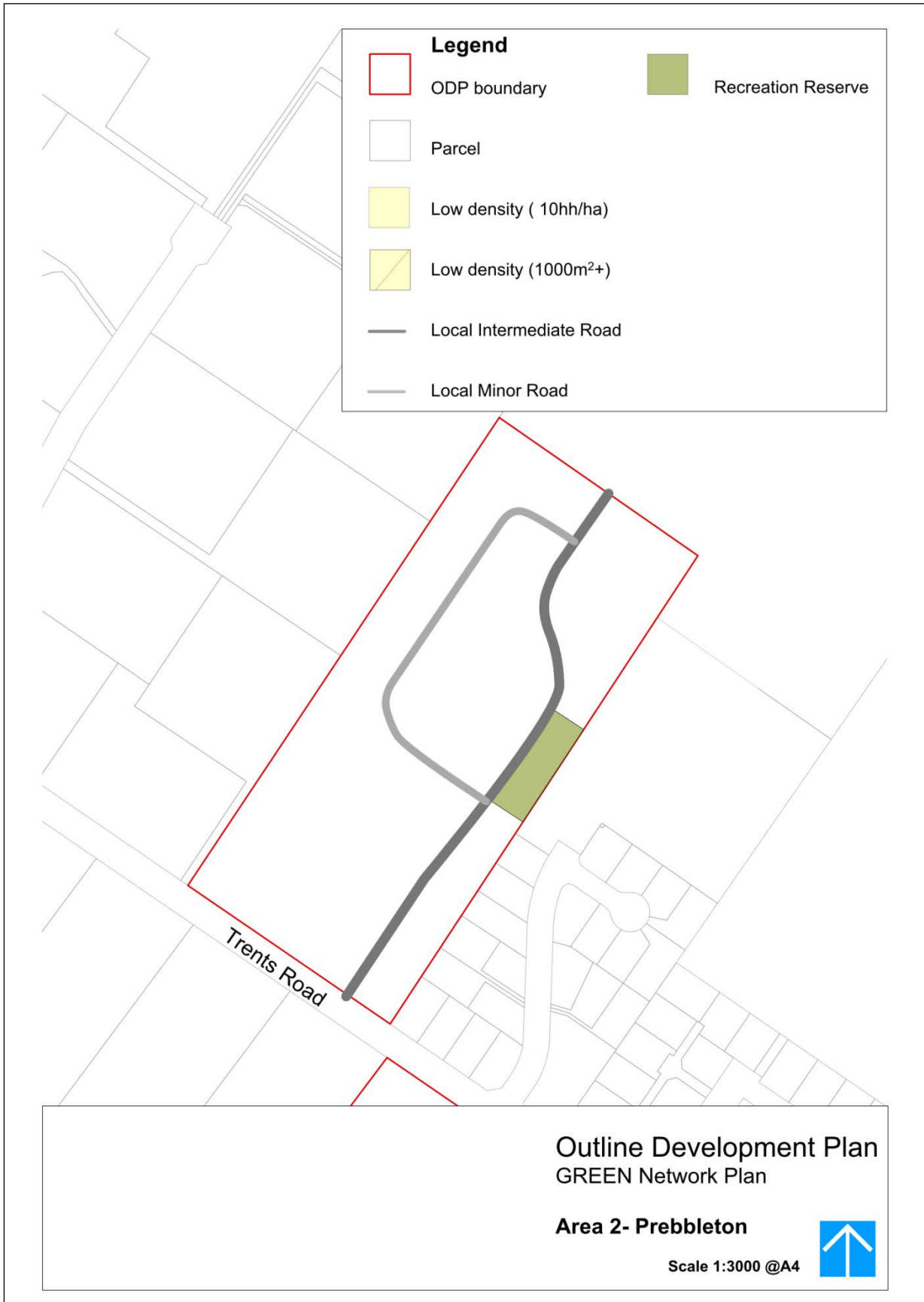
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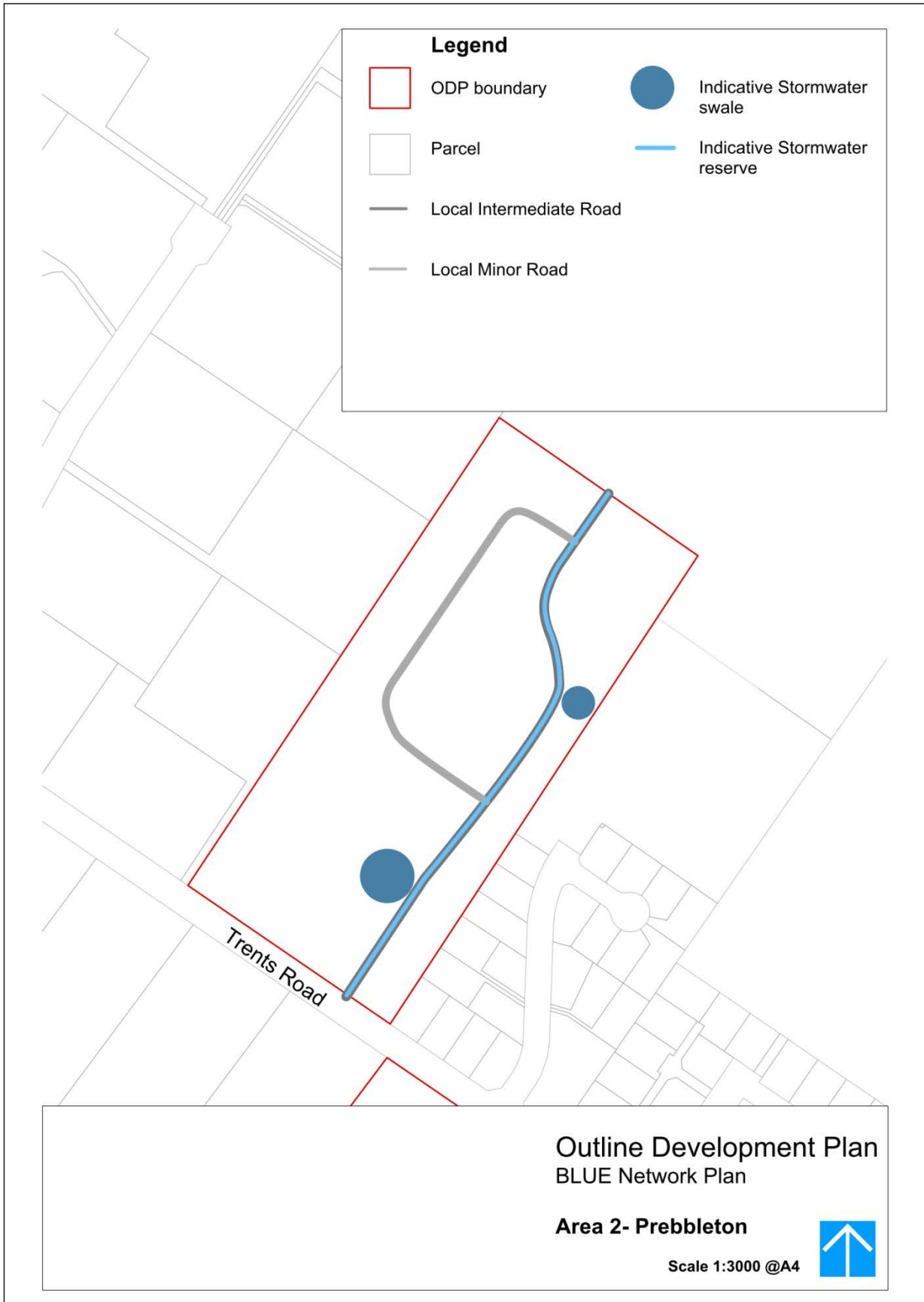












# **OUTLINE DEVELOPMENT PLAN AREA 3**

## **INTRODUCTION**

This Outline Development Plan (ODP) is for Area 3, which is zoned Living Z. Area 3 incorporates 11.7ha of land comprising the five properties that are contained by Trents Road to the north, Hamptons Road to the south, the Sterling Park development (Living X zone) and Prebbleton Nature Park to the east and the Rural (inner Plains) zone to the west.

The ODP provides an overarching urban design framework to guide the future development of the land. The ODP includes Land Use, Movement, Green and Blue Networks and incorporates the wider strategic and community outcomes expressed in the Prebbleton Structure Plan.

## **URBAN DESIGN**

The design principles that underpin this ODP are in line with the New Zealand Urban Design Protocol and accord with the Selwyn District Council Subdivision Design Guide (September 2009). The following environmental outcomes are to be achieved:

- Development that meets the District Plan policies, realises an overall increase in residential density, applies urban consolidation principles and assists in achieving a compact concentric settlement pattern for Prebbleton.
- Provision for a range of section sizes and housing typologies to respond to the wider needs of the community, whilst achieving the prescribed minimum household densities and minimum average allotment sizes.
- Subdivision layouts that integrate with adjoining neighbourhoods and incorporate existing land uses where appropriate. The wider context of the development area should influence the subdivision layout by protecting and enhancing cultural, ecological, heritage and Te Taumutu values and existing built features, such as amenity trees and water races.
- Layouts and urban design treatments that create a distinguishable sense of place, assist in enhancing the wider character and amenity of Prebbleton and deliver safe, vibrant and healthy living environments. Layouts should apply Crime Prevention through Environmental Design (CPTED) design principles.
- Integrated and legible road hierarchy that supports safe and efficient connections and promotes walking and cycling. Road design and landscape treatments should contribute to the overall character of Prebbleton and assist in connecting residential development with open space reserves and other public assets and services within the township, such as the Domain, Primary School, Nature Park and the town centre.
- Sustainable methods to treat and dispose of stormwater that protect groundwater resources and surface waterways (including springs, water/stock races) from contamination, while integrating with open space and reserves where appropriate.
- Installation of all the necessary infrastructure services within the ODP area, and the cost effective and efficient connection of those services to the wider network.

## **LAND USE**

The ODP Area shall achieve a minimum net density of 10 households per hectare. Dwellings must front Springs Road and Trents Road to enhance passive surveillance and safety, while creating a high amenity streetscape. Appropriate design layouts should take into consideration the shape, orientation and aspect of sections, with internal roads and access arrangements that support housing that fronts onto Springs Road and Trents Road.

Residential housing established along the boundary with the recreation reserve and Prebbleton Nature Park must front these reserves. Appropriate interface treatments promote passive surveillance and support front yards facing towards the reserves. Streetscapes that are comprised of tall fencing or screening that may undermine the amenity afforded by the reserve are to be avoided. Suitable methods, such as fencing controls and set backs, should be formalised at subdivision stage to ensure all future residential development overlooking these reserves benefit from the high amenity and outlook provided by the

Prebbleton Nature Park and future recreation reserve. Appropriate perimeter planting and fencing must be established along the western boundary of the development block to define the urban/rural interface and avoid adverse reverse sensitivity effects.

## MOVEMENT NETWORK

The proposed roading network consists of a Local Major Road that connects Springs Road with Trents Road. On-road cycling is provided for within the Local Major Road to support the wider circular walking and cycling network identified within the Prebbleton Structure Plan, including safe access to the Prebbleton Nature Park. The northern point of this Local Major Road must align with the entrance to Lindsay Drive to create a cross roads and promote connectivity to adjacent neighbourhoods.

The ODP requires a secondary east-west connection to Sterling Drive. Future indicative connections have been identified between Area 3 and the rural land holdings to the west. This will facilitate connectivity and expansion of the network should this land be developed to residential densities in the future.

Local Minor Roads provide access along the north-western boundary of the Prebbleton Nature Park and the southern boundary of the proposed recreation reserve.

Direct through connections from the development block to Hamptons Road are to be avoided to reduce the potential for conflict with future road users. This road is proposed to be upgraded in the future in accordance with the Christchurch Rolleston and Environs Transportation Study 2007.

A detailed engineering assessment is required to establish the proximity roads able to be established within Area 3 where the land adjoins the Prebbleton Nature Park and the former quarry contained within it. This is to ensure that any roads, or subterranean servicing contained within it, do not undermine the stability of the quarry face or contribute to increased road maintenance and upgrade costs in the future.

The remaining internal roading layout must be able to respond to the possibility that this area may be developed progressively over time. Road alignments must be arranged in such a way that long term inter-connectivity is achieved once the block is fully developed.

## GREEN NETWORK

The ODP shows a single open space reserve that is 3,000m<sup>2</sup> in size. The location of the recreation reserve has been determined based on the amount of reserves established in the wider area and to ensure people living within the development block have access to open space within a 400m walking radius of their homes.

An additional open space setback between the Prebbleton Nature Park and the future roads on the north-western and south-western boundaries may be necessary. This is to ensure the roads are sufficiently set back from the former quarry face contained within the reserve. Further detailed engineering investigations will determine the setback needed between the quarry face and the road.

Further investigations shall be undertaken at subdivision to determine the practicalities of retaining the following existing specimen trees within any future layout:

- Plantings and ornamental shrubs along the eastern boundary of the development block with Springs Road, which are contained within the legal boundary of 670 Springs Road (Lot 1 DP 25587). These plantings are recognised as an attractive gateway feature when entering the township and they compliment the amenity of the adjoining Prebbleton Nature Park.
- The black beech/ *Nothofagus solandri*, blue cedar/ *Cedrus atlantica* and copper beech/ *Fagus sylvatica 'Purpurea'* trees contained within the residential section at the corner of Hamptons and Springs Roads, being 146 Hamptons Road (Lot 1 DP 19741).

These amenity trees provide a link to the previous use of the land, compliment the streetscape and assist in preserving and enhancing the character of Prebbleton. Options to secure the on-going protection of these specimens if retained include consent notices or private covenants to assure the trees longevity.

## BLUE NETWORK

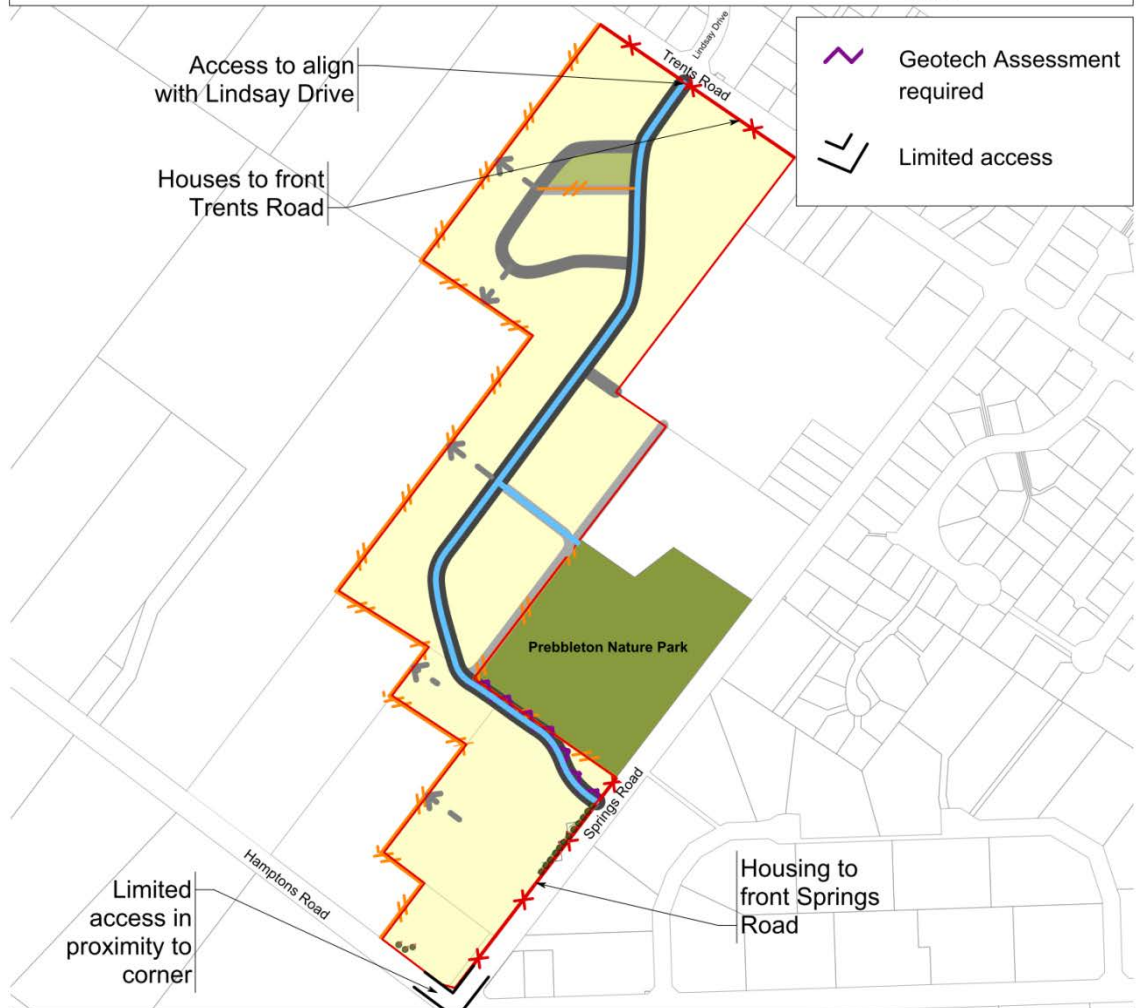
There is sufficient capacity in both the Springs Road and Trent's Road water main and wider network to support the additional water connections required to service the area, with works having to accord with the upgrades identified for the township.

An existing pumping station is located on the corner of Trent's Road and Lindsay Drive. Access to the Springs Road sewer main is restricted. The anticipated wastewater solution is to establish a northern connection to the Trent's Road and Lindsay Drive pumping station.

There are a range of methods available to collect, treat and dispose of stormwater. Options include the discharge of hard surface and roof run-off within residential sections to be disposed on site via soakage pits and for run-off being collected in roadside swales and then discharged to ground via soakage pits within the proposed development area. Stormwater associated with large rainfall events may be directed to the Prebbleton Nature Park and discharged to ground. Account will need to be made for the amenity and ecological values of this reserve and the extent to which this would not be undermined by the intermittent stormwater discharges if the option to discharge into the Prebbleton Nature Park is pursued. The roadside swales referenced on the ODP plan are indicative only. Detailed stormwater solutions are to be determined by the developer in collaboration with Council at subdivision stage and in accordance with Environment Canterbury requirements. Stormwater solutions should integrate into both the road and reserve environments where practicable. The establishment of riparian margins and low impact stormwater management techniques are encouraged where appropriate to establish and enhance ecological corridors, habitats and Te Taumutu values attributed to the water resource.

**Legend**

- |   |                        |   |                                |   |   |
|---|------------------------|---|--------------------------------|---|---|
|  | ODP boundary           |  | Local Intermediate Road        |  | Recreation Reserve                      |
|  | Parcel                 |  | Local Minor Road               |  | Existing Reserve                        |
|  | Low density ( 10hh/ha) |  | Indicative future roading link |  | Potential amenity trees (private space) |
|  | Local Major Road       |  | Interface treatment            |  | Indicative Stormwater swale             |
|   |                        |  | Housing to front road          |   |   |



**Outline Development Plan  
OVERALL ODP**

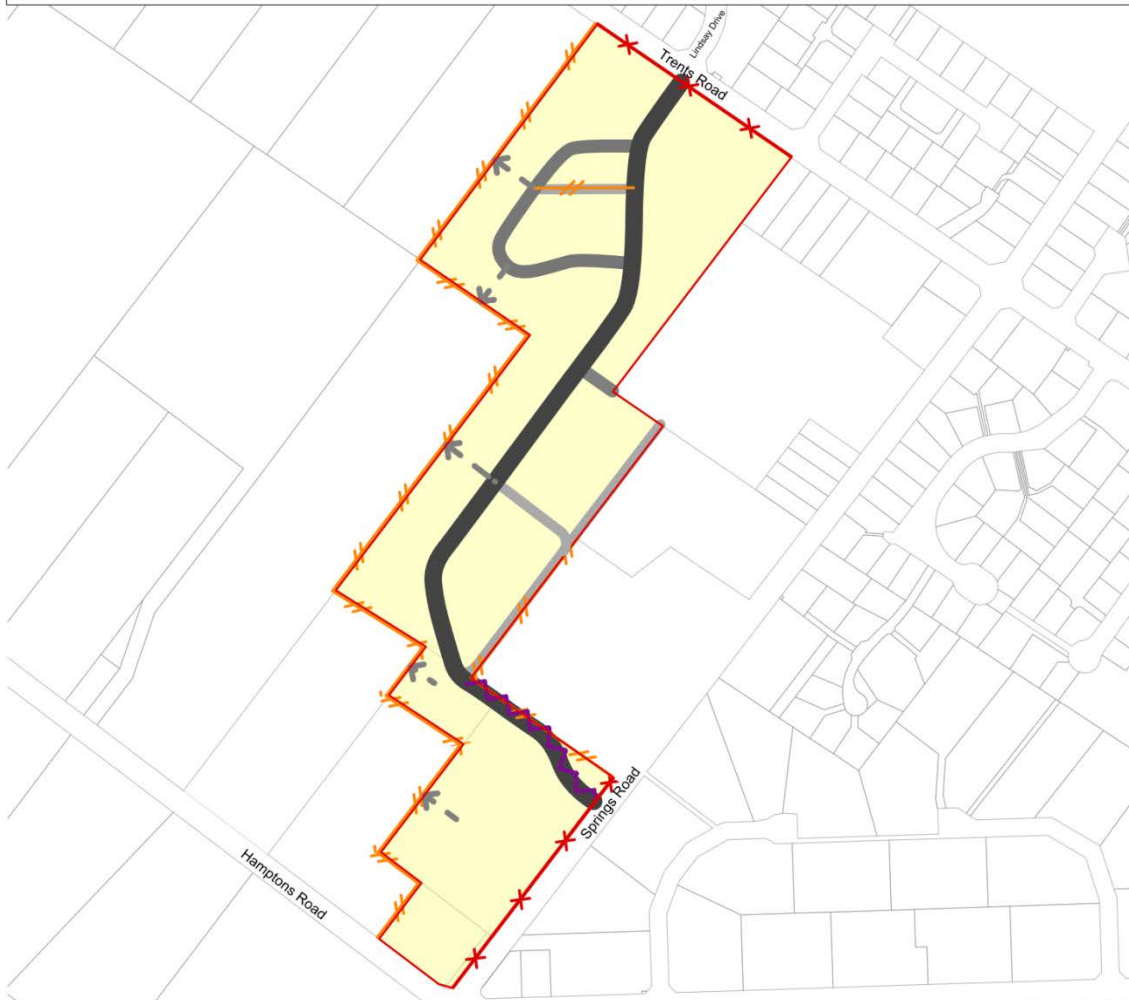
**Area 3- Prebbleton**

Scale 1:5000 @A4



**Legend**

- |   |                        |   |                                |   |                             |
|---|------------------------|---|--------------------------------|---|-----------------------------|
|  | ODP boundary           |  | Local Intermediate Road        |  | Geotech Assessment required |
|  | Parcel                 |  | Local Minor Road               |  | Limited access              |
|  | Low density ( 10hh/ha) |  | Indicative future roading link |  | Housing to front road link  |
|  | Local Major Road       |  | Interface treatment            |   |                             |



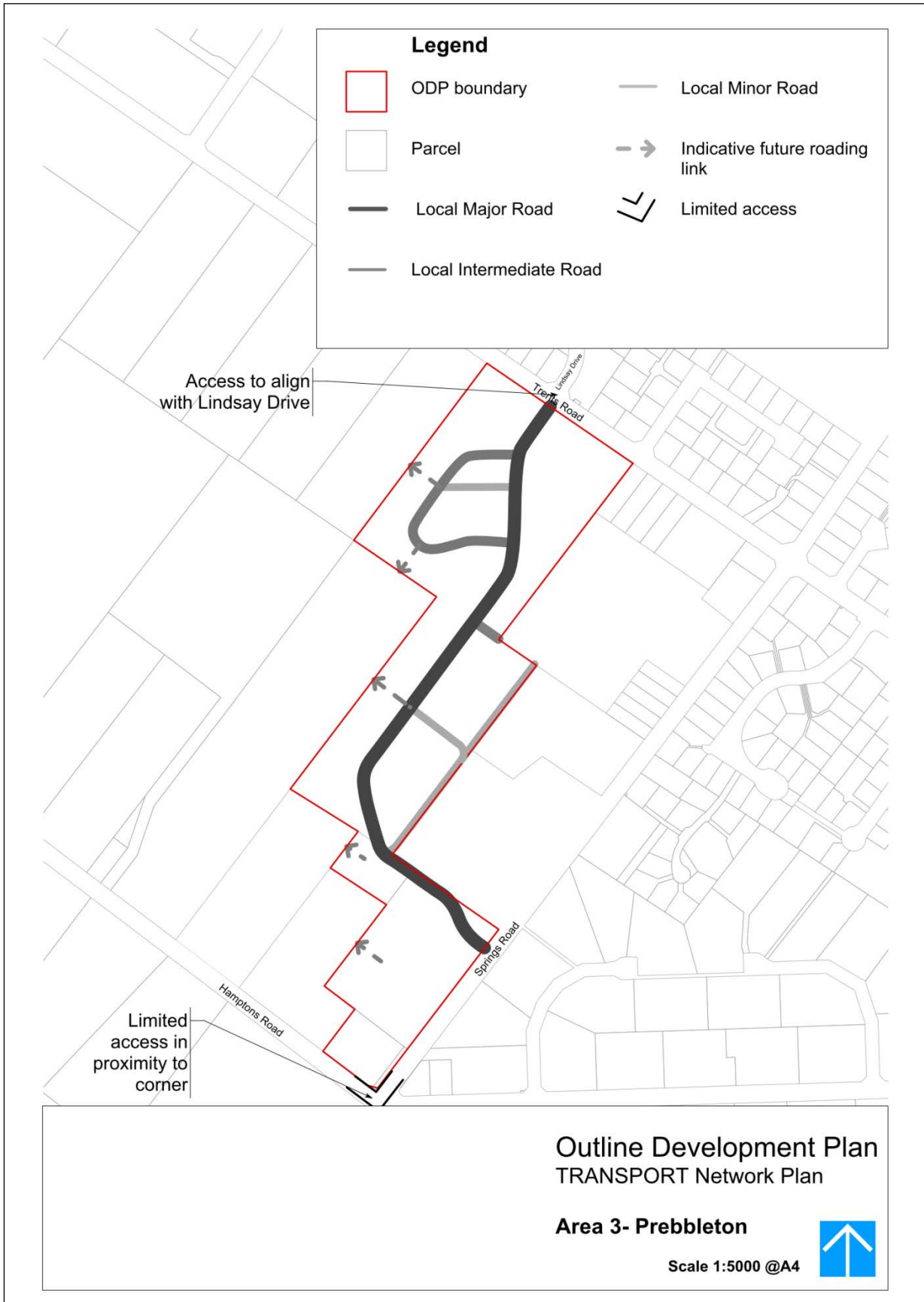
**Outline Development Plan  
LANDUSE Plan**

**Area 3- Prebbleton**

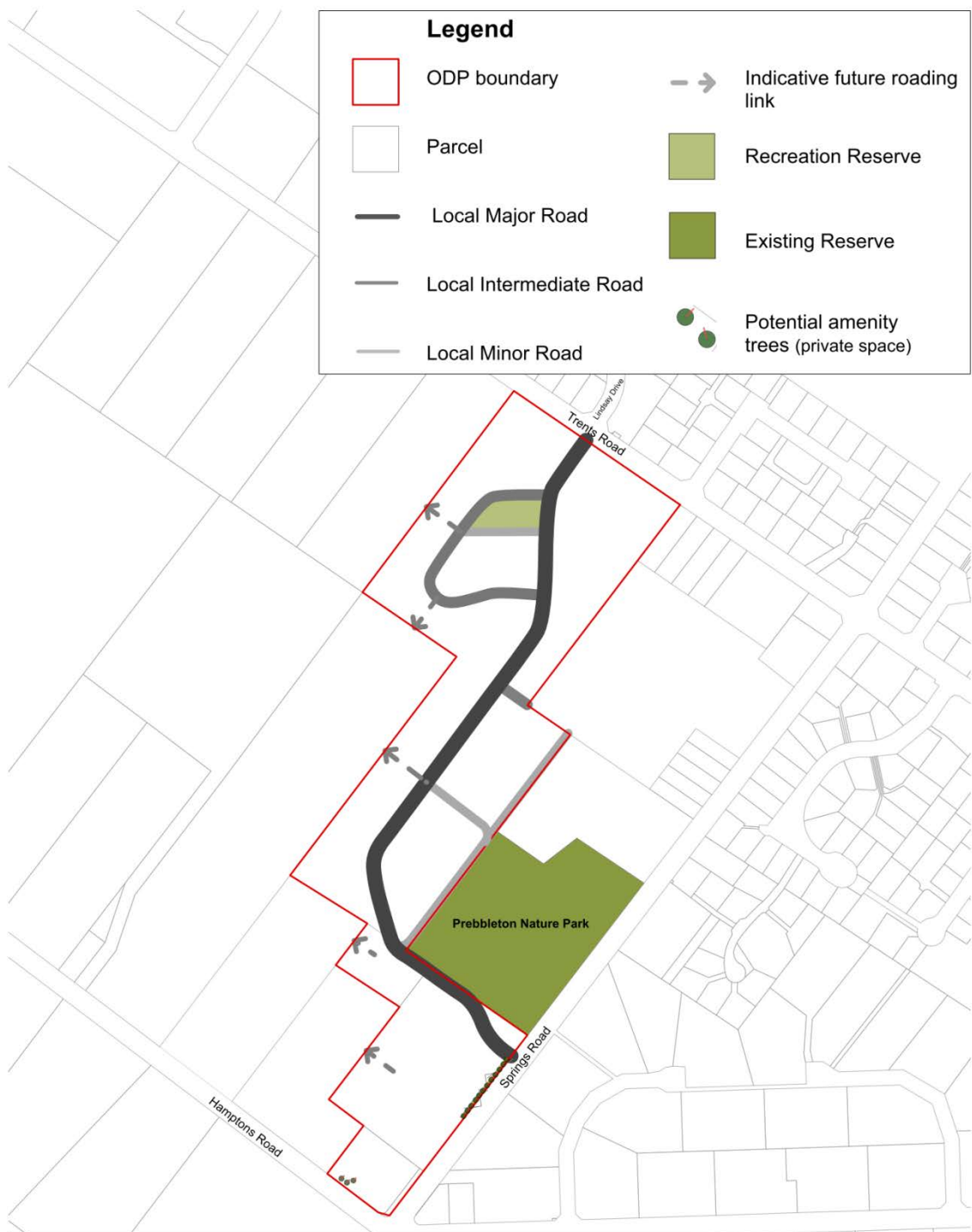
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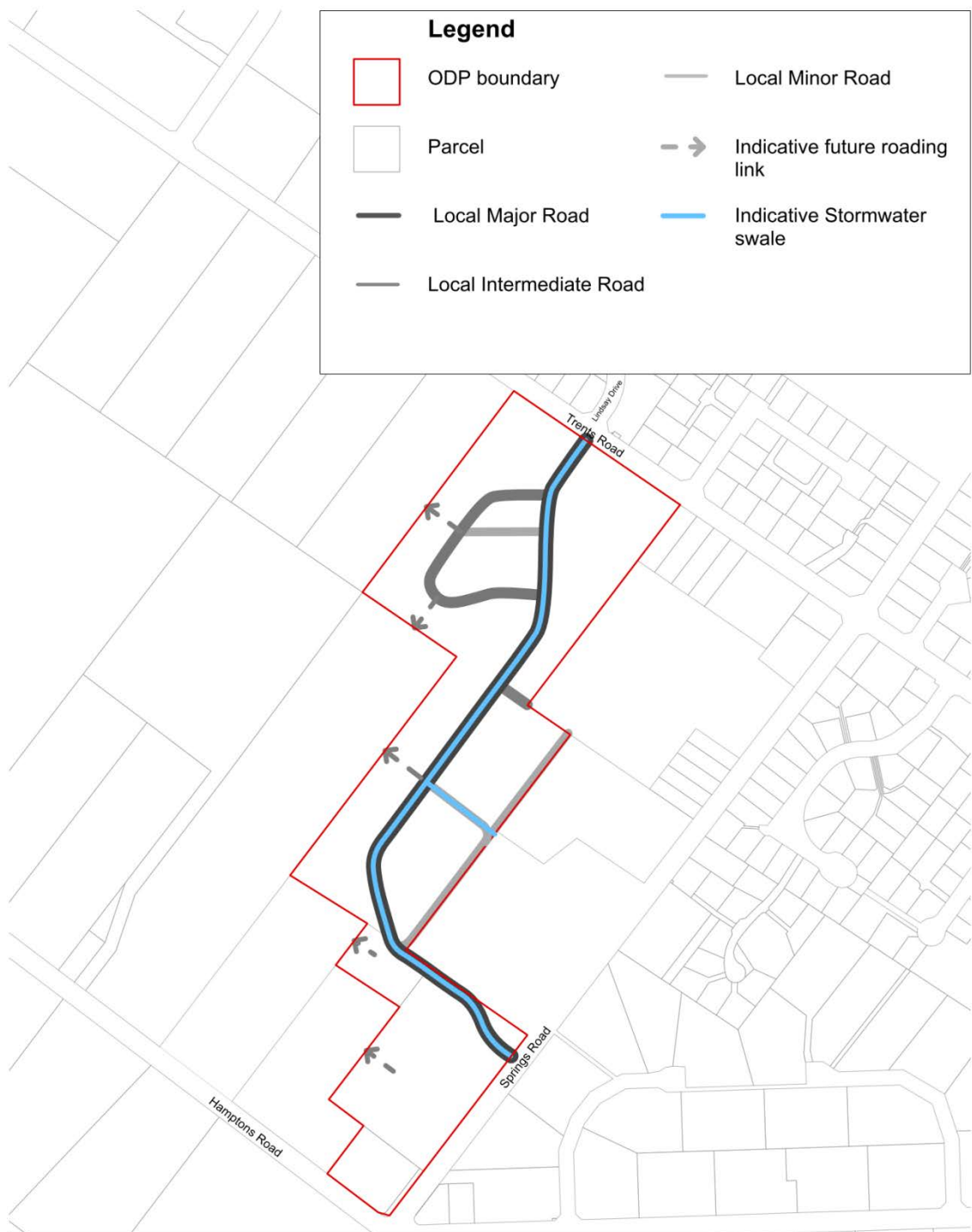
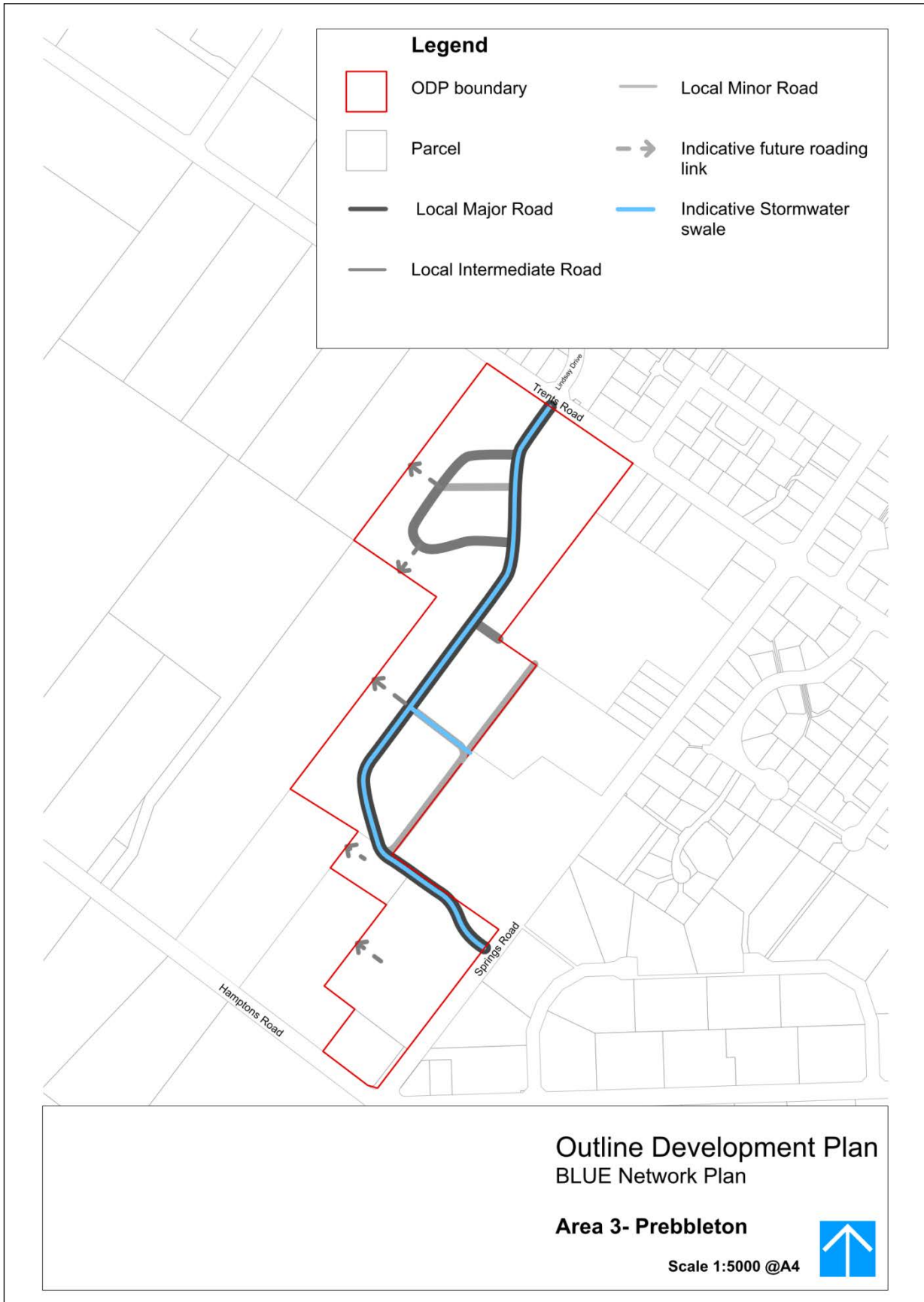


**Outline Development Plan  
GREEN Network ODP**

**Area 3- Prebbleton**

Scale 1:5000 @A4





# **OUTLINE DEVELOPMENT PLAN AREA 4**

## **INTRODUCTION**

This Outline Development Plan (ODP) is for Area 4, which is zoned Living Z. Area 4 includes 25.5ha of land generally comprising three properties that front onto Tosswill Road. The development block is contained by the Prebbleton Central development to the west (Living X zone), Rural (Inner Plains) zone to the north and east, and the Oakwood Mews development (Living 1A1 zone) to the south.

The ODP provides an overarching urban design framework to guide the future development of the land. The ODP includes Land Use, Movement, Green and Blue Networks and incorporates the wider strategic and community outcomes expressed in the Prebbleton Structure Plan.

## **URBAN DESIGN**

The design principles that underpin this ODP are in line with the New Zealand Urban Design Protocol and accord with the Selwyn District Council Subdivision Design Guide (September 2009). The following environmental outcomes are to be achieved:

- Development that meets the District Plan policies, realises an overall increase in residential density, applies urban consolidation principles and assists in achieving a compact concentric settlement pattern for Prebbleton.
- Provision for a range of section sizes and housing typologies to respond to the wider needs of the community, whilst achieving the prescribed minimum household densities and minimum average allotment sizes.
- Subdivision layouts that integrate with adjoining neighbourhoods and incorporate existing land uses where appropriate. The wider context of the development area should influence the subdivision layout by protecting and enhancing cultural, ecological, heritage and Te Taumutu values and existing built features, such as amenity trees and water races.
- Layouts and urban design treatments that create a distinguishable sense of place, assist in enhancing the wider character and amenity of Prebbleton and deliver safe, vibrant and healthy living environments. Layouts should apply Crime Prevention through Environmental Design (CPTED) design principles.
- Integrated and legible road hierarchy that supports safe and efficient connections and promotes walking and cycling. Road design and landscape treatments should contribute to the overall character of Prebbleton and assist in connecting residential development with open space reserves and other public assets and services within the township, such as the Domain, Primary School, Nature Park and the town centre.
- Sustainable methods to treat and dispose of stormwater that protect groundwater resources and surface waterways (including springs, water/stock races) from contamination, while integrating with open space and reserves where appropriate.
- Installation of all the necessary infrastructure services within the ODP area, and the cost effective and efficient connection of those services to the wider network.

## **LAND USE**

The ODP Area shall achieve a minimum net density of 10 households per hectare. Lower density allotments are necessary on the north-eastern boundary of Area 4. This will assist to integrate the site with the adjoining Rural (Inner Plains) zone, to preserve views towards the Port Hills and to increase the separation between future housing and Transpower's 220kV electricity pylons and lines located further to the north-east. Appropriate interface treatments at the boundary between residential and rural activities, and methods to protect these treatments in the long term, shall be established, including appropriate fencing, landscaping and minimum building set backs.

The ODP identifies a Medium Density area situated along the alignment of an open space corridor. The open space corridor provides a high amenity feature that compliments more intensive housing typologies. Its location within the centre of the development area ensures that any effects arising from this form of development on established living environments are internalised to within the development block. The

development blocks proximity to the town centre and the establishment of the open space corridor incorporating a pedestrian and cycling connection promotes ready access to the town centre, Prebbleton Primary School, proposed Domain extension and other services established within the township. Provision must be made for appropriate layouts and housing designs that accord with Council's Medium Density Housing Guide and the District Plan. The ODP requires the open space corridor to form a focal point for residential housing. Permeable fencing (a minimum of 50% transparency along the full length of the fence) and fencing setbacks (5m) are to be formalised to ensure a high quality living environment is established. Road layouts and the size, shape and orientation of these Medium Density sections need to be designed in such a way as to protect the amenity of the open space corridor. CPTED principles should also be applied to promote passive surveillance.

Residential housing established along the boundary with the proposed Domain extension must front this reserve. Appropriate interface treatments must promote passive surveillance, support front yards facing towards the reserve and avoid a streetscape that is comprised of tall fencing or screening that may undermine the amenity afforded by the future reserve. Suitable methods, such as fencing controls and set backs, should be formalised to ensure all future residential development overlooking these reserves benefit from the high amenity and outlook at subdivision stage.

Dwellings must front Tosswill Road to enhance passive surveillance and safety, while creating a high amenity streetscape. Appropriate design layouts should take into consideration the shape, orientation and aspect of sections, with internal roads and access arrangements that support housing that fronts onto Tosswill Road.

### MOVEMENT NETWORK

The proposed roading network is focused around the Local Major Road that connects Station Masters Way with Tosswill Road and the open space corridor that connects the proposed domain extension with the town centre.

On-road cycling is provided for within the Local Major Road to support the wider circular walking and cycling network identified within the Prebbleton Structure Plan. Off-road pedestrian and cycling connections are also identified along the full length of the open space corridor to support safe connections between the proposed domain extension and the town centre. It also provides an alternative alignment of the Christchurch to Little River Rail Trail. Dedicated walking and cycling links are also identified between the Local Major Road and the western point of the open space corridor, and between the Local Intermediate Road and Hodgens Road to the north-east. This road alignment supports section layouts that optimise the amenity provided by the open space corridor, while promoting a well integrated development.

The Local Intermediate and Local Minor Road network connects with Conductors Way and Platform Way established within the Prebbleton Central (Living X zone) to the west. The ODP makes specific provision for a Local Minor Road along the alignment of the open space corridor to the north to ensure section layouts facilitate future development that is well integrated into this high amenity feature. A Local Intermediate Road along the south-eastern boundary with the proposed domain extension also aims to support urban design outcomes and preserve the open space amenity associated within this proposed reserve. The same principles apply to the Local Intermediate Road along the south eastern boundary of the proposed Domain extension.

The remaining internal roading layout must be able to respond to the possibility that this area may be developed progressively over time. Road alignments must be arranged in such a way that long term inter-connectivity is achieved once the block is fully developed.

### GREEN NETWORK

The ODP requires the establishment of an open space corridor that generally follows the alignment of the existing drain that services the area. The alignment and orientation of this open space corridor has been established to support overland flows for stormwater, secure a green space link that protects easterly views to the Port Hills and accommodate a primary pedestrian and cycling connection that links the town centre with the proposed domain extension.

The open space corridor shall be 20m wide on average along its entire length, although this width is able to vary in places depending upon road layouts, reserve provision and use, stormwater attenuation areas and the alignment of pedestrian and cycle ways.

A 2,200m<sup>2</sup> open space reserve is required to be established along the open space corridor to accommodate the large mature macrocarpa/ *Cupressus macrocarpa* tree. This amenity tree is an important land mark and amenity feature that provides a link to Prebbleton's historic past. The tree is contained within the legal boundary of 93 Tosswill Road (Lot 1 and Part Lot 2 DP 5464).

Further investigations shall be undertaken at subdivision to determine the practicalities of retaining the following existing specimen trees within any future layout:

- The row of Alder/*Alnus* trees and hedgerow that extends along a portion of the Tosswill Road frontage of the development block at 55 Tosswill Road (Lot 1 DP 3394 and Lot 2 DP 400006) should be investigated further to confirm that retention is feasible based on road shading, access arrangements and on-going maintenance.

These amenity trees provide a link to the previous use of the land, compliment the streetscape and assist in preserving and enhancing the character of Prebbleton. Options to secure the on-going protection of these specimens if retained include consent notices or private covenants to assure the trees long term retention.

## BLUE NETWORK

There is sufficient capacity in the Tosswill Road water main and wider network to support the additional water connections required to service the area, with works having to accord with the upgrades identified for the township.

The anticipated wastewater solution is to establish a western connection to the pumping station within the Prebbleton Central development (Living X zone) to enable access to the sewer gravity main.

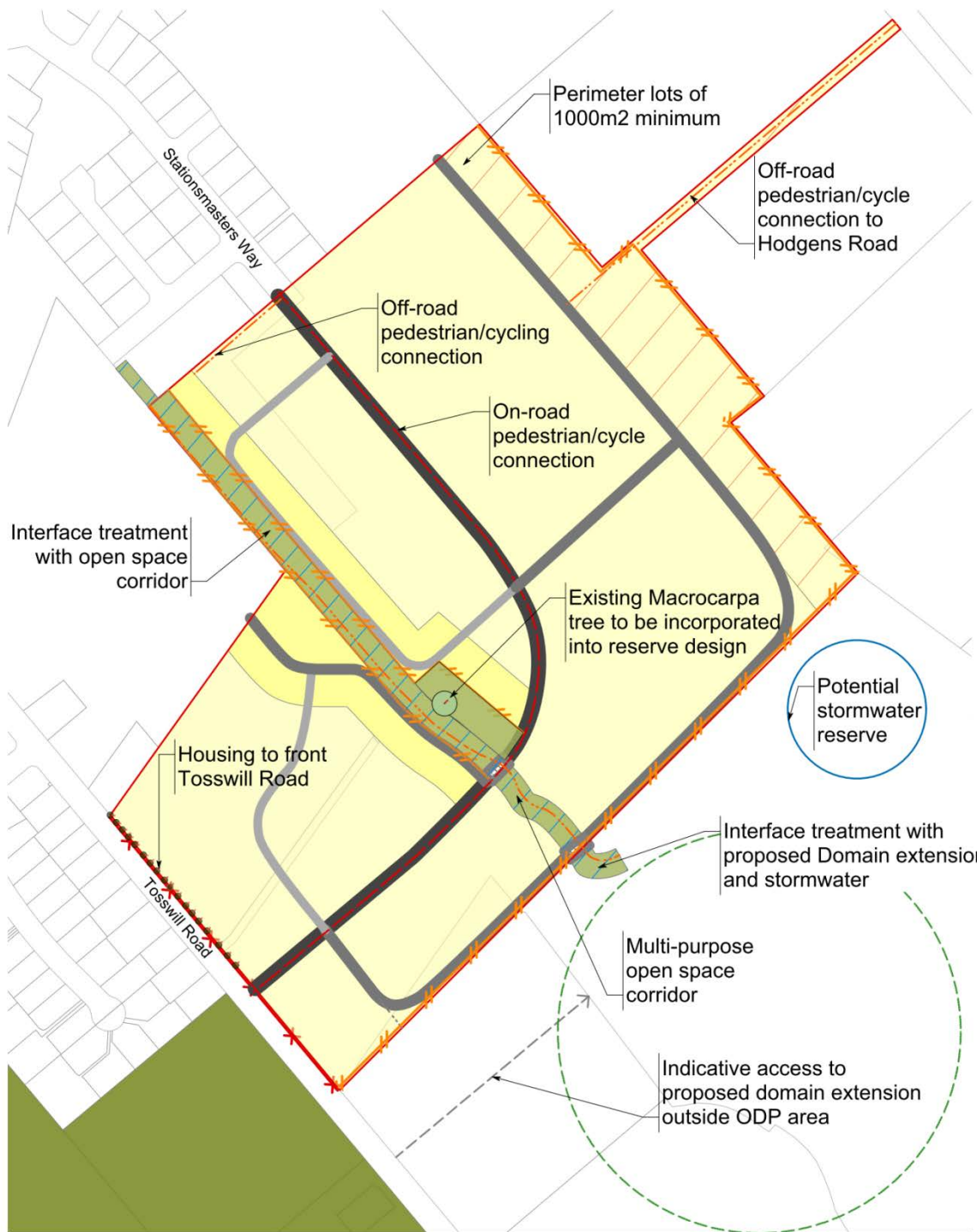
Opportunity exists for a catchment wide approach to manage, treat and dispose of stormwater within an integrated treatment facility to be established to the south-east of the development block. This facility would treat and dispose of stormwater from the areas within the township that are served by the reticulated stormwater network, the Business 1 zone, Prebbleton Central subdivision (Living X zone) and ODP Area 4 to ensure discharges are sufficiently detained within this catchment. An area of 6.4ha north of the proposed Domain extension is anticipated to be required for the stormwater ponds and riparian margins, with overland flow paths being established within the open space corridor.

The overall stormwater solution should integrate with the wider transport and reserve network, including the proposed domain extension, walking and cycling network and open space corridor. Detailed stormwater solutions are to be determined by the developer in collaboration with Council and in accordance with Environment Canterbury requirements. This will involve the development of an integrated stormwater management scheme generally located to the east of the development area and north of the proposed domain extension, with stormwater being directed along the open space corridor. Riparian planting along the existing drain and the formation of wetland environments that support ecological, cultural and Te Taumutu values will also be a key component of the overall design. 'Spring reserves' are to be provided where necessary to separate spring water from stormwater flows to protect the Te Taumutu values of the spring water.

The existing stormwater facility that services the Prebbleton Central subdivision (Living X zone) and town centre could be decommissioned once the catchment wide stormwater treatment facility is established. If this was to occur, the resulting land may then be integrated into the open space corridor, with the balance being able to be developed to accommodate Medium Density housing.

On-site stormwater management that satisfies all Environment Canterbury requirements will be necessary until such time as the integrated stormwater management scheme is established.





Please refer to page [E41-031](#) for Legend

## Outline Development Plan OVERALL ODP

### Area 4- Prebbleton

Scale 1:4000 @A4



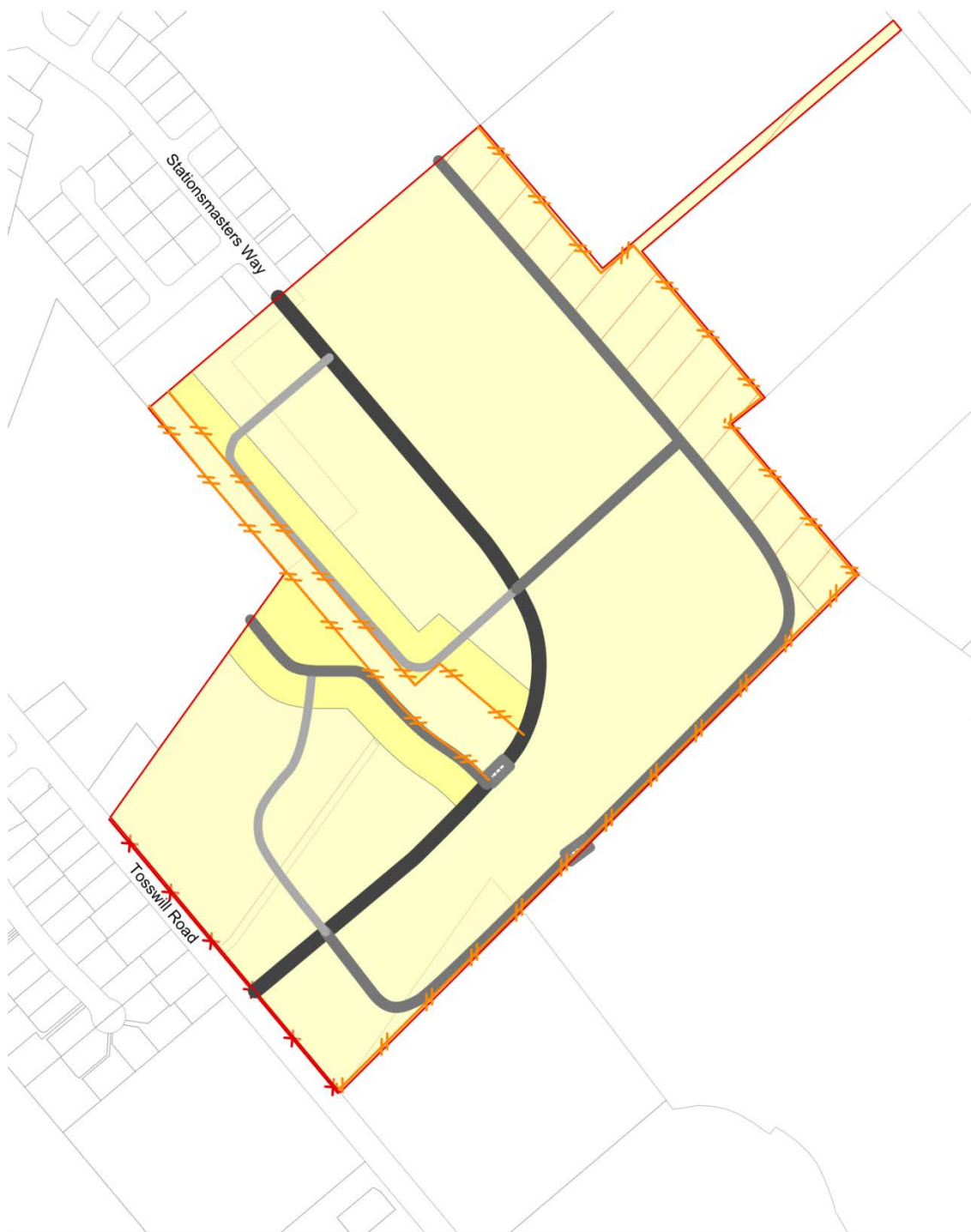
## LEGEND

	ODP boundary		Recreation reserve
	Parcel		Open Space corridor& links
	Low Density (10hh/ha)		Existing reserve
	Low Density (1000m2 +)		Potential domain extension
	Local Major Road		Amenity tree (public space)
	Local Intermediate Road		Possible amenity tree (private space)
	Local Minor Road		Indicative Stormwater swale
	Future Rooding connection		Potential Stormwater reserve outside ODP area
	On-road cycle way		Interface treatments
	Off-road cycle way		Housing fronting road

Please use [this Legend](#) for ODP plans

Outline Development Plan  
LEGEND

**Area 4- Prebbleton**



Please refer to page [E41-031](#) for Legend

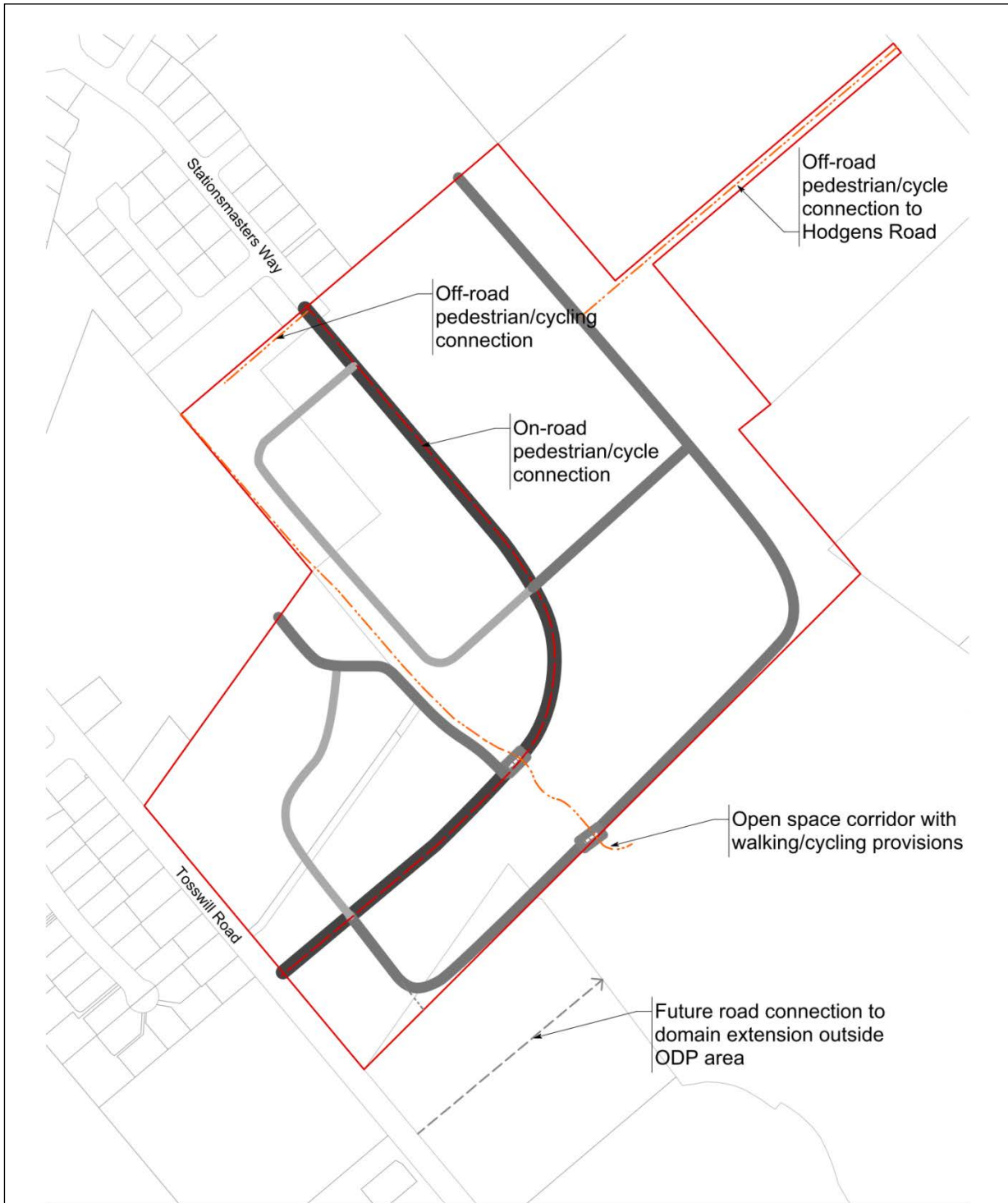
**Outline Development Plan**  
LANDUSE Plan

**Area 4- Prebbleton**

Scale 1:4000 @A4








Please refer to page [E41-031](#) for Legend

**Outline Development Plan**  
**TRANSPORT Network Plan**

**Area 4- Prebbleton**

Scale 1:4000 @A4





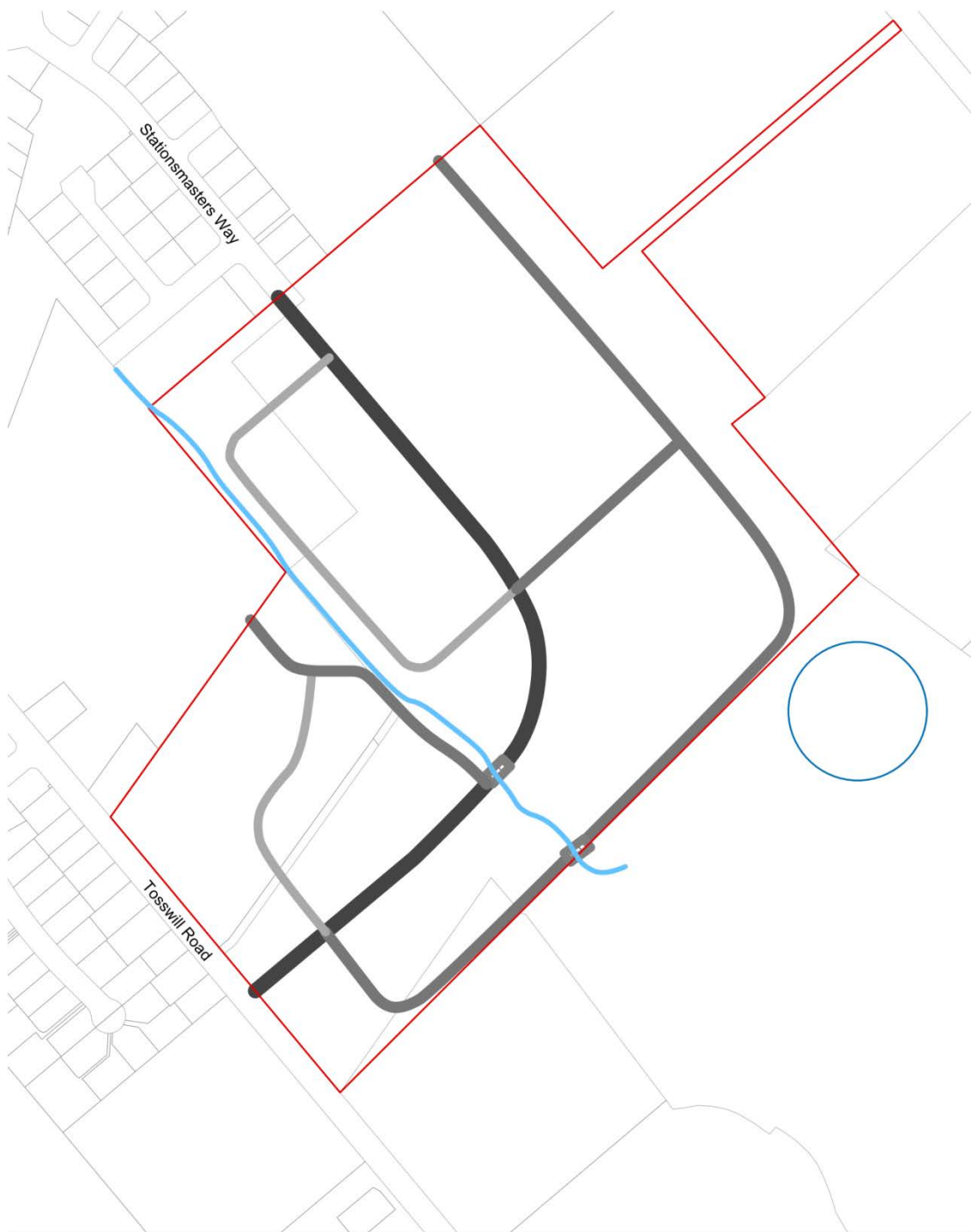
Please refer to page [E41-031](#) for Legend

**Outline Development Plan  
GREEN Network Plan**

**Area 4- Prebbleton**

Scale 1:4000 @A4





Please refer to page [E41-031](#) for Legend

**Outline Development Plan**  
BLUE Network Plan

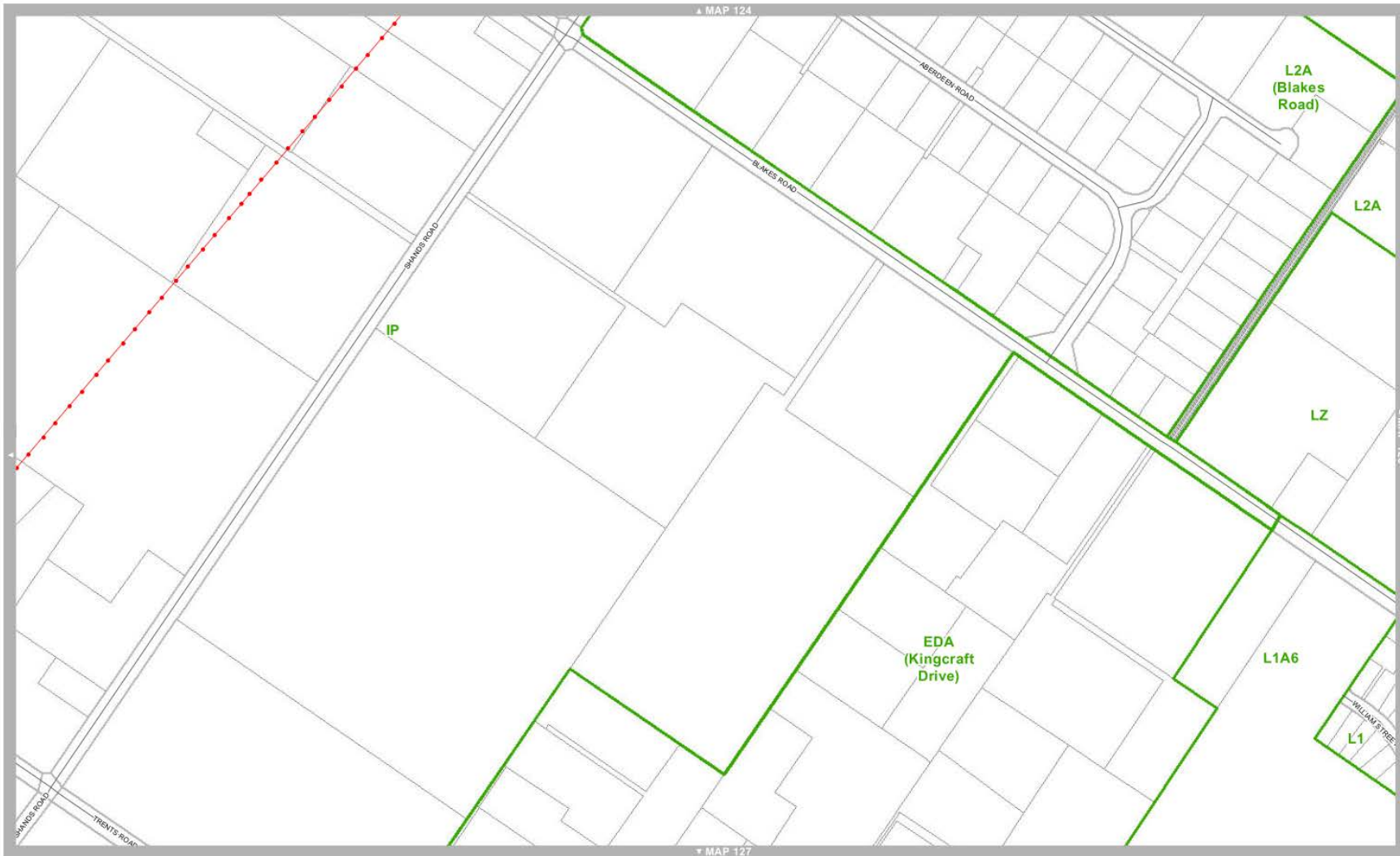
**Area 4- Prebbleton**

Scale 1:4000 @A4



# **Attachment 4**

## **Selwyn District Plan Maps Prebbleton**



MAP  
**125**  
SHEET 1

SELWYN DISTRICT PLAN

Scale: 1:4,000 at A3



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KEY



SDC Designated Site



Other Designations



ChCh International Airport Noise Contour



NZTA Widening Designation



Fault Lines



Coastal Hazard



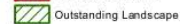
Transpower Main Lines



Zones



Forestry Exclusions



Outstanding Landscape

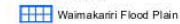


West Melton Observatory Zone

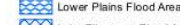


Outstanding Natural Feature

Flood Zone



Waimakariri Flood Plain

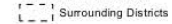


Lower Plains Flood Area

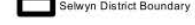


Lake Ellesmere Flood Area

Territorial Authority

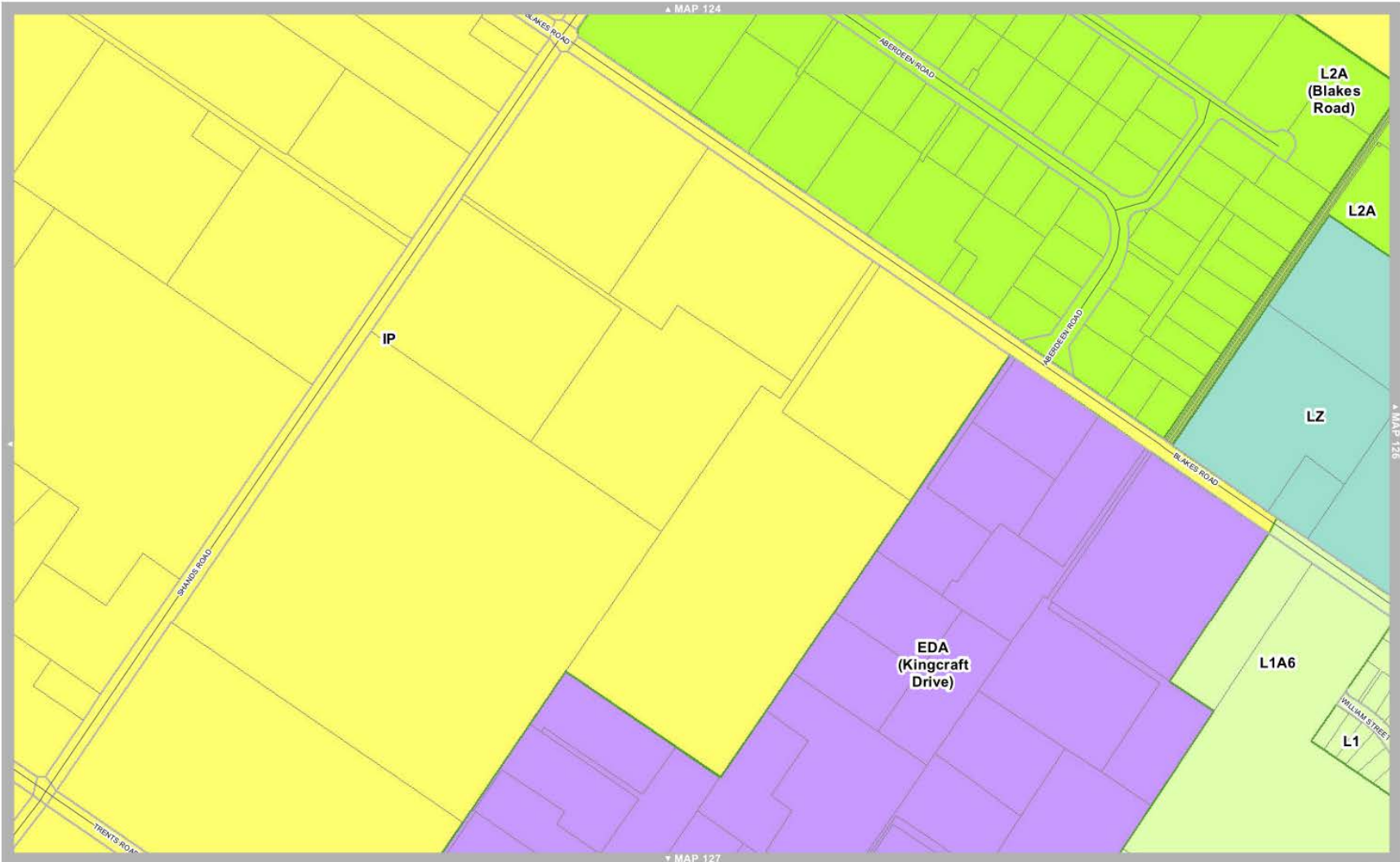


Surrounding Districts



Selwyn District Boundary





**KEY**

- |                             |                                 |                           |               |                   |                 |             |              |                     |
|-----------------------------|---------------------------------|---------------------------|---------------|-------------------|-----------------|-------------|--------------|---------------------|
| Heritage Building           | Protected Tree                  | Scheduled Site            | State Highway | Railway           | Road Parcel     | Land Parcel | Water Body   | Archaeological Site |
| Wāhi Taonga Management Site | Statutory Acknowledgement Site  |                           |               |                   |                 |             |              |                     |
| Silent File Area            | <b>Contours Above Sea Level</b> |                           | <b>Zones</b>  |                   | Living 1        | Living WM   | Business 1   | Outer Plains        |
| Wāhi Taonga Management Area | 160m                            | High Country              | Port Hills    | Living 2          | Living 3        | Living Z    | Business 2   | Malvern Hills       |
|                             | 60m                             | Existing Development Area | Living X      | Living Z Deferred | Deferred Living | Business 3  | Inner Plains |                     |
|                             | 2.74m                           |                           |               |                   |                 |             |              |                     |

MAP

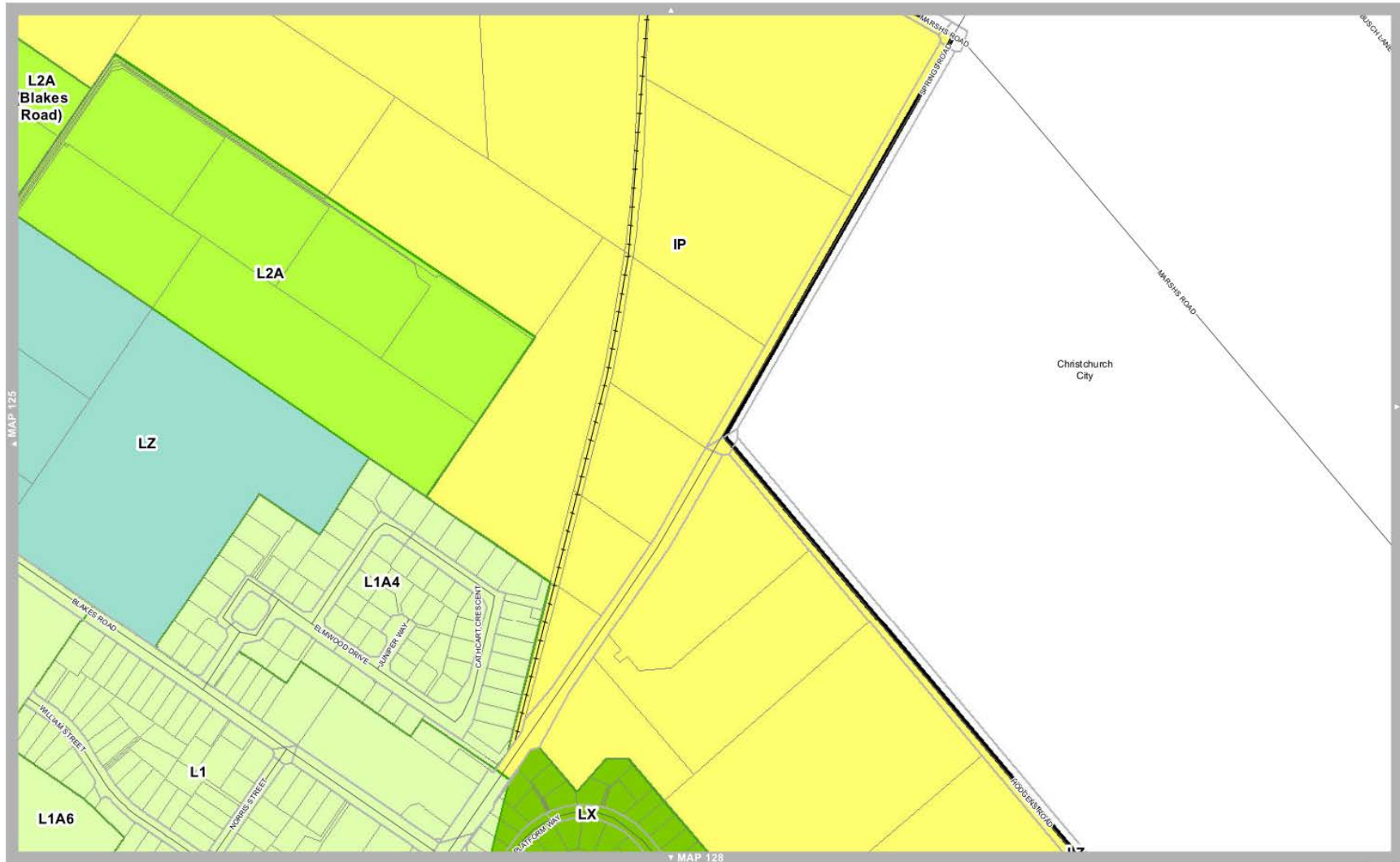
**125**  
SHEET 2



Plan Change 21 still in planning stage - Not Yet Operative

Printed on:  
Monday, 26 November 2012





**KEY**

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|--|--|---------------------------------|--|--------------|--|--|--|--|--|
|  |  |                                 |  |              |  |  |  |  |  |
|  |  | <b>Contours Above Sea Level</b> |  | <b>Zones</b> |  |  |  |  |  |
|  |  |                                 |  |              |  |  |  |  |  |
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MAP

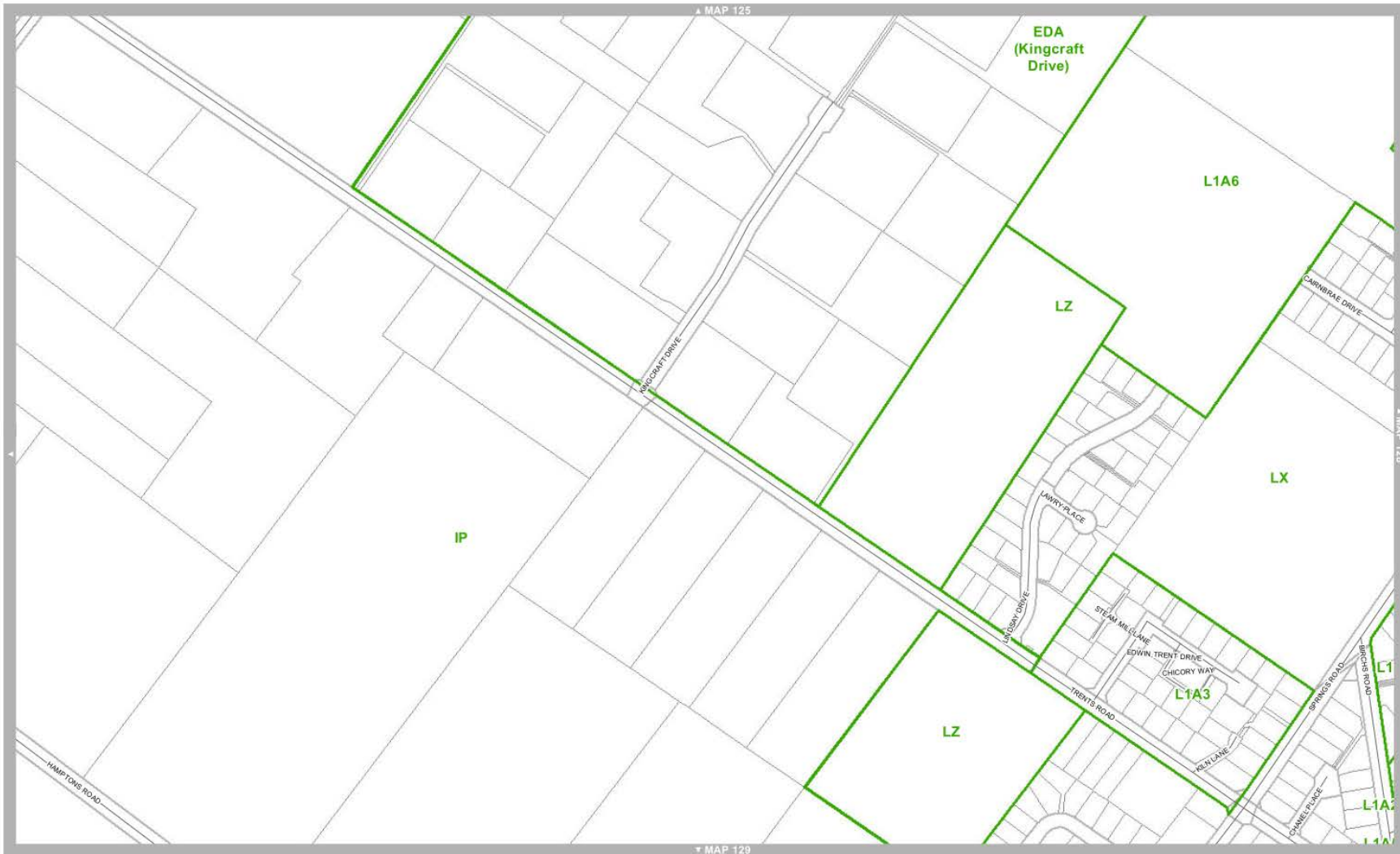
**126**  
SHEET 2



Plan Change 21 still in planning stage - Not Yet Operative

Printed on:  
Monday, 26 November 2012





MAP  
**127**  
SHEET 1

**SELWYN DISTRICT PLAN**

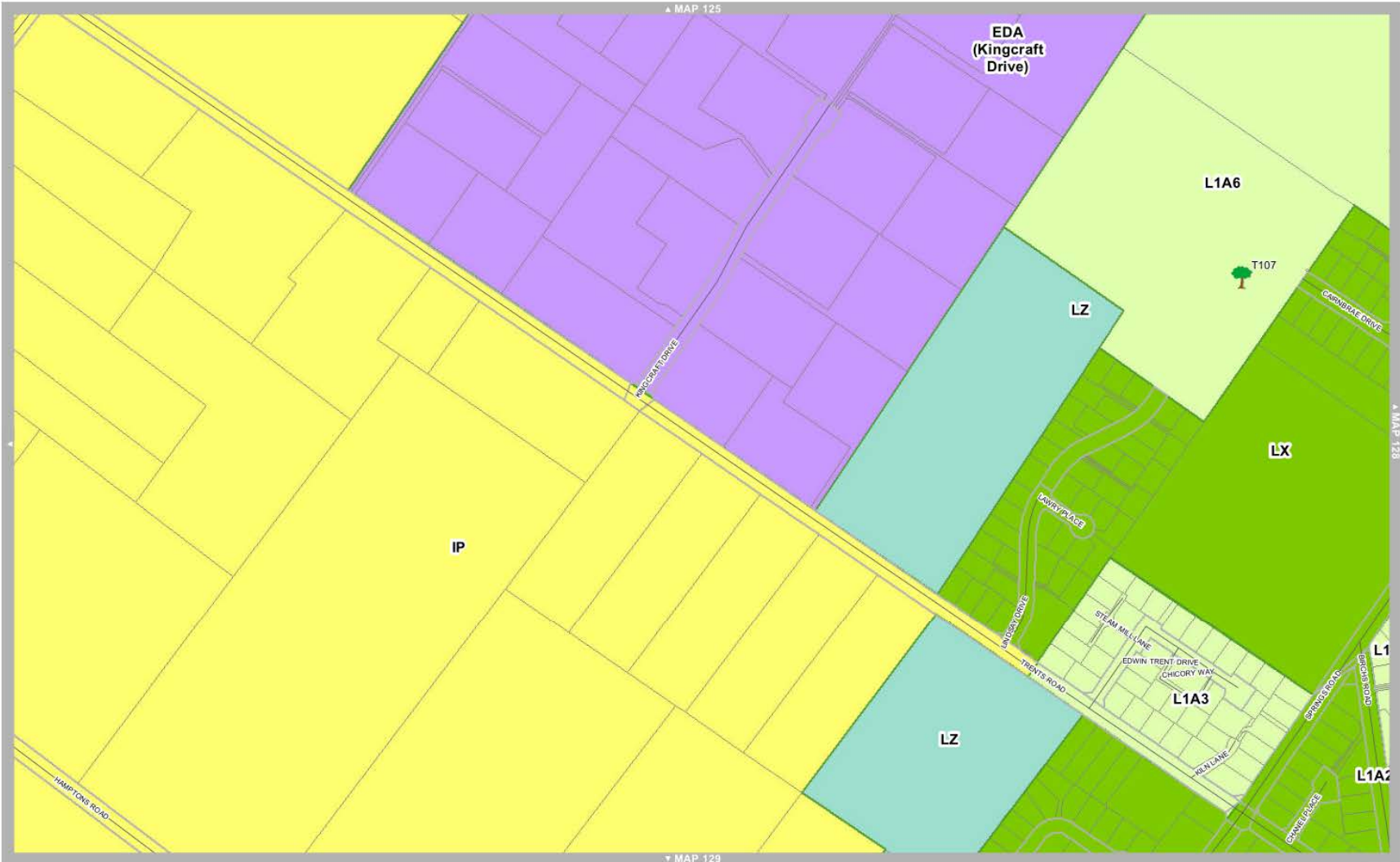
Scale: 1:4,000 at A3



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**KEY**

- |  |                       |                              |                           |                          |
|--|-----------------------|------------------------------|---------------------------|--------------------------|
| SDC Designated Site                      | Fault Lines           | Forestry Exclusions          | <b>Flood Zone</b>         | Surrounding Districts    |
| Other Designations                       | Coastal Hazard        | Outstanding Landscape        | Waimakariri Flood Plain   | Selwyn District Boundary |
| ChCh International Airport Noise Contour | Transpower Main Lines | West Melton Observatory Zone | Lower Plains Flood Area   |                          |
| NZTA Widening Designation                | Zones                 | Outstanding Natural Feature  | Lake Ellesmere Flood Area |                          |



**KEY**

- |              |                                 |  |  |  |  |  |  |  |
|--------------|---------------------------------|--|--|--|--|--|--|--|
|              |                                 |  |  |  |  |  |  |  |
|              |                                 |  |  |  |  |  |  |  |
|              | <b>Contours Above Sea Level</b> |  |  |  |  |  |  |  |
|              |                                 |  |  |  |  |  |  |  |
| <b>Zones</b> |                                 |  |  |  |  |  |  |  |
|              |                                 |  |  |  |  |  |  |  |
|              |                                 |  |  |  |  |  |  |  |

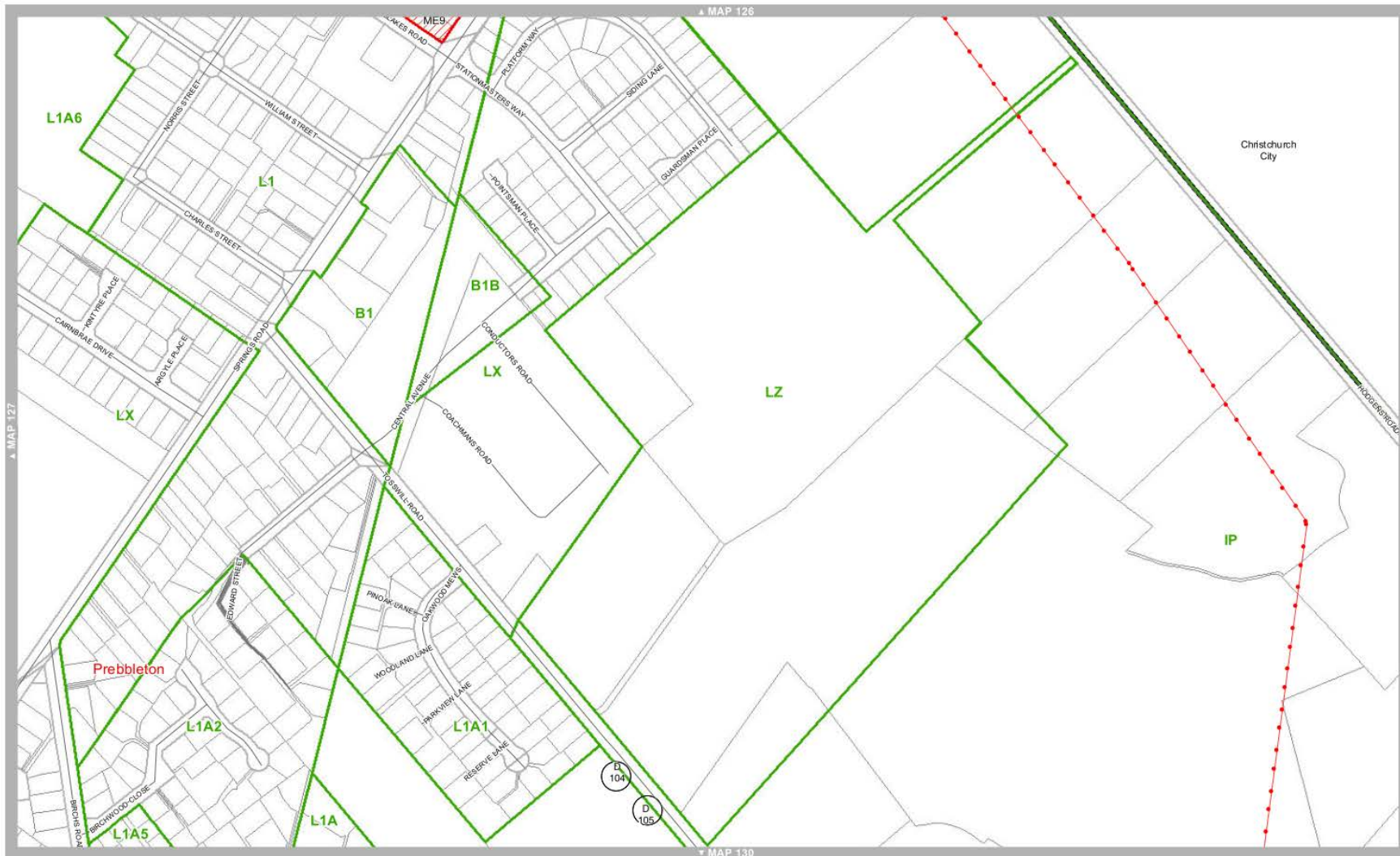
MAP

**127**  
SHEET 2



Plan Change 21 still in planning stage - Not Yet Operative

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Monday, 26 November 2012



MAP

128

SHEET 1

SELWYN DISTRICT PLAN

Scale: 1:4,000 at A3



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KEY

- SDC Designated Site
- Other Designations
- ChCh International Airport Noise Contour
- NZTA Widening Designation
- Fault Lines
- Coastal Hazard
- Transpower Main Lines
- Zones

- Forestry Exclusions
- Outstanding Landscape
- West Melton Observatory Zone
- Outstanding Natural Feature

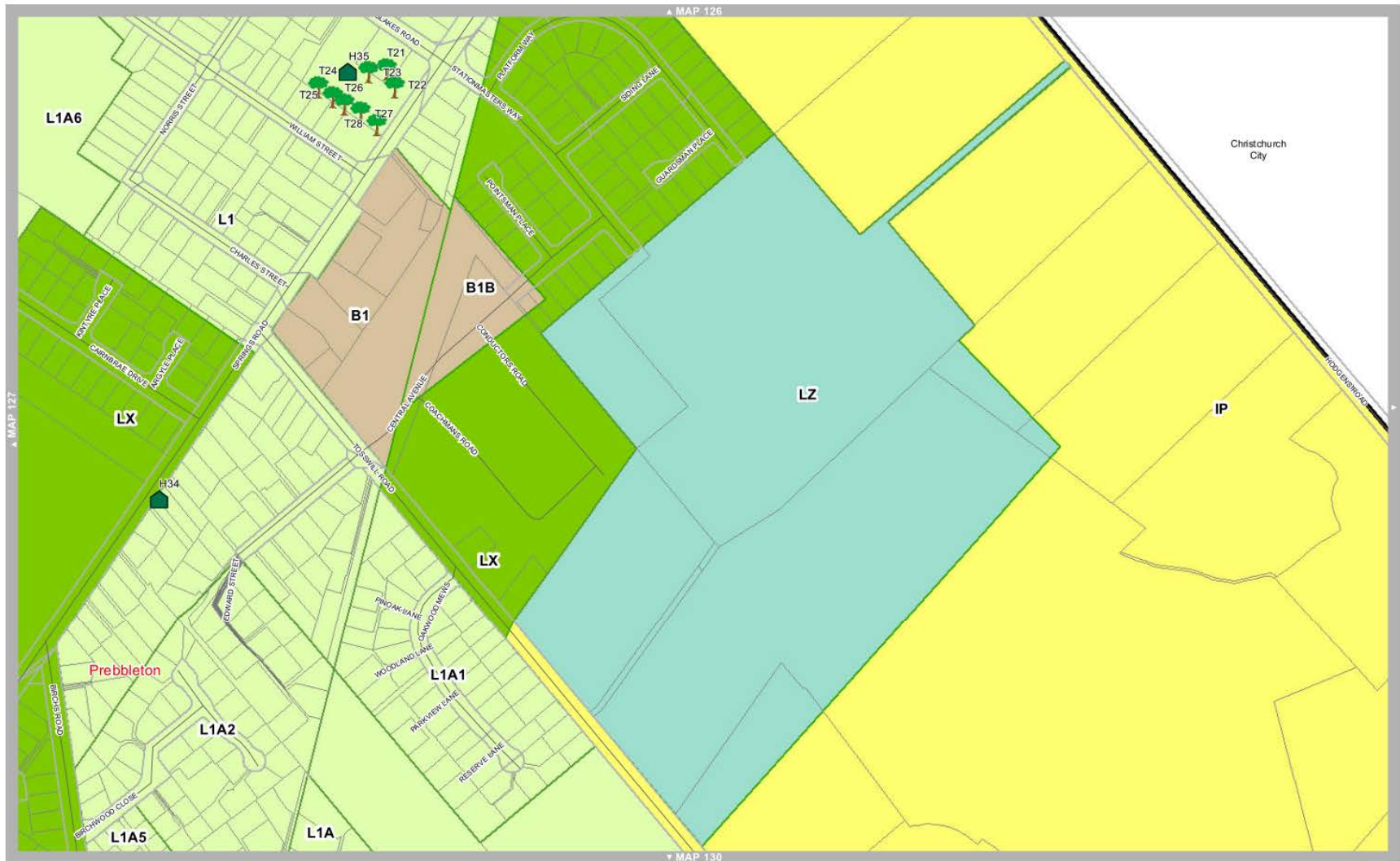
Flood Zone

- Waimakariri Flood Plain
- Lower Plains Flood Area
- Lake Ellesmere Flood Area

Territorial Authority

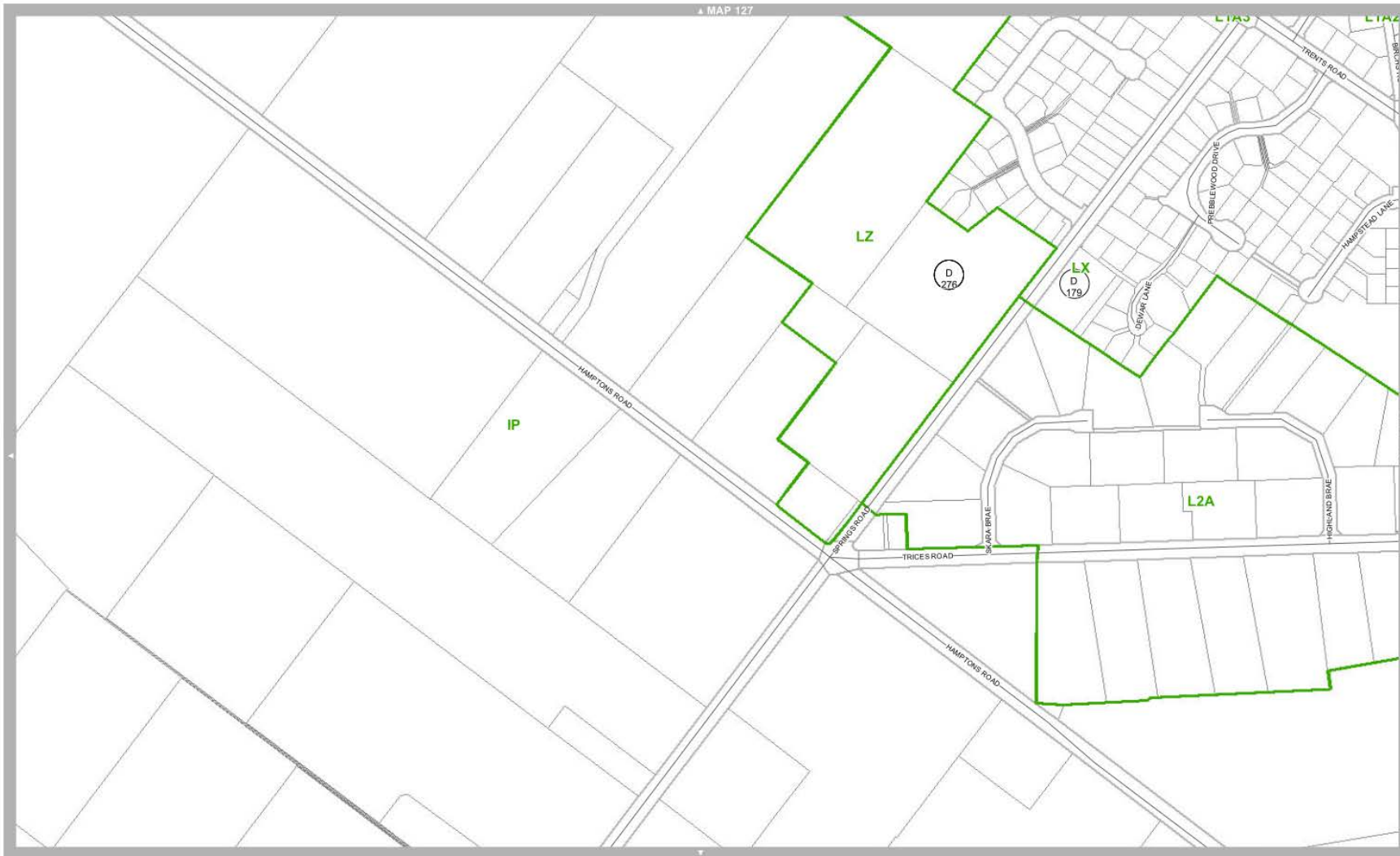
- Surrounding Districts
- Selwyn District Boundary





**KEY**

- |                             |                                 |                |                           |              |                   |                 |              |                     |
|-----------------------------|---------------------------------|----------------|---------------------------|--------------|-------------------|-----------------|--------------|---------------------|
| Heritage Building           | Protected Tree                  | Scheduled Site | State Highway             | Railway      | Road Parcel       | Land Parcel     | Water Body   | Archaeological Site |
| Wahi Taonga Management Site | Statutory Acknowledgement Site  |                |                           |              |                   |                 |              |                     |
| Silent File Area            | <b>Contours Above Sea Level</b> |                |                           |              |                   |                 |              |                     |
| Wahi Taonga Management Area | 160m                            | 60m            | 2.74m                     | <b>Zones</b> |                   |                 |              |                     |
|                             | High Country                    | Port Hills     | Existing Development Area | Living 1     | Living 2          | Living 3        | Living X     | Living WM           |
|                             | Business 1                      | Business 2     | Business 3                | Living Z     | Living Z Deferred | Deferred Living | Inner Plains | Outer Plains        |
|                             | Malvern Hills                   |                |                           |              |                   |                 |              |                     |



MAP  
**129**  
SHEET 1

**SELWYN DISTRICT PLAN**

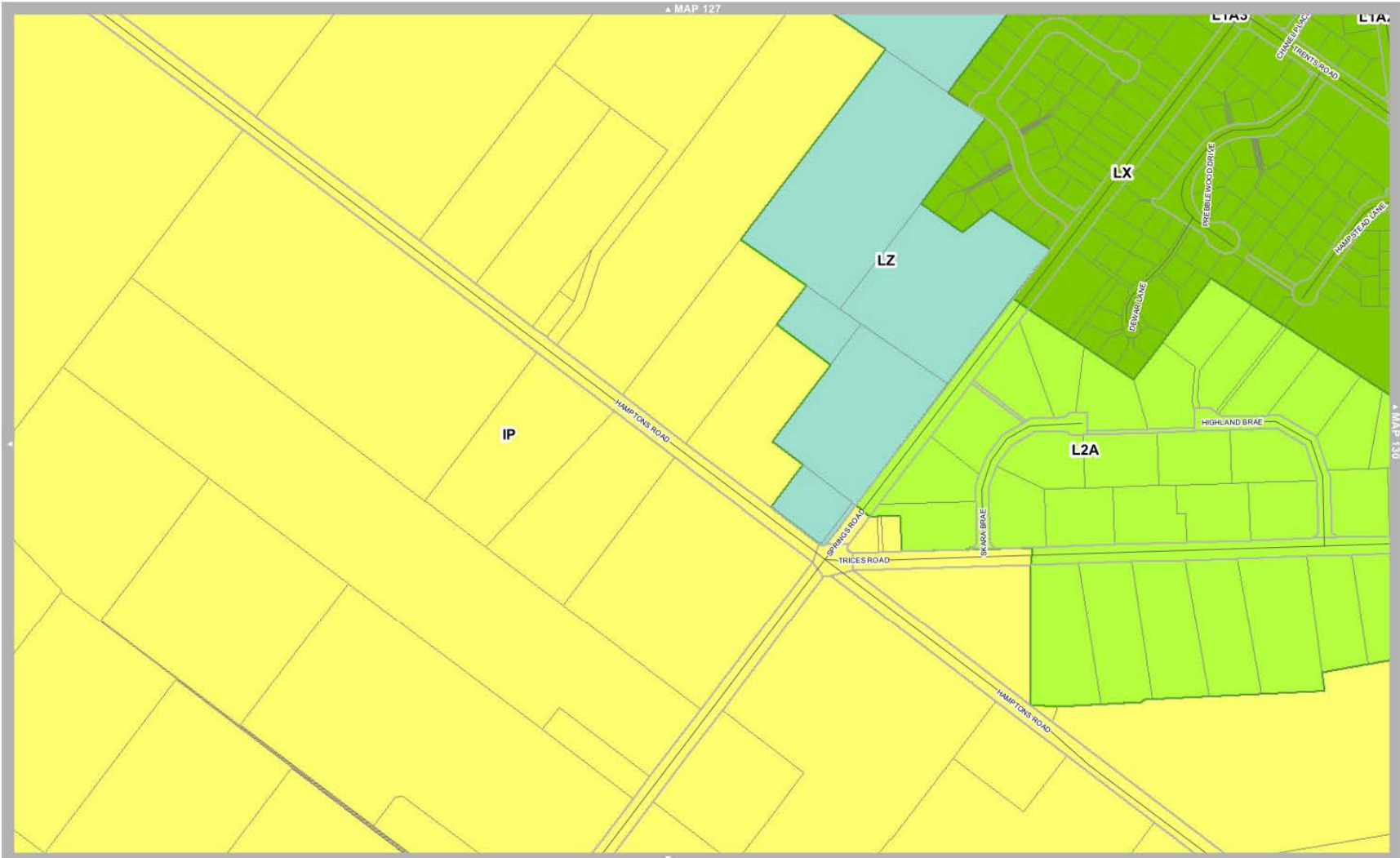
Scale: 1:4,000 at A3  
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**KEY**

- |  |                       |                              |                           |                          |
|--|-----------------------|------------------------------|---------------------------|--------------------------|
| SDC Designated Site                      | Fault Lines           | Forestry Exclusions          | <b>Flood Zone</b>         | Surrounding Districts    |
| Other Designations                       | Coastal Hazard        | Outstanding Landscape        | Waimakariri Flood Plain   | Selwyn District Boundary |
| ChCh International Airport Noise Contour | Transpower Main Lines | West Melton Observatory Zone | Lower Plains Flood Area   |                          |
| NZTA Widening Designation                | Zones                 | Outstanding Natural Feature  | Lake Ellesmere Flood Area |                          |



**KEY**

Heritage Building	Protected Tree	Scheduled Site	State Highway	Railway	Road Parcel	Land Parcel	Water Body	Archaeological Site	
Wāhi Taonga Management Site	Statutory Acknowledgement Site	<b>Contours Above Sea Level</b>		<b>Zones</b>		Living 1	Living WM	Business 1	Outer Plains
Silent File Area	160m	High Country	Port Hills	Living 2	Living Z	Living 3	Living Z Deferred	Business 2	Malvern Hills
Wāhi Taonga Management Area	60m	Existing Development Area	Living X	Living X	Deferred Living	Business 3	Inner Plains		
	2.74m								



Plan Change 21 still in planning stage - Not Yet Operative

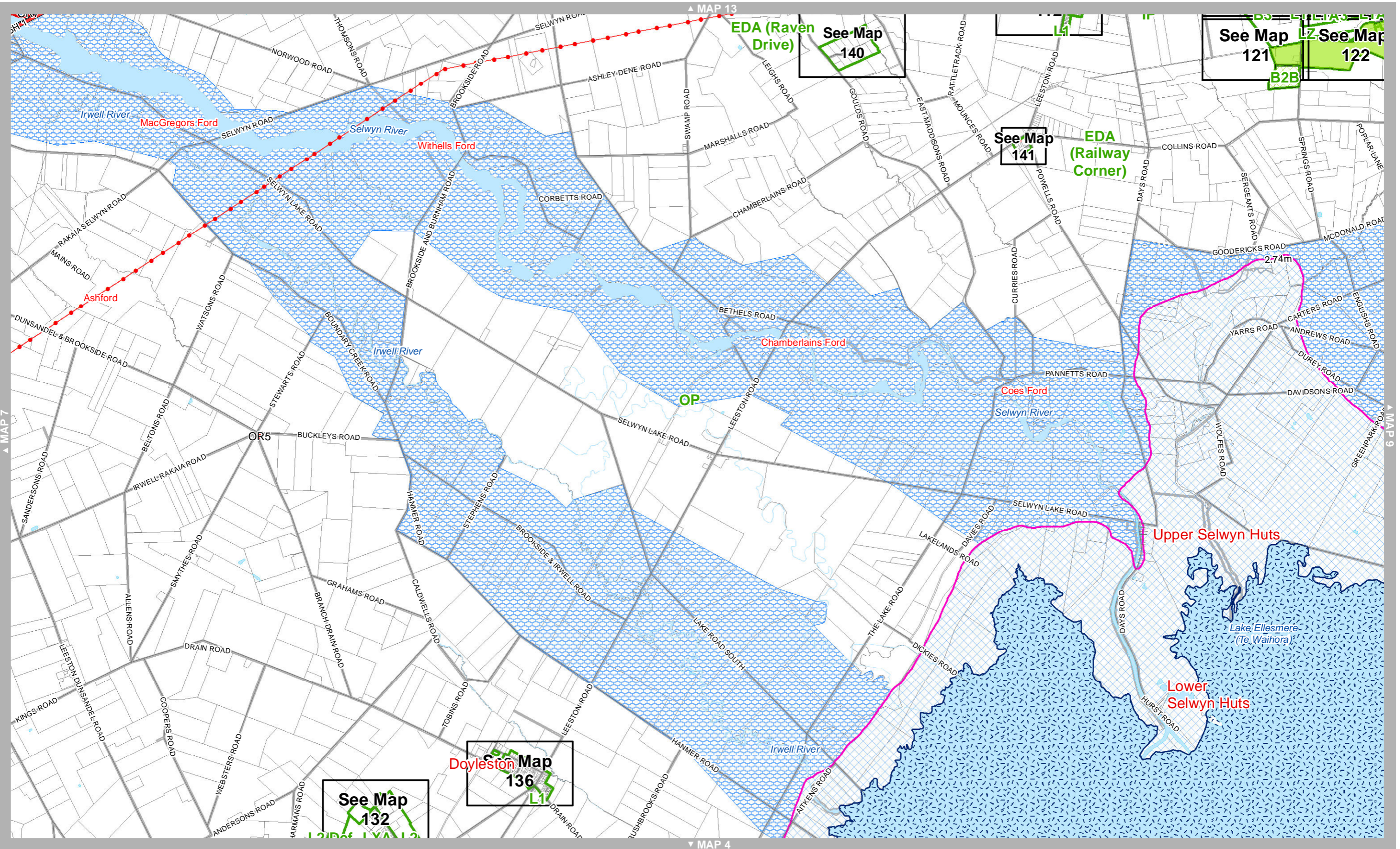
Printed on:  
Monday, 26 November 2012

MAP  
**129**  
SHEET 2

# **Attachment 5**

## **Priority Area Planning Maps**





MAP  
**008**  
SHEET 1

SELWYN DISTRICT PLAN

Scale: 1:50,000 at A3  
0 1,000 m

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KEY

- SDC Designated Site
- Other Designations
- ChCh International Airport Noise Contour
- NZTA Widening Designation
- Zones
- Forestry Exclusions
- Outstanding Landscape

- Fault Lines
- Coastal Hazard
- Transpower Main Lines
- Zones
- Forestry Exclusions
- Outstanding Landscape

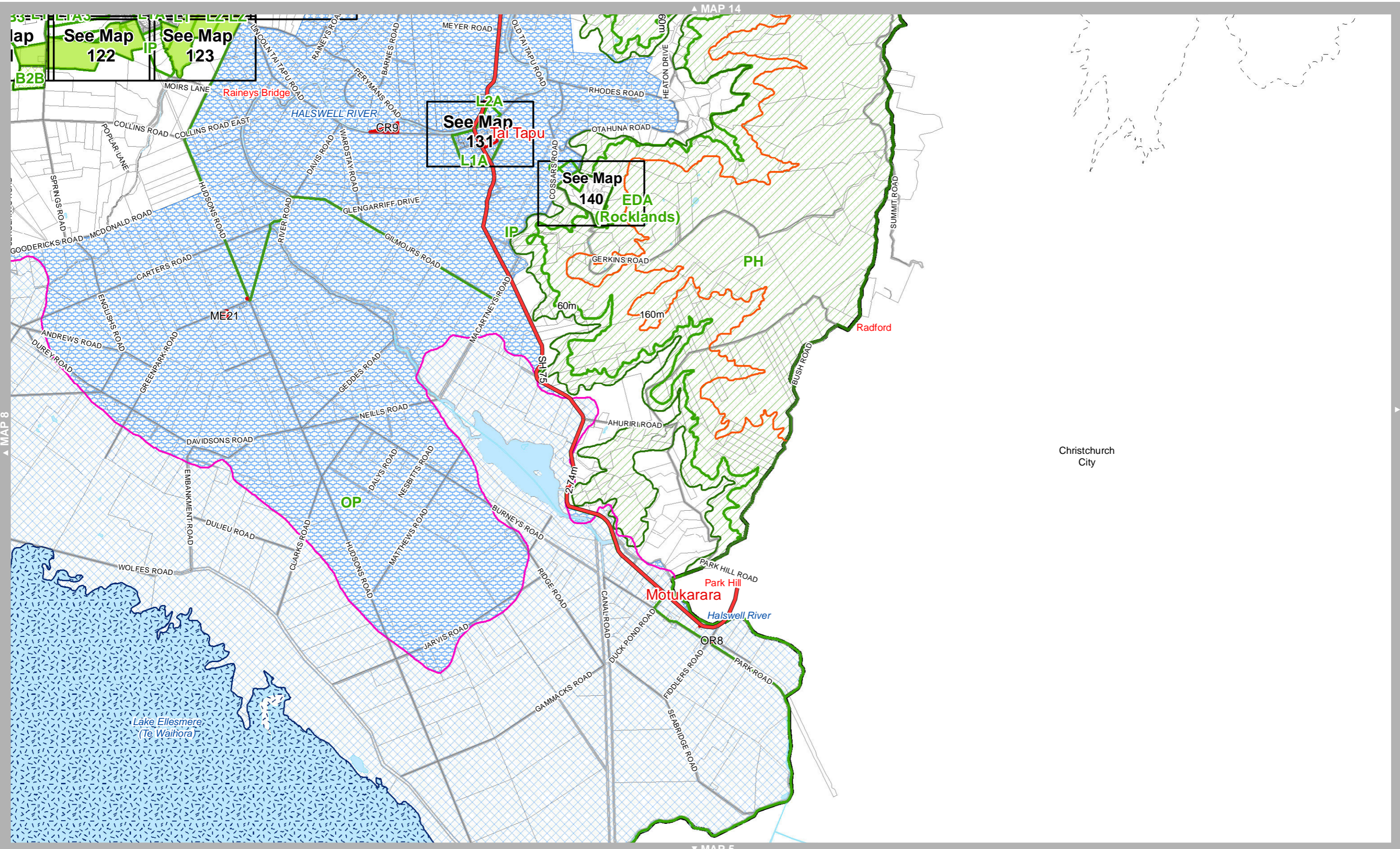
- West Melton Observatory Zone
- Outstanding Natural Feature
- Flood Zone**
- Waimakariri Flood Plain
- Lower Plains Flood Area
- Lake Ellesmere Flood Area

- Priority Areas - Business**
- Business Zoned
- Business Not Yet Zoned
- Priority Areas - Residential**
- Residential Zoned
- Residential Not Yet Zoned

- Greenfields Areas**
- Business Greenfields - post Land Use Recovery Plan
- Residential Greenfields - post Land Use Recovery Plan
- Territorial Authority**
- Surrounding Districts
- Selwyn District Boundary

LURP Priority and Greenfield Areas  
Plan Change  
**NOT YET OPERATIVE**  
Map Made On: 8/05/2013





MAP  
**009**  
SHEET 1

SELWYN DISTRICT PLAN

Scale: 1:50,000 at A3  
0 1,000 m

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KEY

- SDC Designated Site
- Other Designations
- ChCh International Airport Noise Contour
- NZTA Widening Designation

- Fault Lines
- Coastal Hazard
- Transpower Main Lines
- Zones
- Forestry Exclusions
- Outstanding Landscape

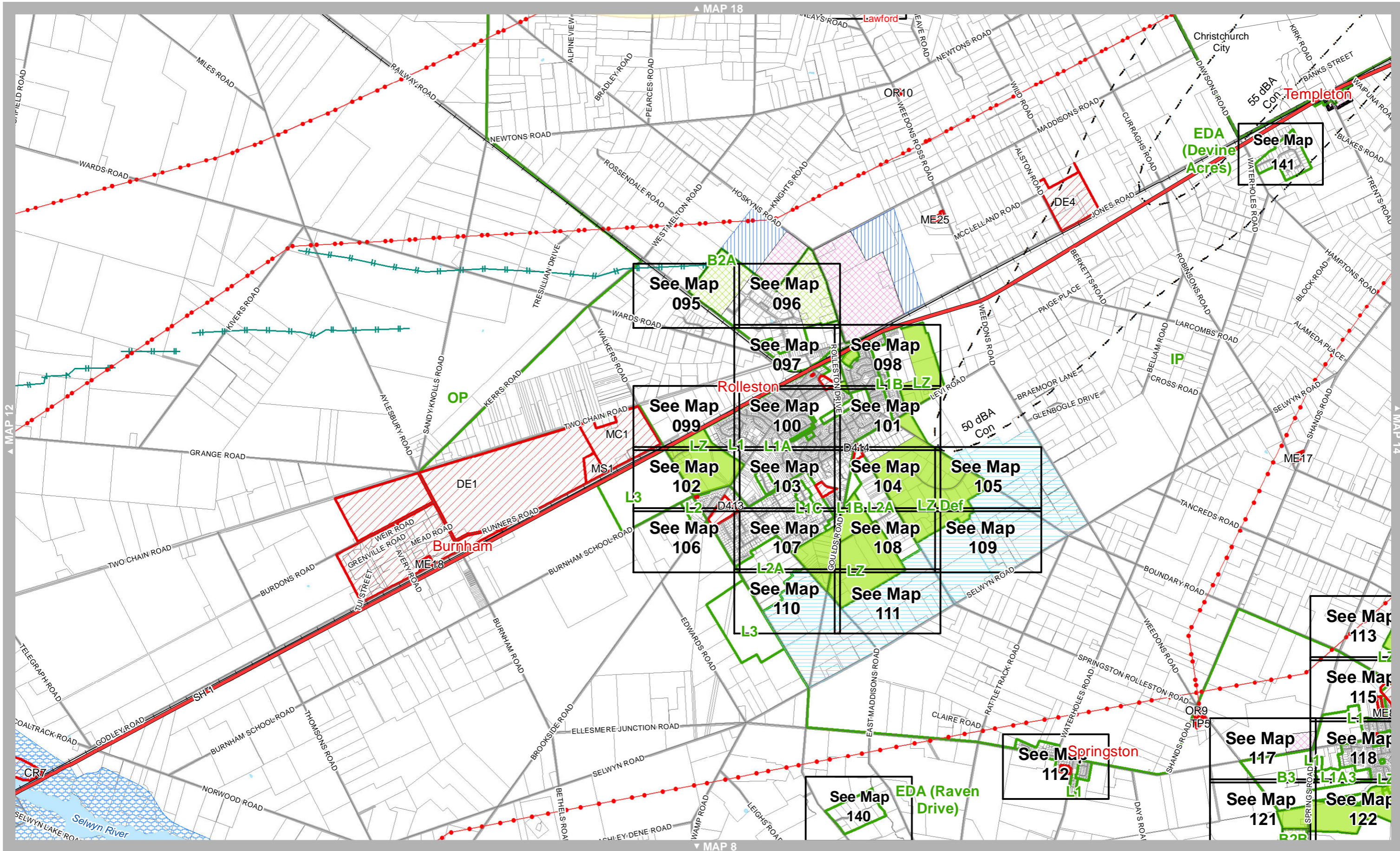
- West Melton Observatory Zone
- Outstanding Natural Feature
- Flood Zone**
- Waimakariri Flood Plain
- Lower Plains Flood Area
- Lake Ellesmere Flood Area

- Priority Areas - Business**
- Business Zoned
- Business Not Yet Zoned
- Priority Areas - Residential**
- Residential Zoned
- Residential Not Yet Zoned

- Greenfields Areas**
- Business Greenfields - post Land Use Recovery Plan
- Residential Greenfields - post Land Use Recovery Plan
- Territorial Authority**
- Surrounding Districts
- Selwyn District Boundary

LURP Priority and Greenfield Areas  
Plan Change  
**NOT YET OPERATIVE**  
Map Made On: 8/05/2013





MAP  
**013**  
SHEET 1

SELWYN DISTRICT PLAN

Scale: 1:50,000 at A3  
0 1,000 m

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KEY

- SDC Designated Site
- Other Designations
- ChCh International Airport Noise Contour
- NZTA Widening Designation

- Fault Lines
- Coastal Hazard
- Transpower Main Lines
- Zones
- Forestry Exclusions
- Outstanding Landscape

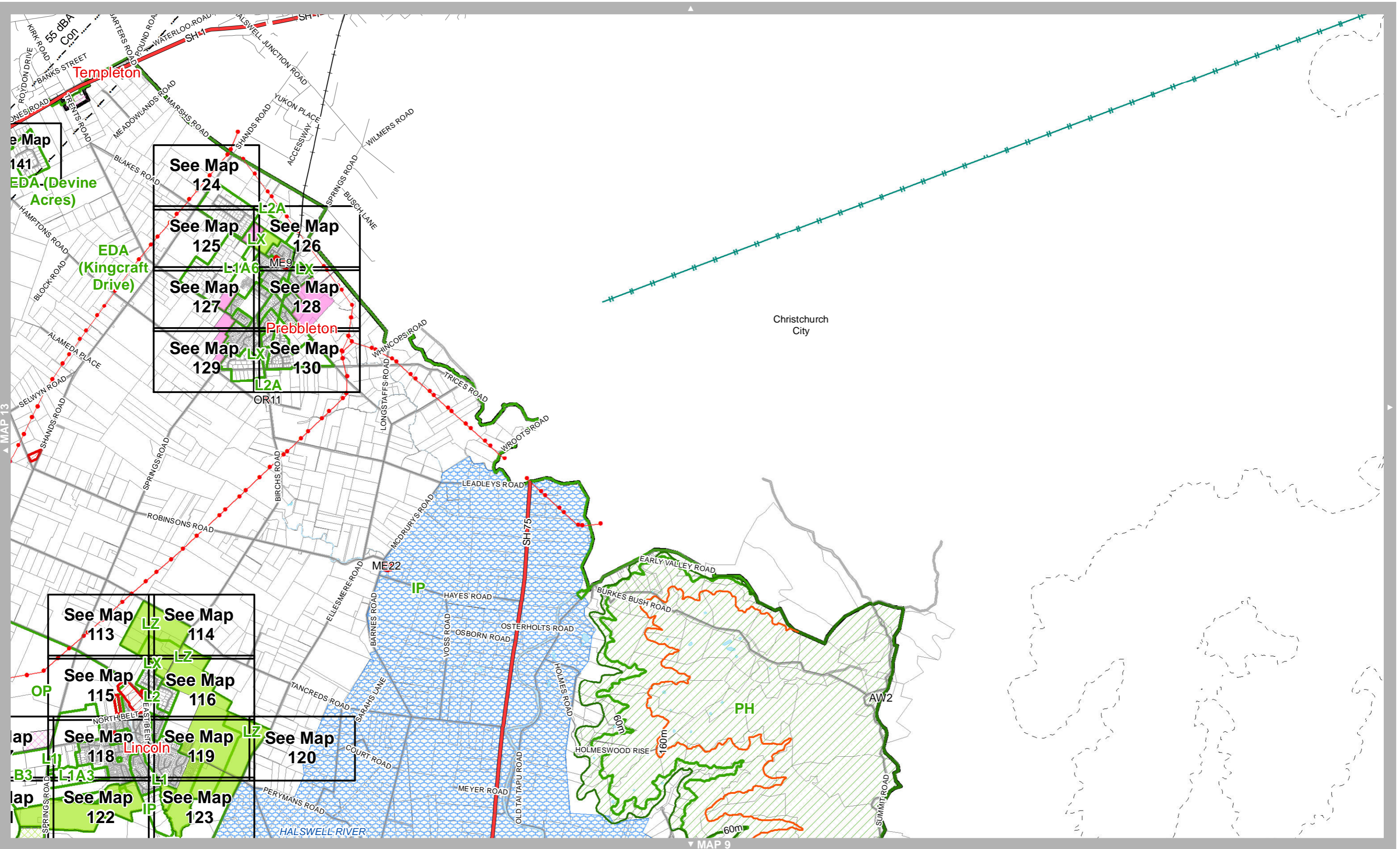
- West Melton Observatory Zone
- Outstanding Natural Feature
- Flood Zone**
- Waimakariri Flood Plain
- Lower Plains Flood Area
- Lake Ellesmere Flood Area

- Priority Areas - Business**
- Business Zoned
- Business Not Yet Zoned
- Priority Areas - Residential**
- Residential Zoned
- Residential Not Yet Zoned

- Greenfields Areas**
- Business Greenfields - post Land Use Recovery Plan
- Residential Greenfields - post Land Use Recovery Plan
- Territorial Authority**
- Surrounding Districts
- Selwyn District Boundary

LURP Priority and Greenfield Areas  
Plan Change  
**NOT YET OPERATIVE**  
Map Made On: 8/05/2013





MAP  
**014**  
SHEET 1

**SELWYN DISTRICT PLAN**  
Scale: 1:50,000 at A3  
0 1,000 m

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- KEY**
- SDC Designated Site
  - Other Designations
  - ChCh International Airport Noise Contour
  - NZTA Widening Designation

- Fault Lines
- Coastal Hazard
- Transpower Main Lines
- Zones
- Forestry Exclusions
- Outstanding Landscape

- West Melton Observatory Zone
- Outstanding Natural Feature
- Flood Zone**
- Waimakariri Flood Plain
- Lower Plains Flood Area
- Lake Ellesmere Flood Area

- Priority Areas - Business**
- Business Zoned
- Business Not Yet Zoned
- Priority Areas - Residential**
- Residential Zoned
- Residential Not Yet Zoned

- Greenfields Areas**
- Business Greenfields - post Land Use Recovery Plan
- Residential Greenfields - post Land Use Recovery Plan
- Territorial Authority**
- Surrounding Districts
- Selwyn District Boundary

LURP Priority and Greenfield Areas  
Plan Change  
**NOT YET OPERATIVE**  
Map Made On: 8/05/2013





MAP  
**095**  
SHEET 1

SELWYN DISTRICT PLAN

Scale: 1:4,000 at A3  
0 100 m

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- KEY**
- SDC Designated Site
  - Other Designations
  - ChCh International Airport Noise Contour
  - NZTA Widening Designation

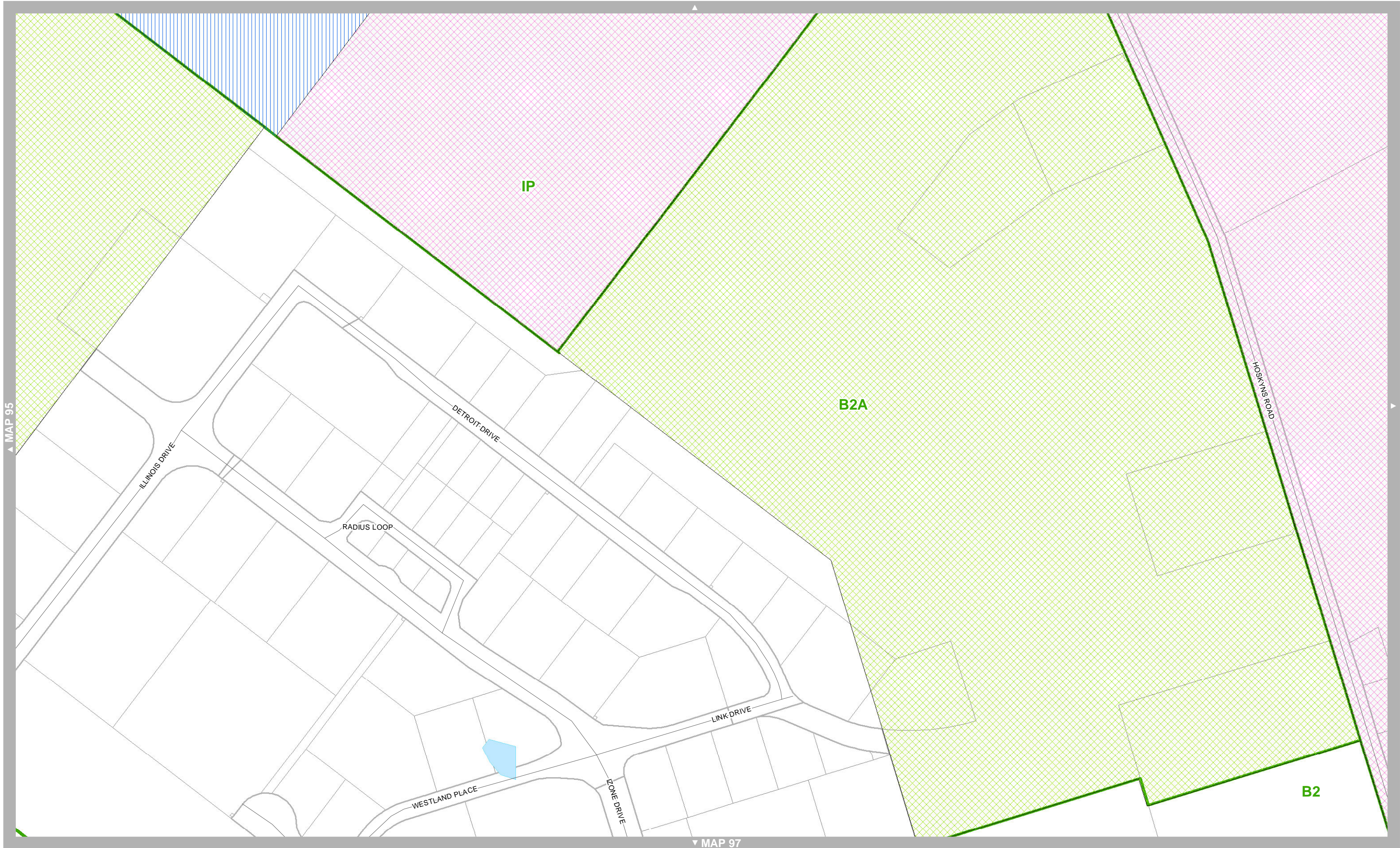
- Fault Lines
- Coastal Hazard
- Transpower Main Lines
- Zones
- Forestry Exclusions
- Outstanding Landscape
- West Melton Observatory Zone
- Outstanding Natural Feature
- Flood Zone**
- Waimakariri Flood Plain
- Lower Plains Flood Area
- Lake Ellesmere Flood Area

- Priority Areas - Business**
- Business Zoned
  - Business Not Yet Zoned
- Priority Areas - Residential**
- Residential Zoned
  - Residential Not Yet Zoned

- Greenfields Areas**
- Business Greenfields - post Land Use Recovery Plan
  - Residential Greenfields - post Land Use Recovery Plan
- Territorial Authority**
- Surrounding Districts
  - Selwyn District Boundary

LURP Priority and Greenfield Areas  
Plan Change  
**NOT YET OPERATIVE**  
Map Made On: 6/05/2013





MAP  
**096**  
SHEET 1

SELWYN DISTRICT PLAN

Scale: 1:4,000 at A3



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**KEY**

- SDC Designated Site
- Other Designations
- ChCh International Airport Noise Contour
- NZTA Widening Designation

- Fault Lines
- Coastal Hazard
- Transpower Main Lines
- Zones
- Forestry Exclusions
- Outstanding Landscape
- West Melton Observatory Zone
- Outstanding Natural Feature
- Flood Zone**
- Waimakariri Flood Plain
- Lower Plains Flood Area
- Lake Ellesmere Flood Area

**Priority Areas - Business**

- Business Zoned
- Business Not Yet Zoned

**Priority Areas - Residential**

- Residential Zoned
- Residential Not Yet Zoned

**Greenfields Areas**

- Business Greenfields - post Land Use Recovery Plan
- Residential Greenfields - post Land Use Recovery Plan

**Territorial Authority**

- Surrounding Districts
- Selwyn District Boundary

LURP Priority and Greenfield Areas  
Plan Change  
**NOT YET OPERATIVE**  
Map Made On: 6/05/2013



# MAP 097 SHEET 1

## SELWYN DISTRICT PLAN

Scale: 1:4,000 at A3



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### KEY

- SDC Designated Site
- Other Designations
- ChCh International Airport Noise Contour
- NZTA Widening Designation

- Fault Lines
- Coastal Hazard
- Transpower Main Lines
- Zones
- Forestry Exclusions
- Outstanding Landscape

- West Melton Observatory Zone
- Outstanding Natural Feature
- Flood Zone**
- Waimakariri Flood Plain
- Lower Plains Flood Area
- Lake Ellesmere Flood Area

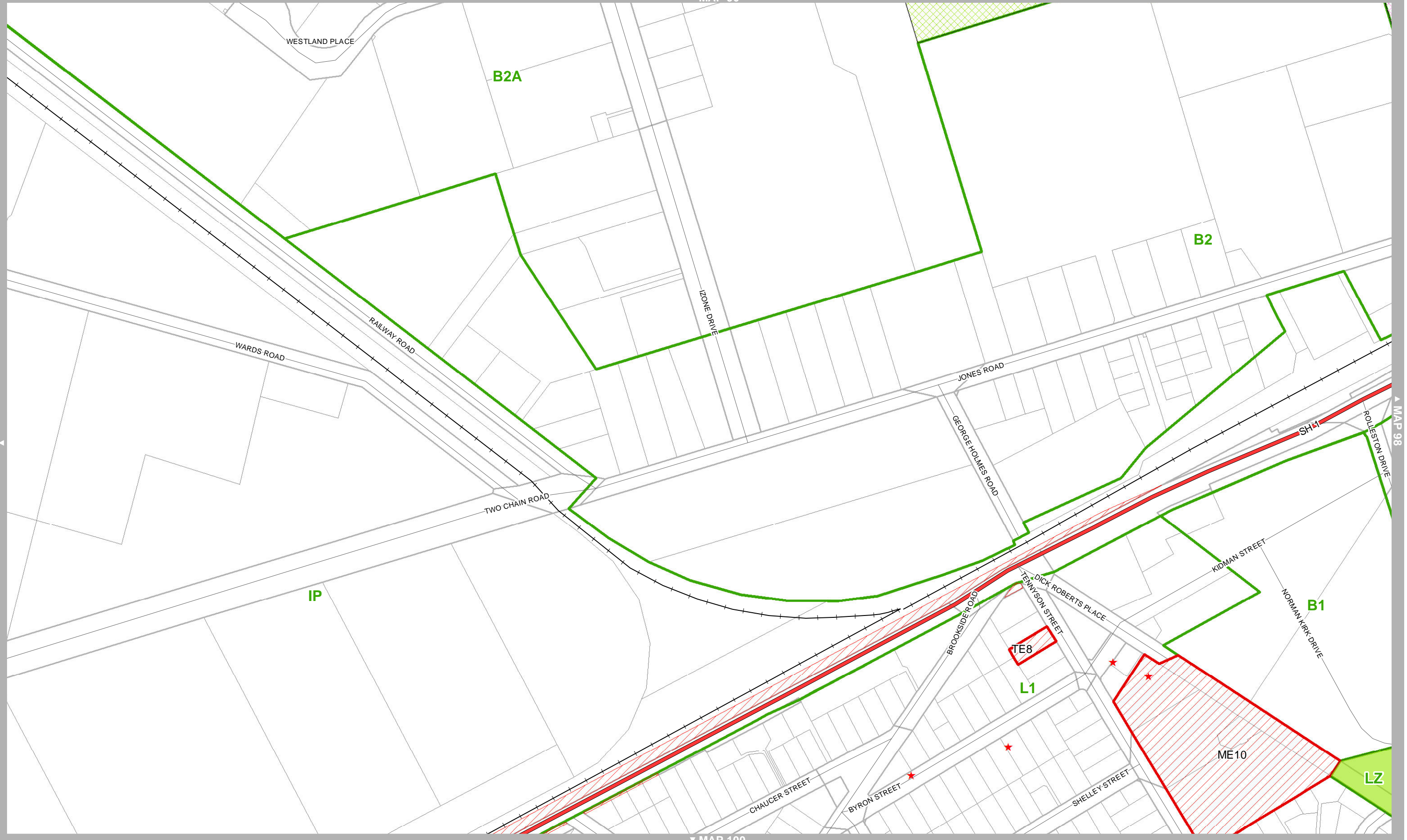
- Priority Areas - Business**
- Business Zoned
- Business Not Yet Zoned
- Priority Areas - Residential**
- Residential Zoned
- Residential Not Yet Zoned

- Greenfields Areas**
- Business Greenfields - post Land Use Recovery Plan
- Residential Greenfields - post Land Use Recovery Plan
- Territorial Authority**
- Surrounding Districts
- Selwyn District Boundary

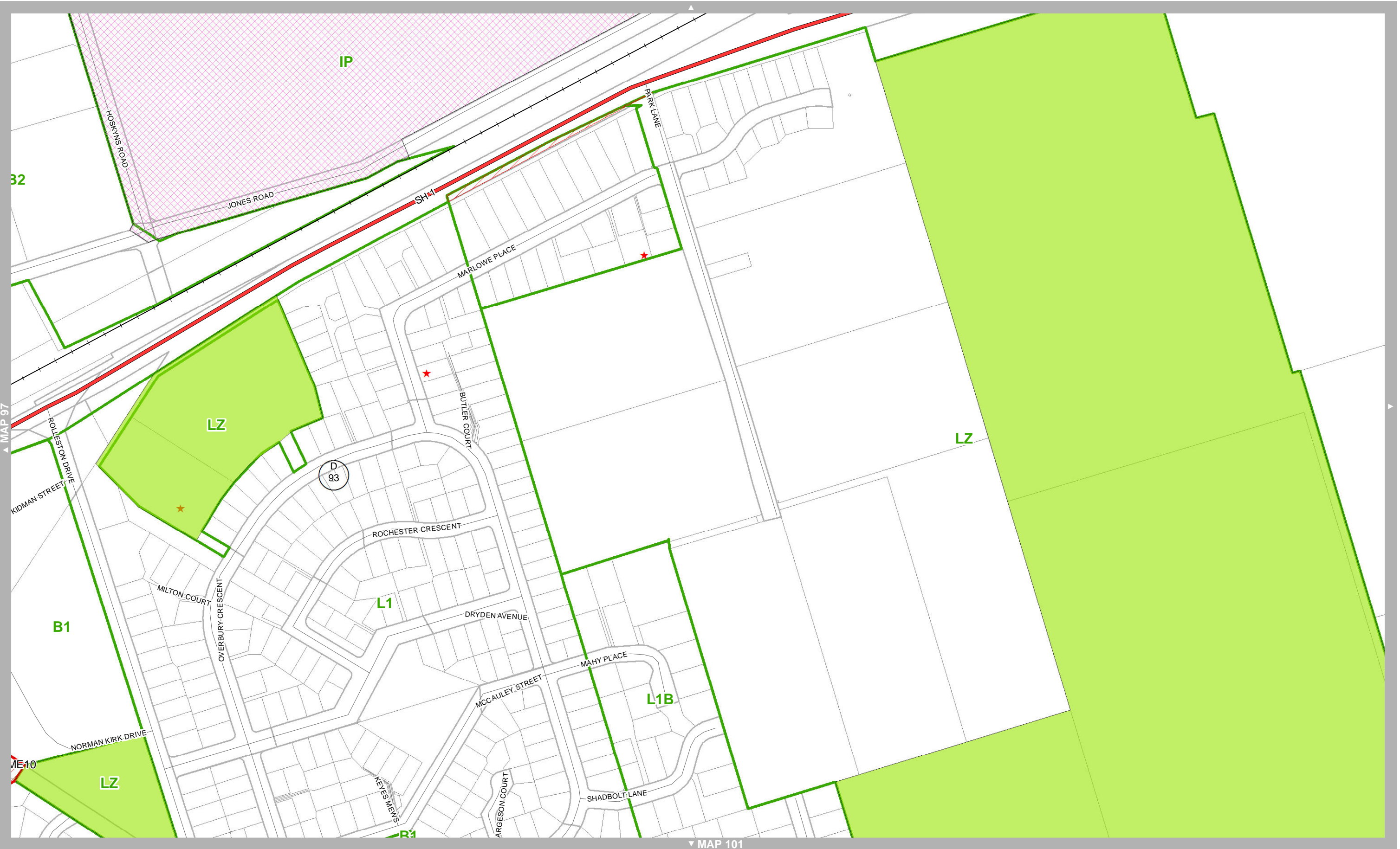
LURP Priority and Greenfield Areas Plan Change

**NOT YET OPERATIVE**

Map Made On: 6/05/2013







MAP  
**098**  
SHEET 1

**SELWYN DISTRICT PLAN**

Scale: 1:4,000 at A3  
0 100 m

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- KEY**
- SDC Designated Site
  - Other Designations
  - ChCh International Airport Noise Contour
  - NZTA Widening Designation

- Fault Lines
- Coastal Hazard
- Transpower Main Lines
- Zones
- Forestry Exclusions
- Outstanding Landscape
- West Melton Observatory Zone
- Outstanding Natural Feature
- Waimakariri Flood Plain
- Lower Plains Flood Area
- Lake Ellesmere Flood Area

- Priority Areas - Business**
- Business Zoned
  - Business Not Yet Zoned
- Priority Areas - Residential**
- Residential Zoned
  - Residential Not Yet Zoned

- Greenfields Areas**
- Business Greenfields - post Land Use Recovery Plan
  - Residential Greenfields - post Land Use Recovery Plan
- Territorial Authority**
- Surrounding Districts
  - Selwyn District Boundary

LURP Priority and Greenfield Areas  
Plan Change  
**NOT YET OPERATIVE**  
Map Made On: 6/05/2013





MAP  
**099**  
SHEET 1

SELWYN DISTRICT PLAN

Scale: 1:4,000 at A3  
0 100 m

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- KEY**
- SDC Designated Site
  - Other Designations
  - ChCh International Airport Noise Contour
  - NZTA Widening Designation

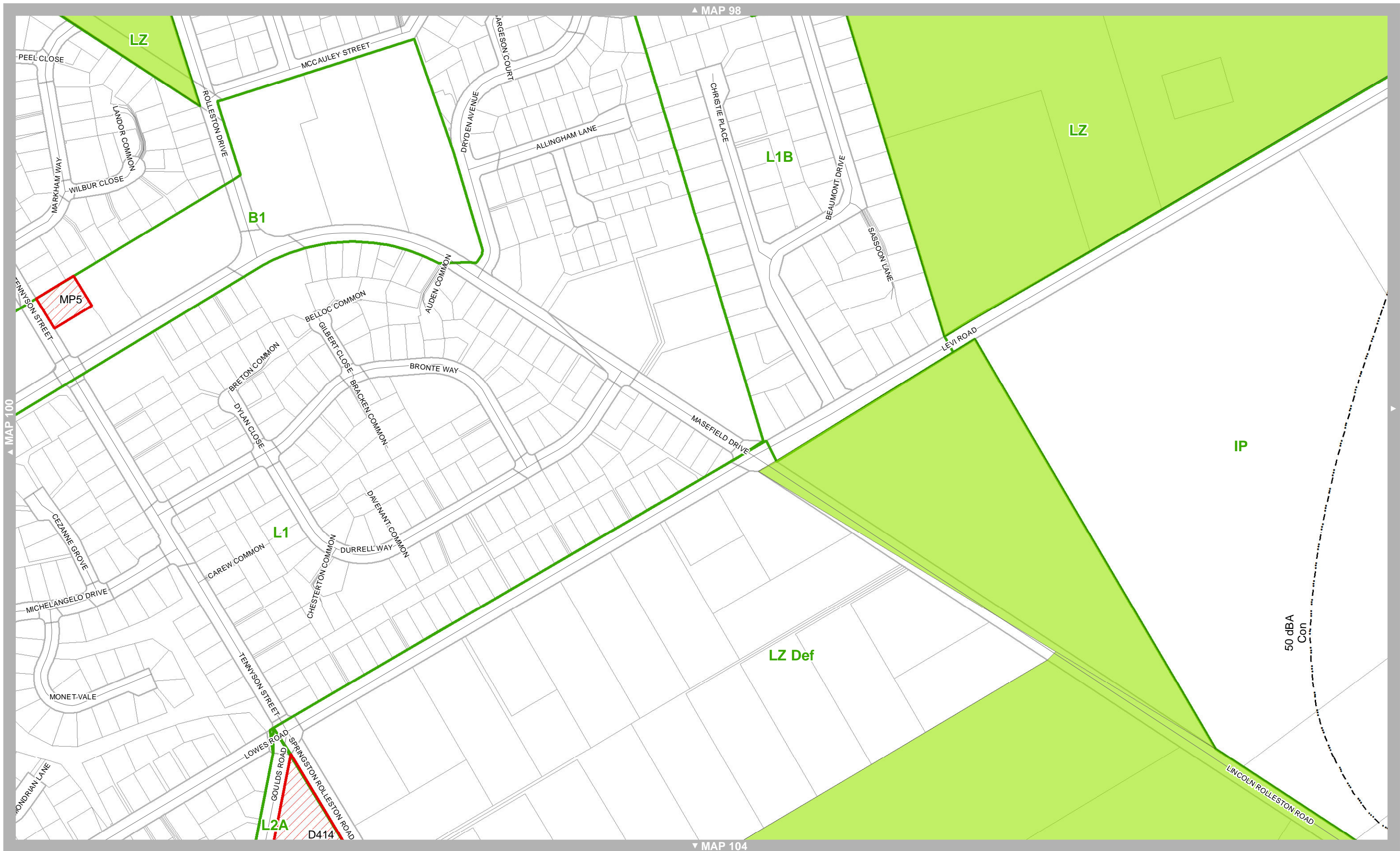
- Fault Lines
- Coastal Hazard
- Transpower Main Lines
- Zones
- Forestry Exclusions
- Outstanding Landscape
- West Melton Observatory Zone
- Outstanding Natural Feature
- Flood Zone**
- Waimakariri Flood Plain
- Lower Plains Flood Area
- Lake Ellesmere Flood Area

- Priority Areas - Business**
- Business Zoned
  - Business Not Yet Zoned
- Priority Areas - Residential**
- Residential Zoned
  - Residential Not Yet Zoned

- Greenfields Areas**
- Business Greenfields - post Land Use Recovery Plan
  - Residential Greenfields - post Land Use Recovery Plan
- Territorial Authority**
- Surrounding Districts
  - Selwyn District Boundary

LURP Priority and Greenfield Areas  
Plan Change  
**NOT YET OPERATIVE**  
Map Made On: 6/05/2013





MAP  
**101**  
SHEET 1

SELWYN DISTRICT PLAN

Scale: 1:4,000 at A3



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KEY

- SDC Designated Site
- Other Designations
- ChCh International Airport Noise Contour
- NZTA Widening Designation

- Fault Lines
- Coastal Hazard
- Transpower Main Lines
- Zones
- Forestry Exclusions
- Outstanding Landscape
- West Melton Observatory Zone
- Outstanding Natural Feature
- Flood Zone
- Waimakariri Flood Plain
- Lower Plains Flood Area
- Lake Ellesmere Flood Area

Priority Areas - Business

- Business Zoned
- Business Not Yet Zoned

Priority Areas - Residential

- Residential Zoned
- Residential Not Yet Zoned

Greenfields Areas

- Business Greenfields - post Land Use Recovery Plan
- Residential Greenfields - post Land Use Recovery Plan

Territorial Authority

- Surrounding Districts
- Selwyn District Boundary

LURP Priority and Greenfield Areas  
Plan Change  
**NOT YET OPERATIVE**  
Map Made On: 6/05/2013



# MAP 102 SHEET 1

## SELWYN DISTRICT PLAN

Scale: 1:4,000 at A3



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### KEY

- SDC Designated Site
- Other Designations
- ChCh International Airport Noise Contour
- NZTA Widening Designation

- Fault Lines
- Coastal Hazard
- Transpower Main Lines
- Zones
- Forestry Exclusions
- Outstanding Landscape

- West Melton Observatory Zone
- Outstanding Natural Feature
- Flood Zone**
- Waimakariri Flood Plain
- Lower Plains Flood Area
- Lake Ellesmere Flood Area

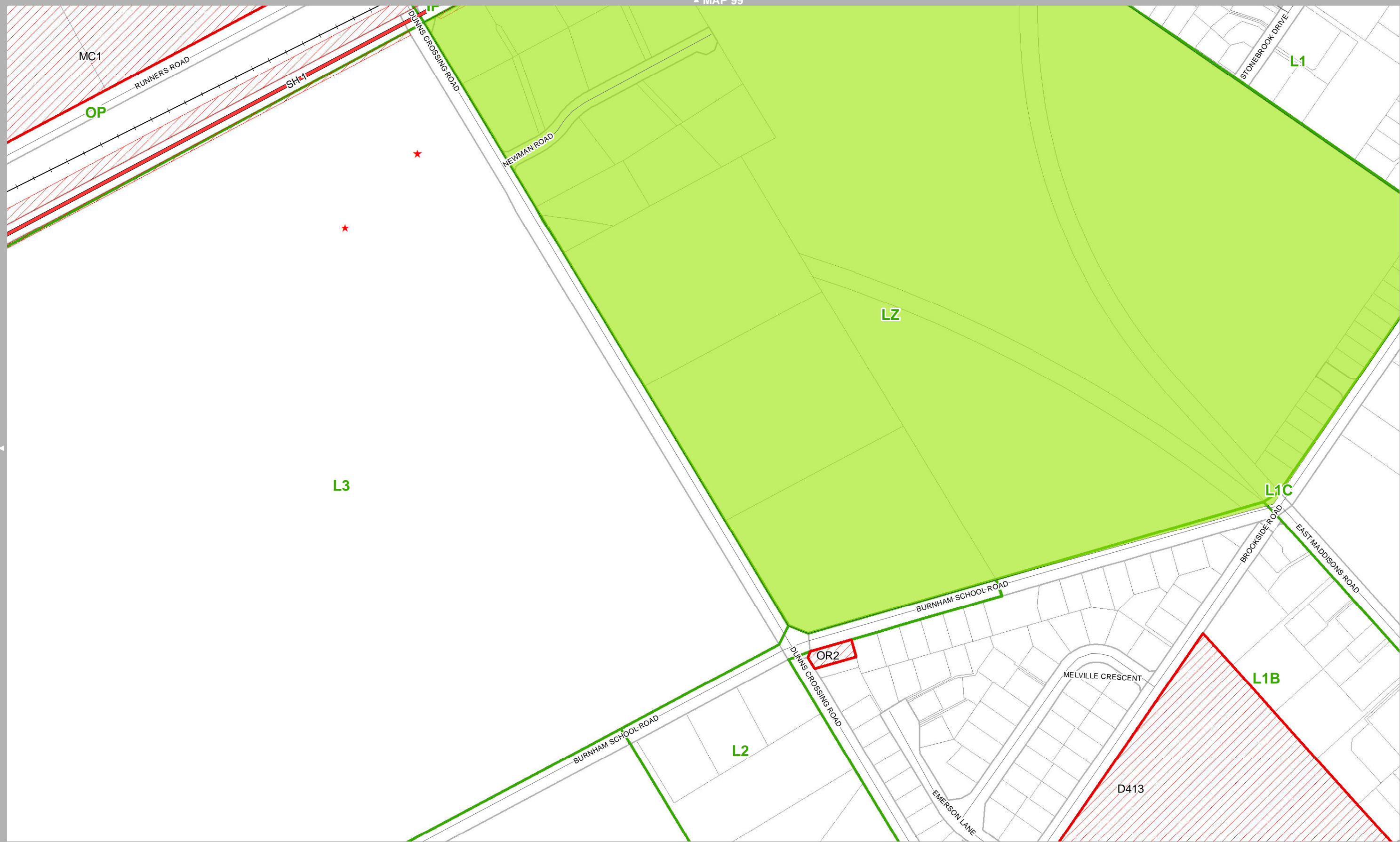
- Priority Areas - Business**
- Business Zoned
- Business Not Yet Zoned
- Priority Areas - Residential**
- Residential Zoned
- Residential Not Yet Zoned

- Greenfields Areas**
- Business Greenfields - post Land Use Recovery Plan
- Residential Greenfields - post Land Use Recovery Plan
- Territorial Authority**
- Surrounding Districts
- Selwyn District Boundary

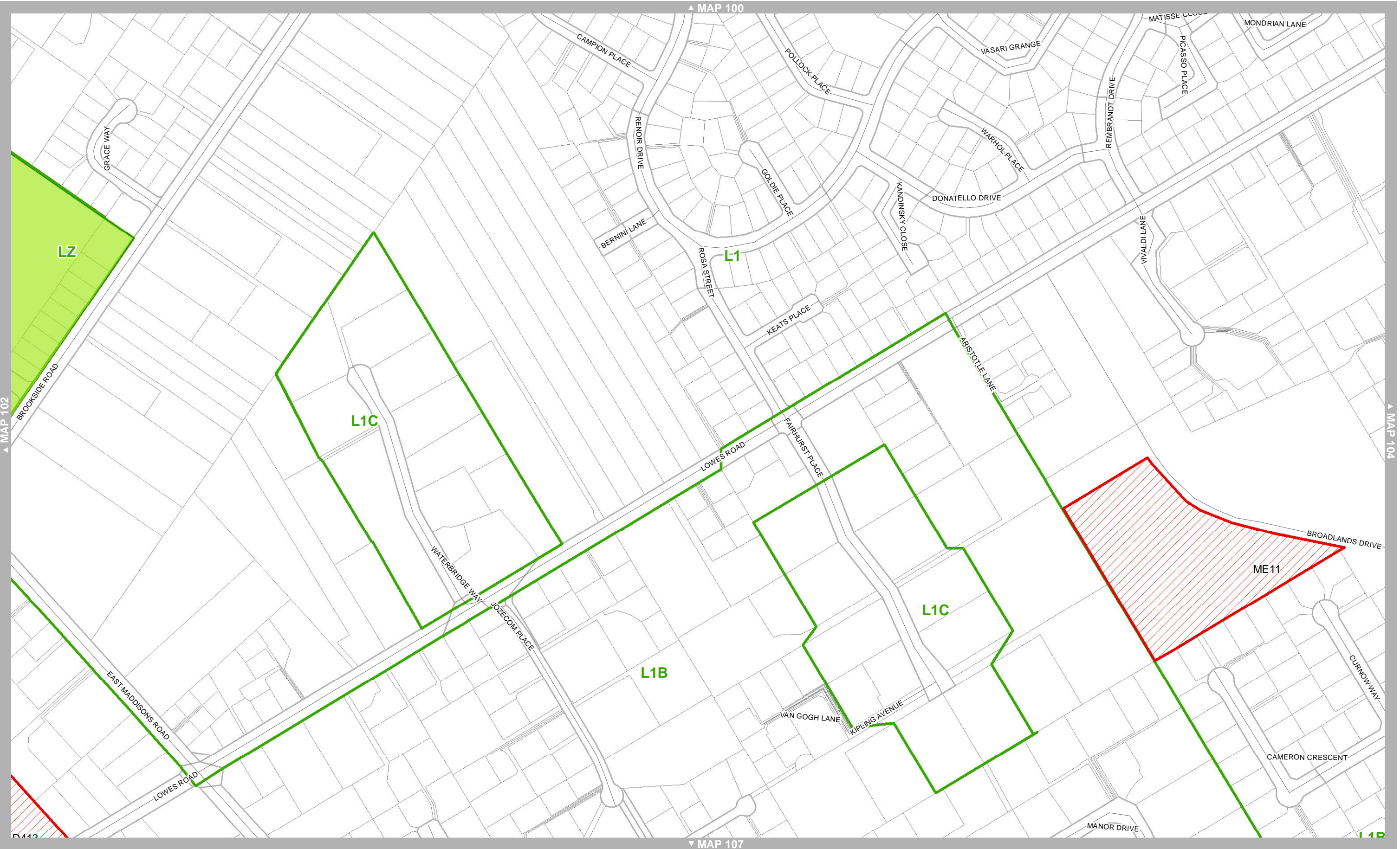
LURP Priority and Greenfield Areas Plan Change

**NOT YET OPERATIVE**

Map Made On: 6/05/2013







MAP  
**103**  
SHEET 1

SELWYN DISTRICT PLAN

Scale: 1:4,000 at A3  
0 100 m

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- KEY**
- SDC Designated Site
  - Other Designations
  - ChCh International Airport Noise Contour
  - NZTA Widening Designation

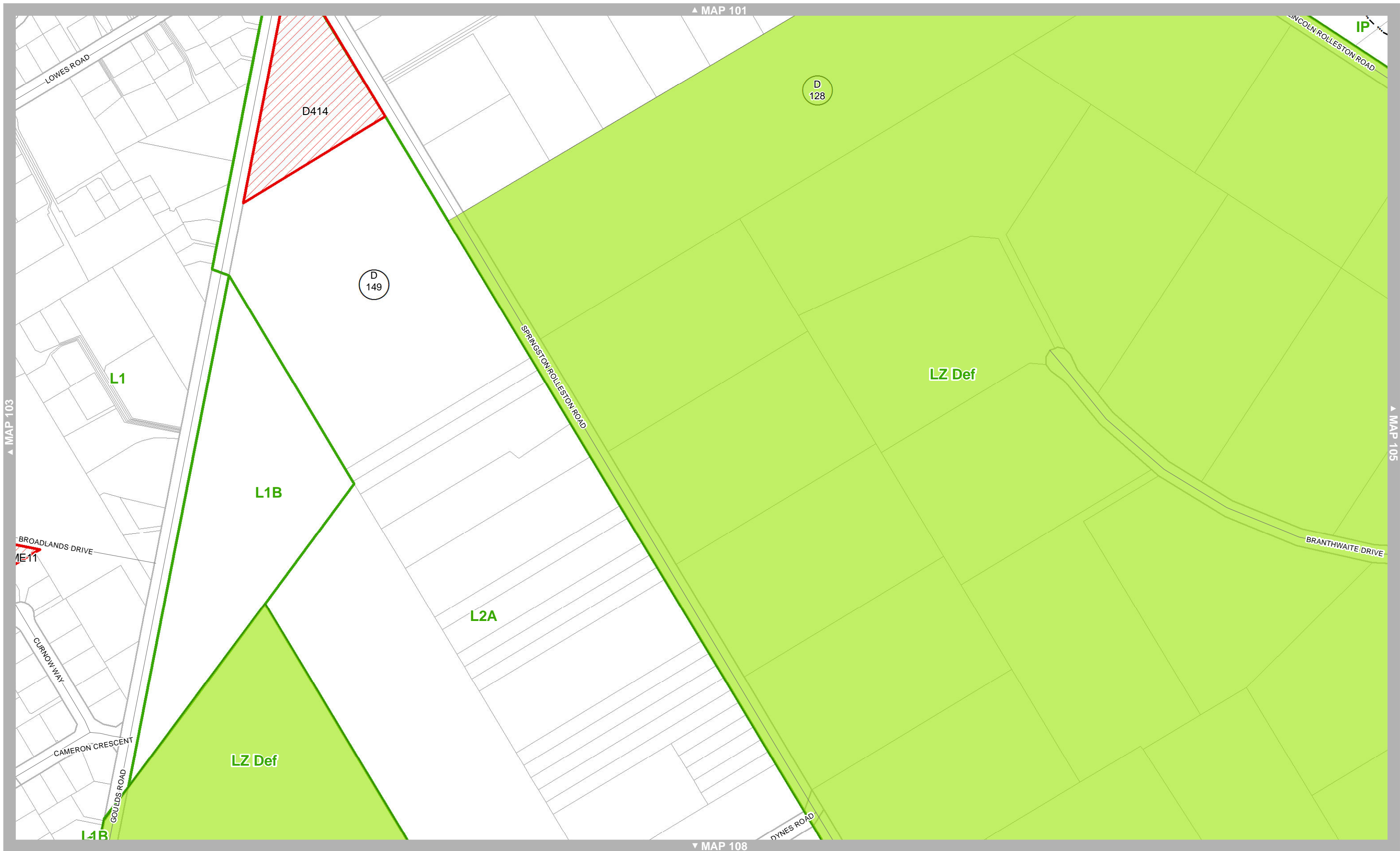
- Fault Lines
- Coastal Hazard
- Transpower Main Lines
- Zones
- Forestry Exclusions
- Outstanding Landscape
- West Melton Observatory Zone
- Outstanding Natural Feature
- Flood Zone
- Waimakariri Flood Plain
- Lower Plains Flood Area
- Lake Ellesmere Flood Area

- Priority Areas - Business**
- Business Zoned
  - Business Not Yet Zoned
- Priority Areas - Residential**
- Residential Zoned
  - Residential Not Yet Zoned

- Greenfields Areas**
- Business Greenfields - post Land Use Recovery Plan
  - Residential Greenfields - post Land Use Recovery Plan
- Territorial Authority**
- Surrounding Districts
  - Selwyn District Boundary

LURP Priority and Greenfield Areas  
Plan Change  
**NOT YET OPERATIVE**  
Map Made On: 6/05/2013





MAP  
**104**  
SHEET 1

SELWYN DISTRICT PLAN

Scale: 1:4,000 at A3



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KEY

- SDC Designated Site
- Other Designations
- ChCh International Airport Noise Contour
- NZTA Widening Designation

- Fault Lines
- Coastal Hazard
- Transpower Main Lines
- Zones
- Forestry Exclusions
- Outstanding Landscape
- West Melton Observatory Zone
- Outstanding Natural Feature
- Flood Zone
- Waimakariri Flood Plain
- Lower Plains Flood Area
- Lake Ellesmere Flood Area

Priority Areas - Business

- Business Zoned
- Business Not Yet Zoned

Priority Areas - Residential

- Residential Zoned
- Residential Not Yet Zoned

Greenfields Areas

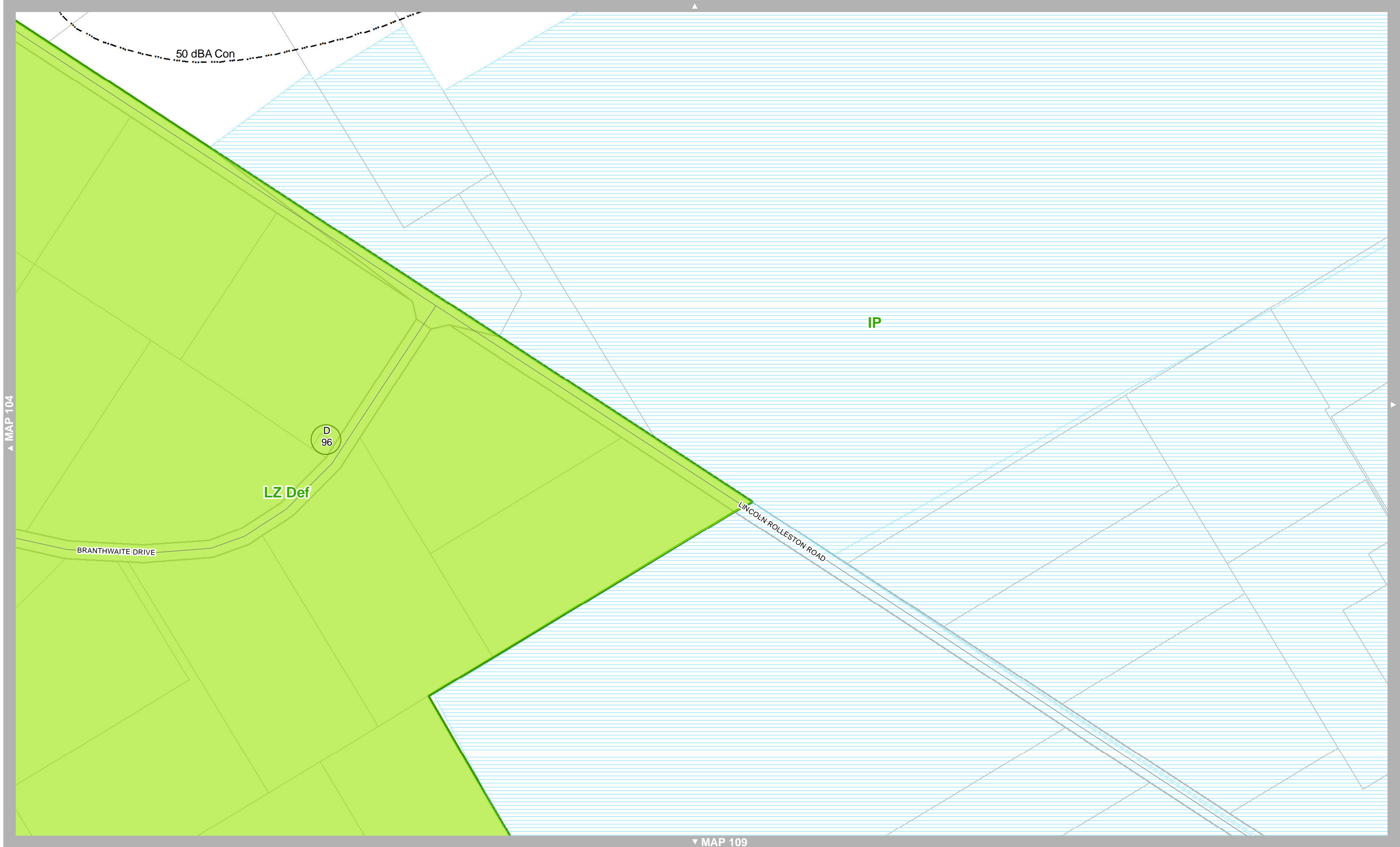
- Business Greenfields - post Land Use Recovery Plan
- Residential Greenfields - post Land Use Recovery Plan

Territorial Authority

- Surrounding Districts
- Selwyn District Boundary

LURP Priority and Greenfield Areas  
Plan Change  
**NOT YET OPERATIVE**  
Map Made On: 6/05/2013

MAP 104



MAP 109

MAP  
**105**  
SHEET 1

SELWYN DISTRICT PLAN

Scale: 1:4,000 at A3



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**KEY**

- SDC Designated Site
- Other Designations
- ChCh International Airport Noise Contour
- NZTA Widening Designation

- Fault Lines
- Coastal Hazard
- Transpower Main Lines
- Zones
- Forestry Exclusions
- Outstanding Landscape

- West Melton Observatory Zone
- Outstanding Natural Feature
- Flood Zone**
- Waimakariri Flood Plain
- Lower Plains Flood Area
- Lake Ellesmere Flood Area

**Priority Areas - Business**

- Business Zoned
- Business Not Yet Zoned

**Priority Areas - Residential**

- Residential Zoned
- Residential Not Yet Zoned

**Greenfields Areas**

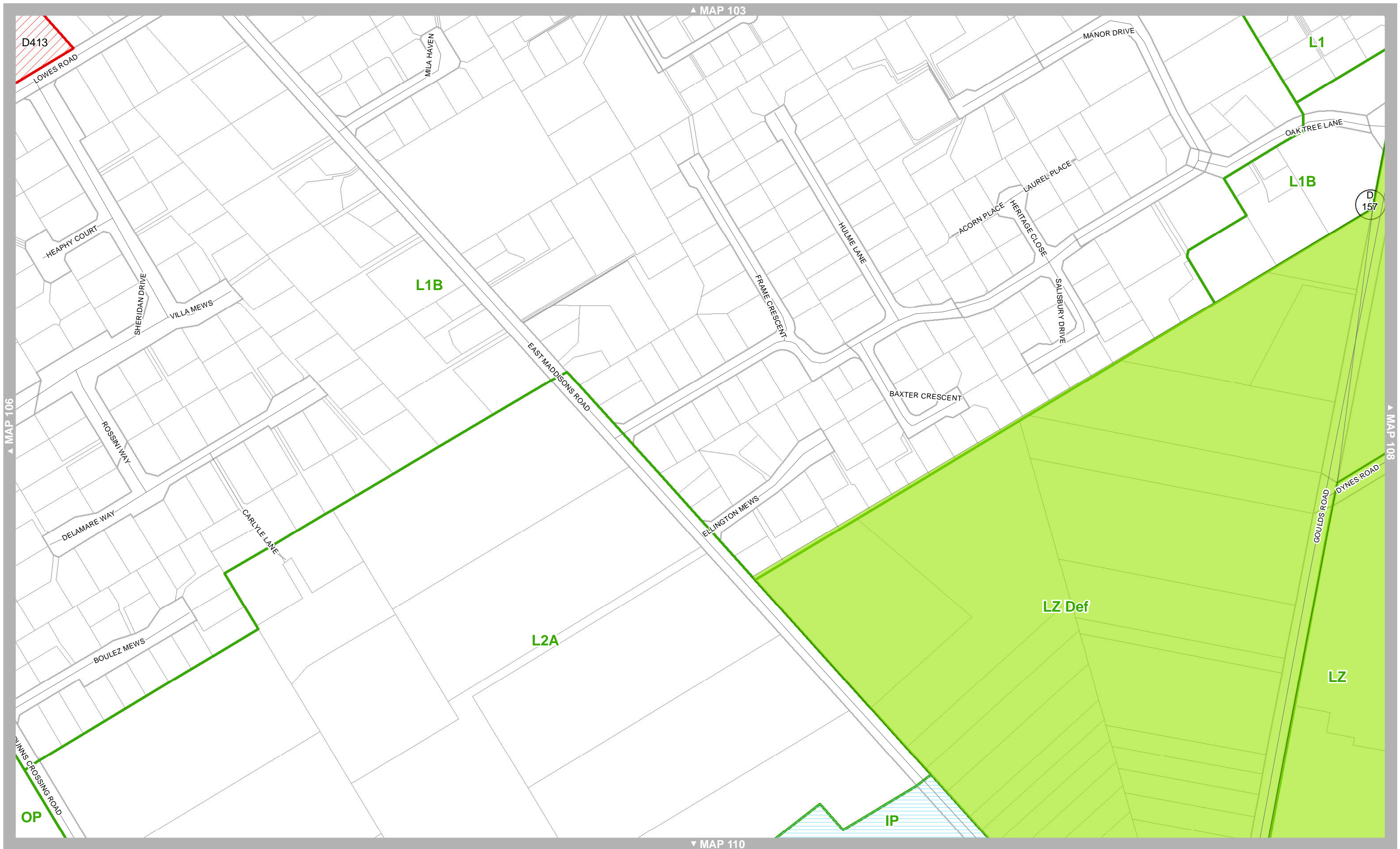
- Business Greenfields - post Land Use Recovery Plan
- Residential Greenfields - post Land Use Recovery Plan

**Territorial Authority**

- Surrounding Districts
- Selwyn District Boundary

LURP Priority and Greenfield Areas  
Plan Change  
**NOT YET OPERATIVE**  
Map Made On: 6/05/2013





MAP  
**107**  
SHEET 1

SELWYN DISTRICT PLAN

Scale: 1:4,000 at A3  
0 100 m

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- KEY**
- SDC Designated Site
  - Other Designations
  - ChCh International Airport Noise Contour
  - NZTA Widening Designation

- Fault Lines
- Coastal Hazard
- Transpower Main Lines
- Zones
- Forestry Exclusions
- Outstanding Landscape
- West Melton Observatory Zone
- Outstanding Natural Feature
- Flood Zone
- Waimakariri Flood Plain
- Lower Plains Flood Area
- Lake Ellesmere Flood Area

- Priority Areas - Business**
- Business Zoned
  - Business Not Yet Zoned
- Priority Areas - Residential**
- Residential Zoned
  - Residential Not Yet Zoned

- Greenfields Areas**
- Business Greenfields - post Land Use Recovery Plan
  - Residential Greenfields - post Land Use Recovery Plan
- Territorial Authority**
- Surrounding Districts
  - Selwyn District Boundary

LURP Priority and Greenfield Areas  
Plan Change  
**NOT YET OPERATIVE**  
Map Made On: 6/05/2013





MAP  
**108**  
SHEET 1

**SELWYN DISTRICT PLAN**

Scale: 1:4,000 at A3

0 100 m

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**KEY**

- SDC Designated Site
- Other Designations
- ChCh International Airport Noise Contour
- NZTA Widening Designation
- Zones
- Forestry Exclusions
- Outstanding Landscape
- Fault Lines
- Coastal Hazard
- Transpower Main Lines
- West Melton Observatory Zone
- Outstanding Natural Feature
- Waimakariri Flood Plain
- Lower Plains Flood Area
- Lake Ellesmere Flood Area

**Flood Zone**

- Waimakariri Flood Plain
- Lower Plains Flood Area
- Lake Ellesmere Flood Area

**Priority Areas - Business**

- Business Zoned
- Business Not Yet Zoned

**Priority Areas - Residential**

- Residential Zoned
- Residential Not Yet Zoned

**Greenfields Areas**

- Business Greenfields - post Land Use Recovery Plan
- Residential Greenfields - post Land Use Recovery Plan

**Territorial Authority**

- Surrounding Districts
- Selwyn District Boundary

LURP Priority and Greenfield Areas Plan Change

**NOT YET OPERATIVE**

Map Made On: 6/05/2013

LINCOLN ROLLESTON ROAD

LZ Def

IP

SELWYN ROAD

# MAP 109 SHEET 1

## SELWYN DISTRICT PLAN

Scale: 1:4,000 at A3



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### KEY

- SDC Designated Site
- Other Designations
- ChCh International Airport Noise Contour
- NZTA Widening Designation

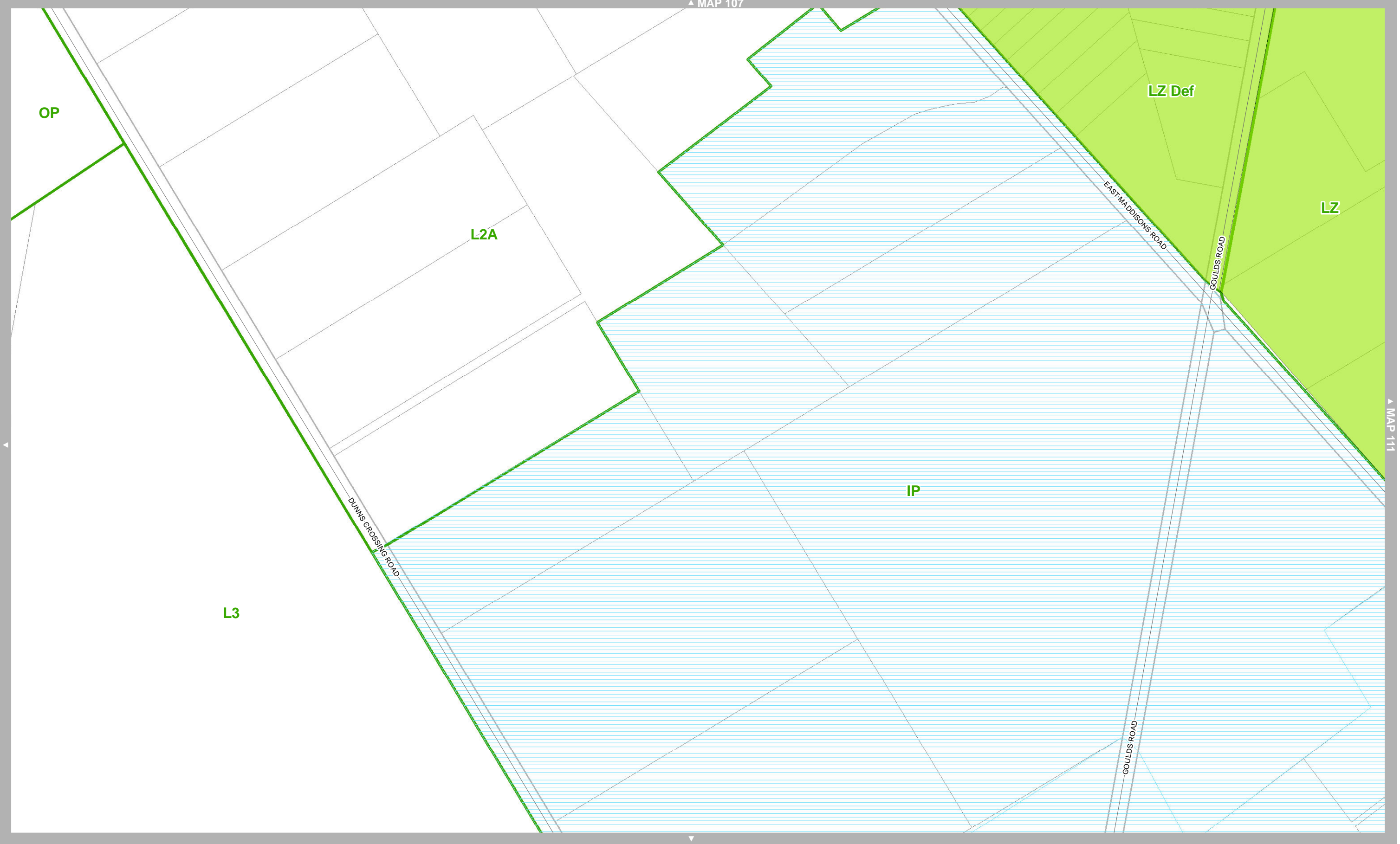
- Fault Lines
- Coastal Hazard
- Transpower Main Lines
- Zones
- Forestry Exclusions
- Outstanding Landscape

- West Melton Observatory Zone
- Outstanding Natural Feature
- Flood Zone**
- Waimakariri Flood Plain
- Lower Plains Flood Area
- Lake Ellesmere Flood Area

- Priority Areas - Business**
- Business Zoned
- Business Not Yet Zoned
- Priority Areas - Residential**
- Residential Zoned
- Residential Not Yet Zoned

- Greenfields Areas**
- Business Greenfields - post Land Use Recovery Plan
- Residential Greenfields - post Land Use Recovery Plan
- Territorial Authority**
- Surrounding Districts
- Selwyn District Boundary

LURP Priority and Greenfield Areas Plan Change  
**NOT YET OPERATIVE**  
 Map Made On: 6/05/2013



MAP  
**110**  
SHEET 1

SELWYN DISTRICT PLAN

Scale: 1:4,000 at A3



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**KEY**

- SDC Designated Site
- Other Designations
- ChCh International Airport Noise Contour
- NZTA Widening Designation

- Fault Lines
- Coastal Hazard
- Transpower Main Lines
- Zones
- Forestry Exclusions
- Outstanding Landscape

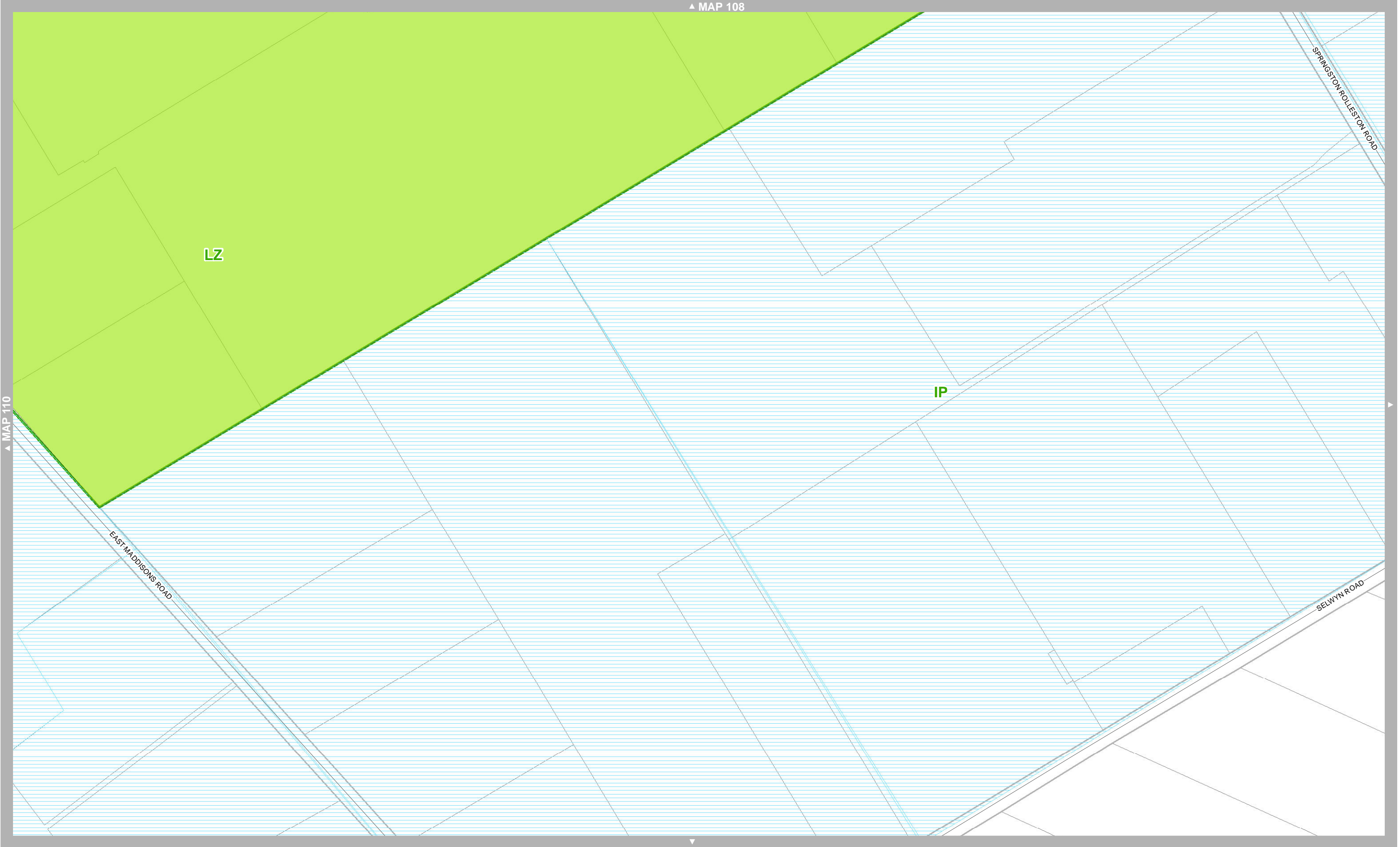
- West Melton Observatory Zone
- Outstanding Natural Feature
- Flood Zone**
- Waimakariri Flood Plain
- Lower Plains Flood Area
- Lake Ellesmere Flood Area

- Priority Areas - Business**
- Business Zoned
- Business Not Yet Zoned
- Priority Areas - Residential**
- Residential Zoned
- Residential Not Yet Zoned

- Greenfields Areas**
- Business Greenfields - post Land Use Recovery Plan
- Residential Greenfields - post Land Use Recovery Plan
- Territorial Authority**
- Surrounding Districts
- Selwyn District Boundary

LURP Priority and Greenfield Areas  
Plan Change  
**NOT YET OPERATIVE**  
Map Made On: 6/05/2013





MAP  
**111**  
SHEET 1

SELWYN DISTRICT PLAN

Scale: 1:4,000 at A3



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KEY

- SDC Designated Site
- Other Designations
- ChCh International Airport Noise Contour
- NZTA Widening Designation

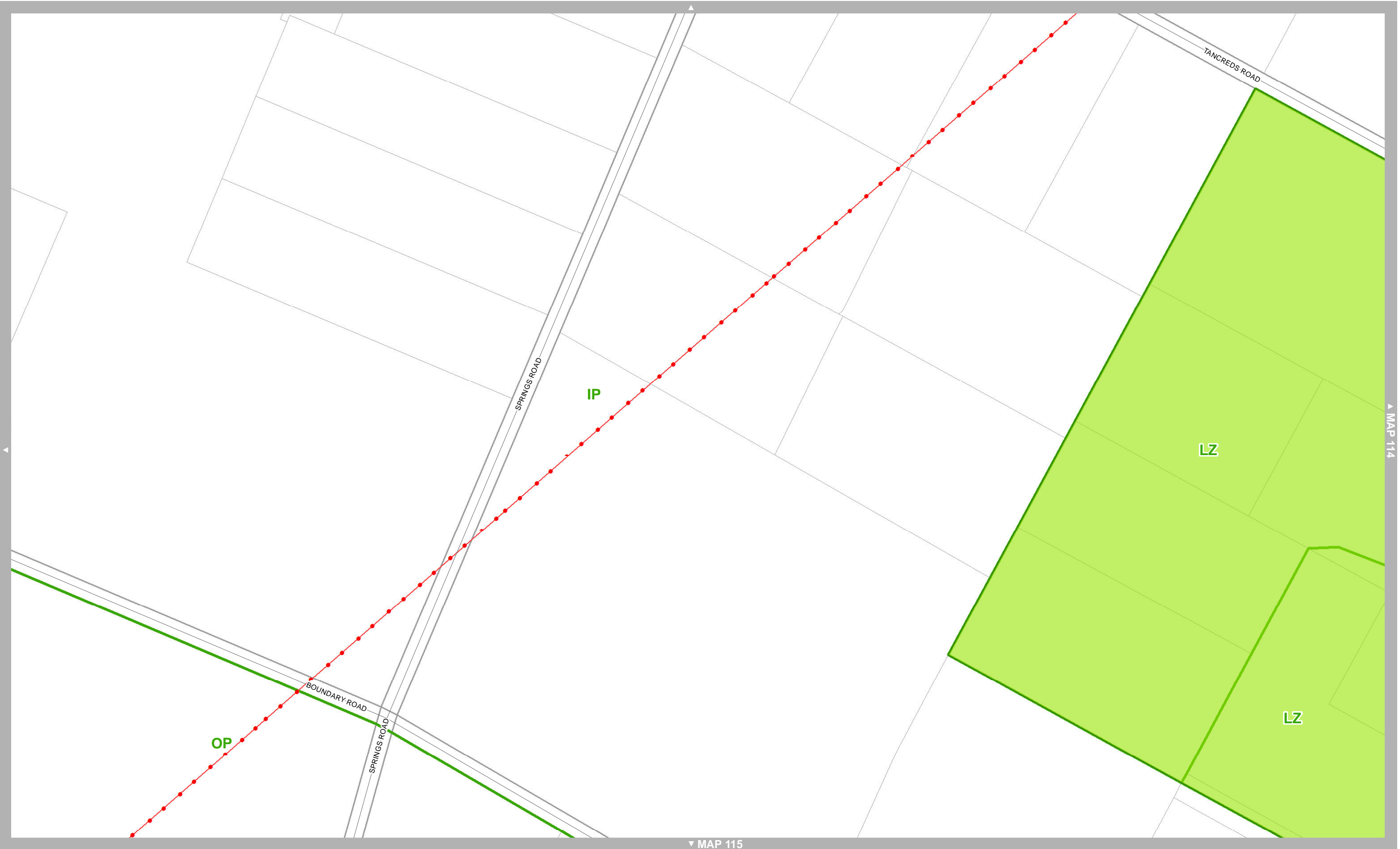
- Fault Lines
- Coastal Hazard
- Transpower Main Lines
- Zones
- Forestry Exclusions
- Outstanding Landscape

- West Melton Observatory Zone
- Outstanding Natural Feature
- Flood Zone**
- Waimakariri Flood Plain
- Lower Plains Flood Area
- Lake Ellesmere Flood Area

- Priority Areas - Business**
- Business Zoned
- Business Not Yet Zoned
- Priority Areas - Residential**
- Residential Zoned
- Residential Not Yet Zoned

- Greenfields Areas**
- Business Greenfields - post Land Use Recovery Plan
- Residential Greenfields - post Land Use Recovery Plan
- Territorial Authority**
- Surrounding Districts
- Selwyn District Boundary

LURP Priority and Greenfield Areas  
Plan Change  
**NOT YET OPERATIVE**  
Map Made On: 6/05/2013



MAP  
**113**  
SHEET 1

SELWYN DISTRICT PLAN

Scale: 1:4,000 at A3  
0 100 m

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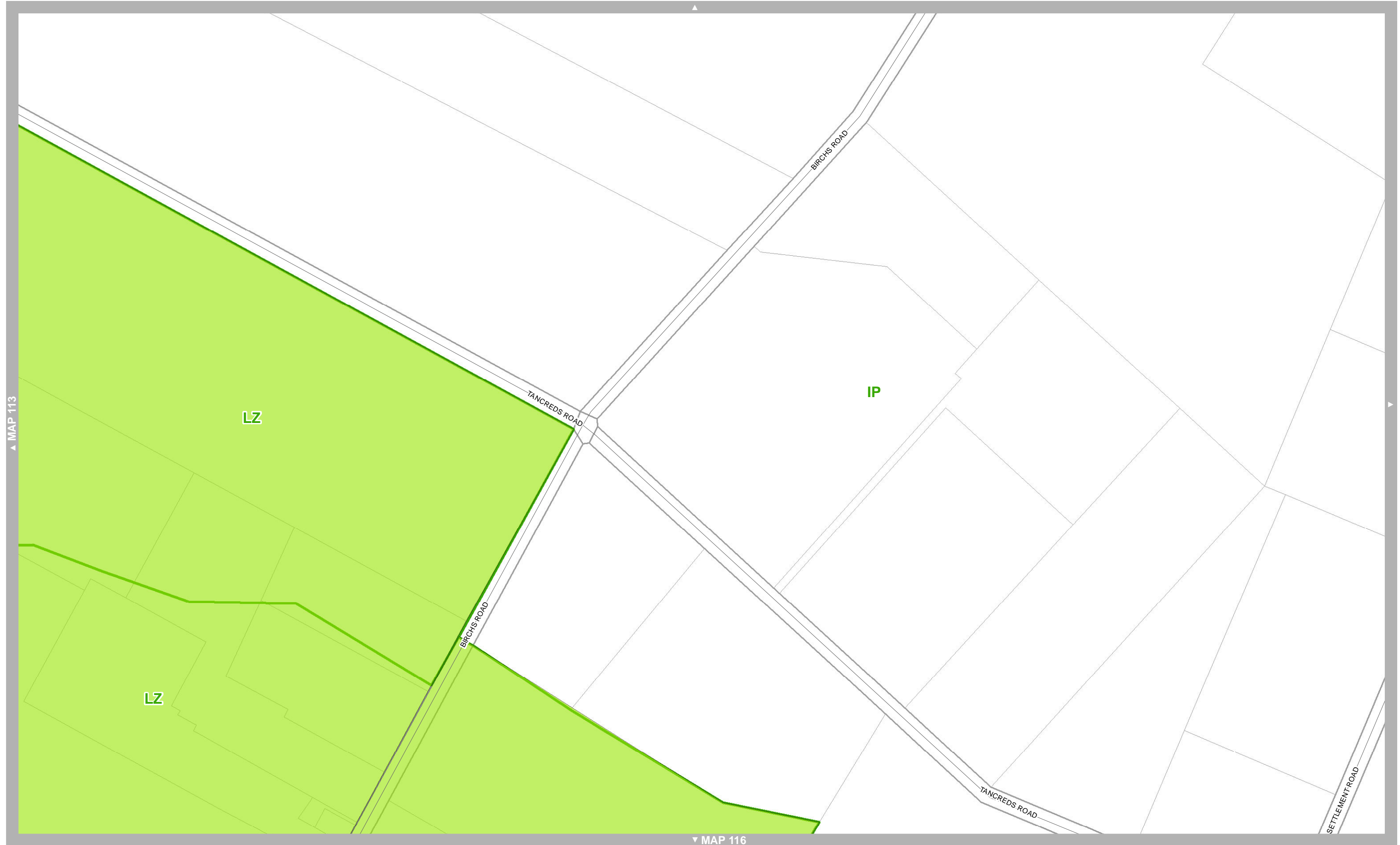
- KEY**
- SDC Designated Site
  - Other Designations
  - ChCh International Airport Noise Contour
  - NZTA Widening Designation

- Fault Lines
- Coastal Hazard
- Transpower Main Lines
- Zones
- Forestry Exclusions
- Outstanding Landscape
- West Melton Observatory Zone
- Outstanding Natural Feature
- Waimakariri Flood Plain
- Lower Plains Flood Area
- Lake Ellesmere Flood Area

- Priority Areas - Business**
- Business Zoned
  - Business Not Yet Zoned
- Priority Areas - Residential**
- Residential Zoned
  - Residential Not Yet Zoned

- Greenfields Areas**
- Business Greenfields - post Land Use Recovery Plan
  - Residential Greenfields - post Land Use Recovery Plan
- Territorial Authority**
- Surrounding Districts
  - Selwyn District Boundary

LURP Priority and Greenfield Areas  
Plan Change  
**NOT YET OPERATIVE**  
Map Made On: 8/05/2013



MAP  
**114**  
SHEET 1

SELWYN DISTRICT PLAN

Scale: 1:4,000 at A3



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KEY

- SDC Designated Site
- Other Designations
- ChCh International Airport Noise Contour
- NZTA Widening Designation

- Fault Lines
- Coastal Hazard
- Transpower Main Lines
- Zones
- Forestry Exclusions
- Outstanding Landscape

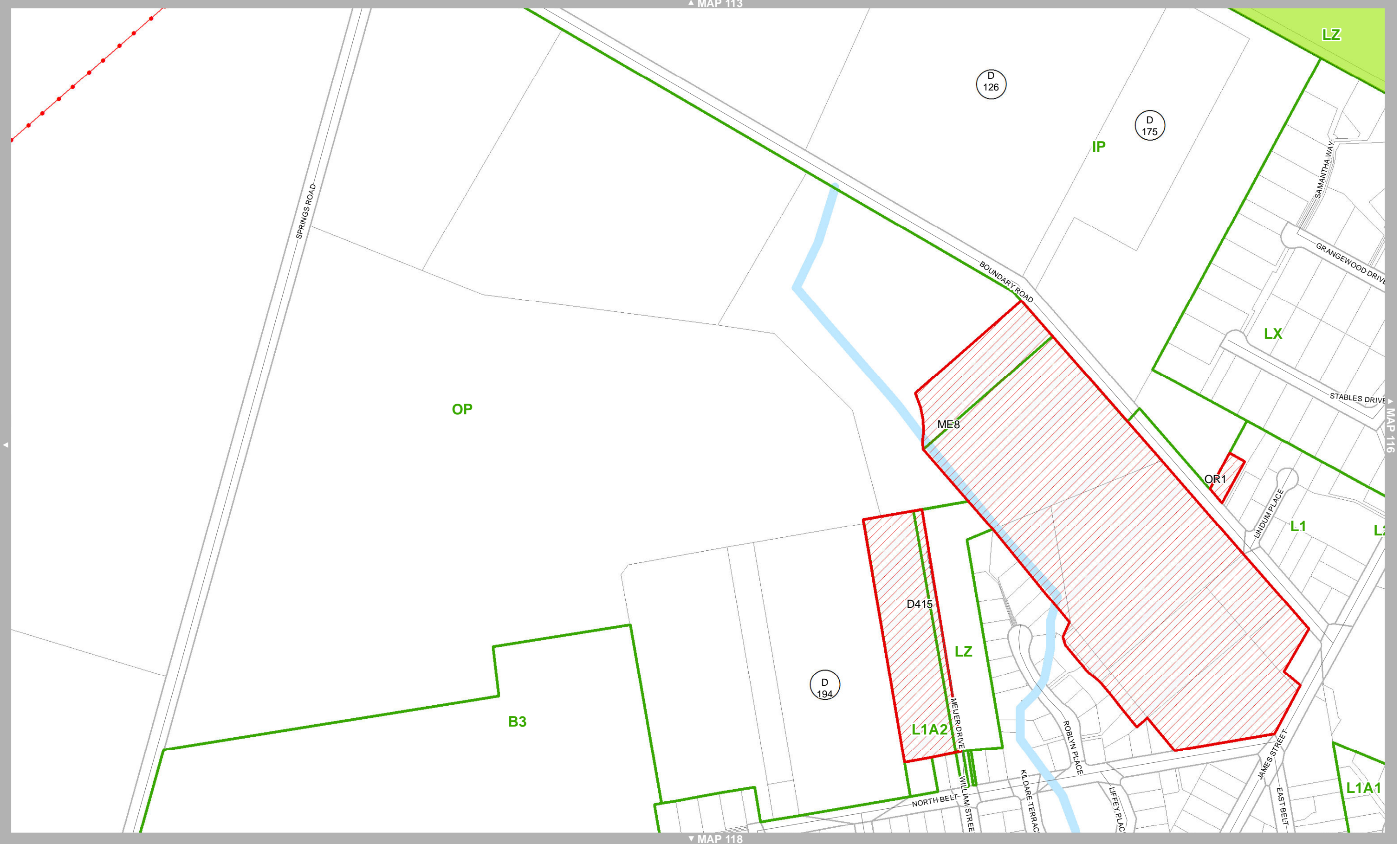
- West Melton Observatory Zone
- Outstanding Natural Feature
- Flood Zone**
- Waimakariri Flood Plain
- Lower Plains Flood Area
- Lake Ellesmere Flood Area

- Priority Areas - Business**
- Business Zoned
- Business Not Yet Zoned
- Priority Areas - Residential**
- Residential Zoned
- Residential Not Yet Zoned

- Greenfields Areas**
- Business Greenfields - post Land Use Recovery Plan
- Residential Greenfields - post Land Use Recovery Plan
- Territorial Authority**
- Surrounding Districts
- Selwyn District Boundary

LURP Priority and Greenfield Areas  
Plan Change  
**NOT YET OPERATIVE**  
Map Made On: 8/05/2013





MAP  
**115**  
SHEET 1

SELWYN DISTRICT PLAN

Scale: 1:4,000 at A3



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**KEY**

- SDC Designated Site
- Other Designations
- ChCh International Airport Noise Contour
- NZTA Widening Designation
- Zones
- Forestry Exclusions
- Outstanding Landscape

- Fault Lines
- Coastal Hazard
- Transpower Main Lines
- Zones
- Forestry Exclusions
- Outstanding Landscape

**Flood Zone**

- West Melton Observatory Zone
- Outstanding Natural Feature
- Waimakariri Flood Plain
- Lower Plains Flood Area
- Lake Ellesmere Flood Area

**Priority Areas - Business**

- Business Zoned
- Business Not Yet Zoned

**Priority Areas - Residential**

- Residential Zoned
- Residential Not Yet Zoned

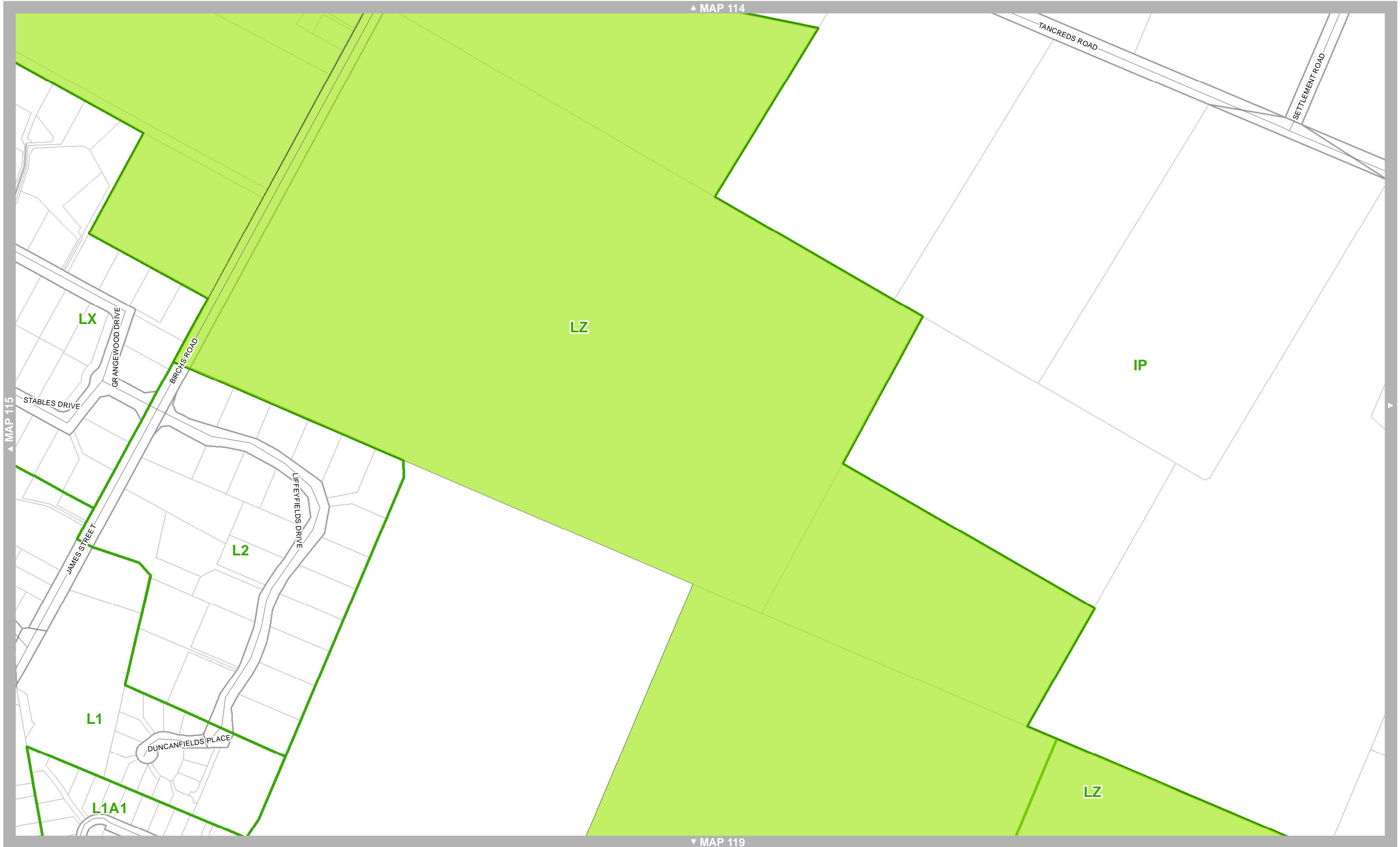
**Greenfields Areas**

- Business Greenfields - post Land Use Recovery Plan
- Residential Greenfields - post Land Use Recovery Plan

**Territorial Authority**

- Surrounding Districts
- Selwyn District Boundary

LURP Priority and Greenfield Areas  
Plan Change  
**NOT YET OPERATIVE**  
Map Made On: 6/05/2013



MAP  
**116**  
SHEET 1

SELWYN DISTRICT PLAN

Scale: 1:4,000 at A3



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**KEY**

- SDC Designated Site
- Other Designations
- ChCh International Airport Noise Contour
- NZTA Widening Designation

- Fault Lines
- Coastal Hazard
- Transpower Main Lines
- Zones
- Forestry Exclusions
- Outstanding Landscape
- West Melton Observatory Zone
- Outstanding Natural Feature
- Flood Zone**
- Waimakariri Flood Plain
- Lower Plains Flood Area
- Lake Ellesmere Flood Area

**Priority Areas - Business**

- Business Zoned
- Business Not Yet Zoned

**Priority Areas - Residential**

- Residential Zoned
- Residential Not Yet Zoned

**Greenfields Areas**

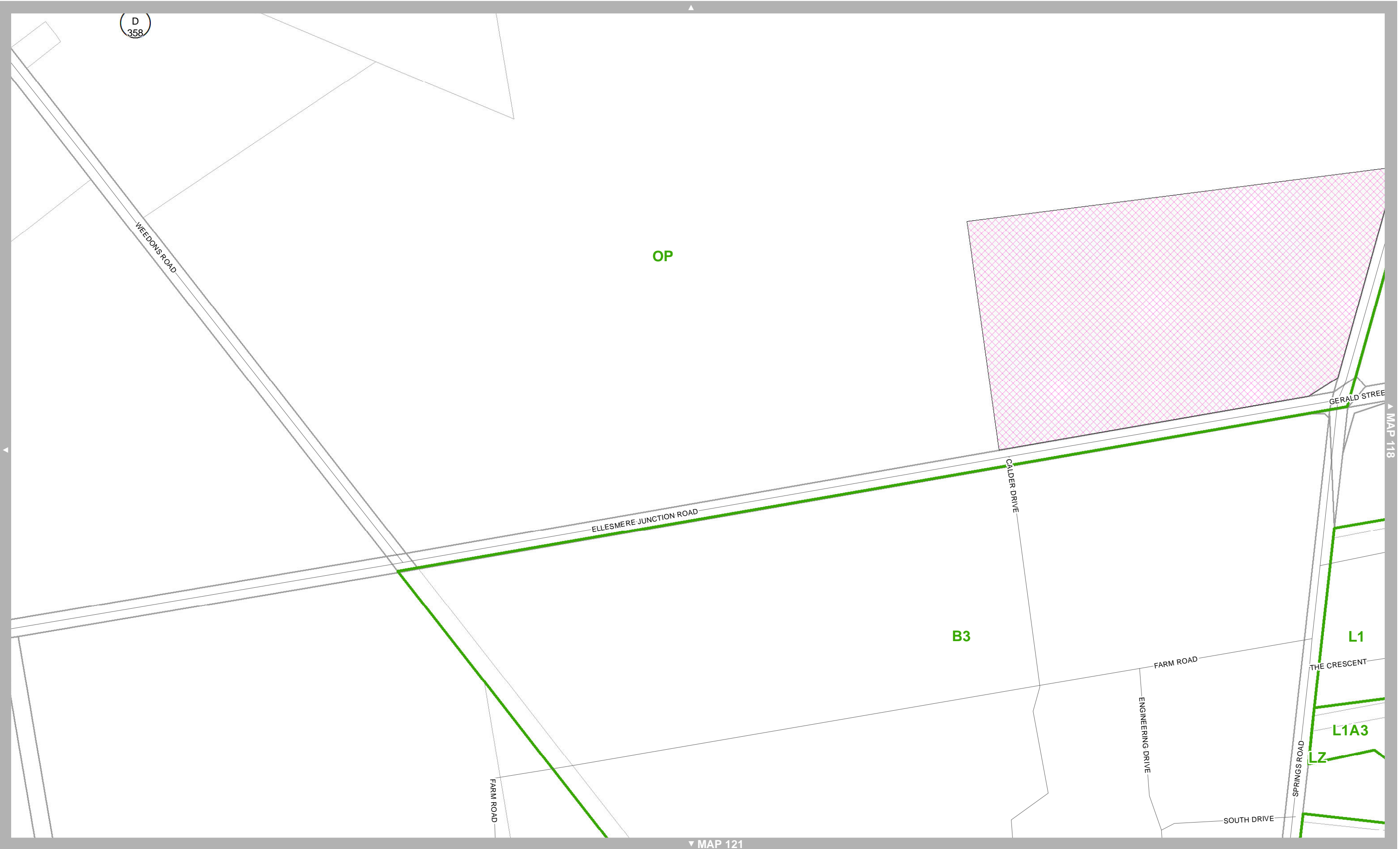
- Business Greenfields - post Land Use Recovery Plan
- Residential Greenfields - post Land Use Recovery Plan

**Territorial Authority**

- Surrounding Districts
- Selwyn District Boundary

LURP Priority and Greenfield Areas  
Plan Change  
**NOT YET OPERATIVE**  
Map Made On: 8/05/2013





MAP  
**117**  
SHEET 1

SELWYN DISTRICT PLAN

Scale: 1:4,000 at A3



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KEY

- SDC Designated Site
- Other Designations
- ChCh International Airport Noise Contour
- NZTA Widening Designation

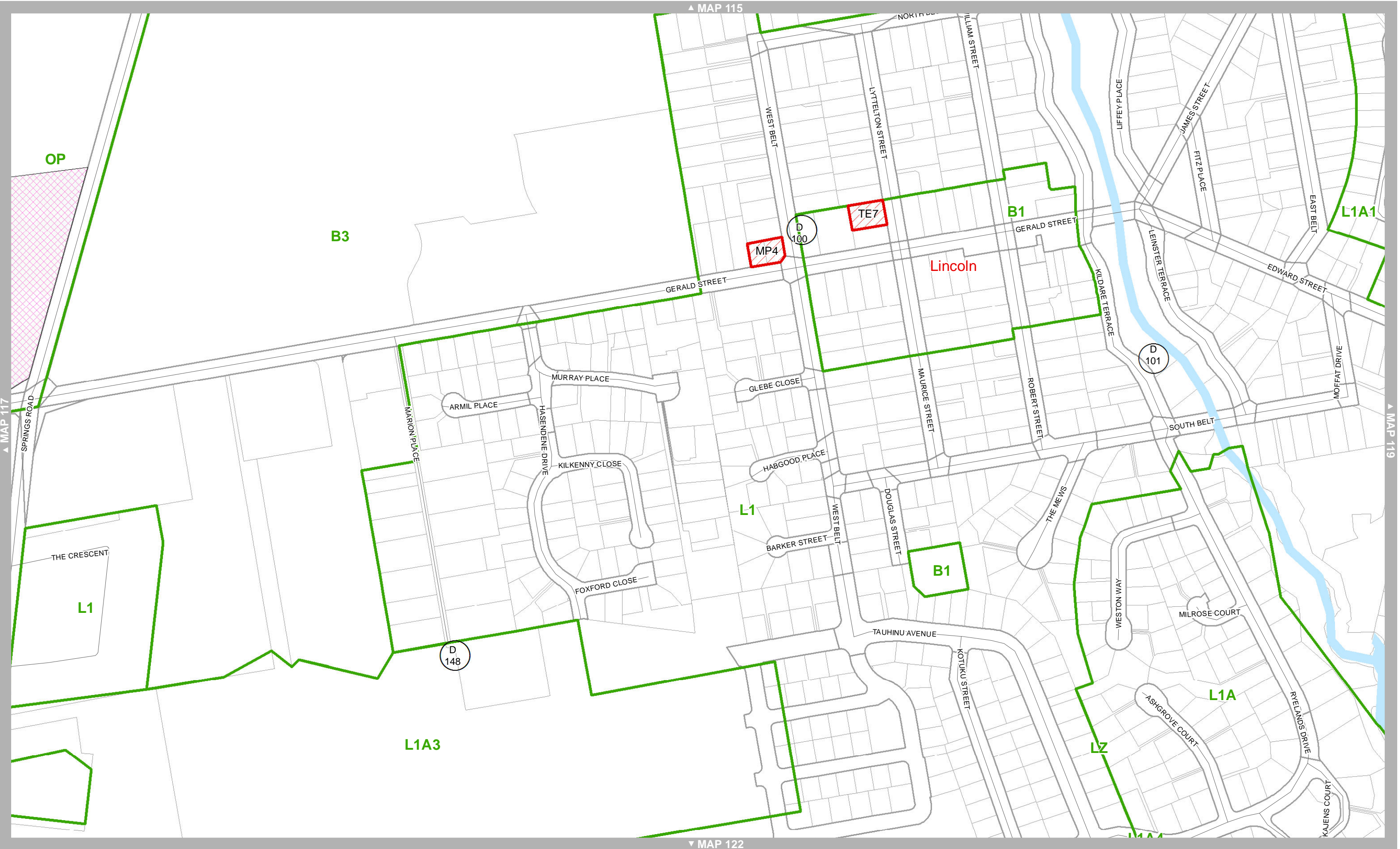
- Fault Lines
- Coastal Hazard
- Transpower Main Lines
- Zones
- Forestry Exclusions
- Outstanding Landscape
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- Outstanding Natural Feature
- Flood Zone
- Waimakariri Flood Plain
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- Priority Areas - Business**
- Business Zoned
- Business Not Yet Zoned
- Priority Areas - Residential**
- Residential Zoned
- Residential Not Yet Zoned

- Greenfields Areas**
- Business Greenfields - post Land Use Recovery Plan
- Residential Greenfields - post Land Use Recovery Plan
- Territorial Authority**
- Surrounding Districts
- Selwyn District Boundary

LURP Priority and Greenfield Areas  
Plan Change  
**NOT YET OPERATIVE**  
Map Made On: 8/05/2013





MAP  
**118**  
SHEET 1

SELWYN DISTRICT PLAN

Scale: 1:4,000 at A3  
0 100 m

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- KEY**
- SDC Designated Site
  - Other Designations
  - ChCh International Airport Noise Contour
  - NZTA Widening Designation

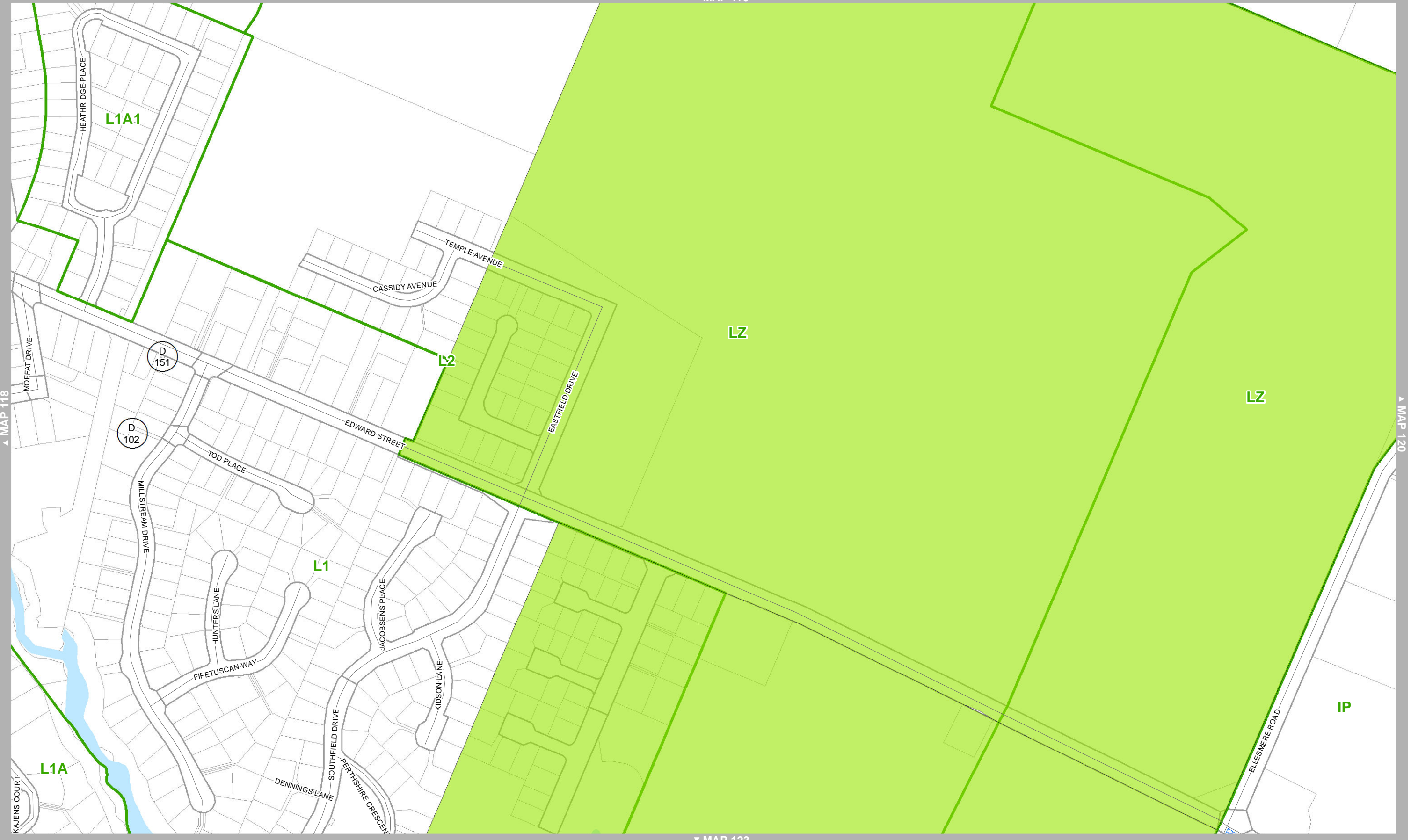
- Fault Lines
- Coastal Hazard
- Transpower Main Lines
- Zones
- Forestry Exclusions
- Outstanding Landscape
- West Melton Observatory Zone
- Outstanding Natural Feature
- Flood Zone
- Waimakariri Flood Plain
- Lower Plains Flood Area
- Lake Ellesmere Flood Area

- Priority Areas - Business**
- Business Zoned
  - Business Not Yet Zoned
- Priority Areas - Residential**
- Residential Zoned
  - Residential Not Yet Zoned

- Greenfields Areas**
- Business Greenfields - post Land Use Recovery Plan
  - Residential Greenfields - post Land Use Recovery Plan
- Territorial Authority**
- Surrounding Districts
  - Selwyn District Boundary

LURP Priority and Greenfield Areas  
Plan Change  
**NOT YET OPERATIVE**  
Map Made On: 8/05/2013





MAP  
**119**  
SHEET 1

SELWYN DISTRICT PLAN

Scale: 1:4,000 at A3



Prepared by Selwyn District Council.  
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KEY

- SDC Designated Site
- Other Designations
- ChCh International Airport Noise Contour
- NZTA Widening Designation

- Fault Lines
- Coastal Hazard
- Transpower Main Lines
- Zones
- Forestry Exclusions
- Outstanding Landscape
- West Melton Observatory Zone
- Outstanding Natural Feature
- Flood Zone
- Waimakariri Flood Plain
- Lower Plains Flood Area
- Lake Ellesmere Flood Area

Priority Areas - Business

- Business Zoned
- Business Not Yet Zoned

Priority Areas - Residential

- Residential Zoned
- Residential Not Yet Zoned

Greenfields Areas

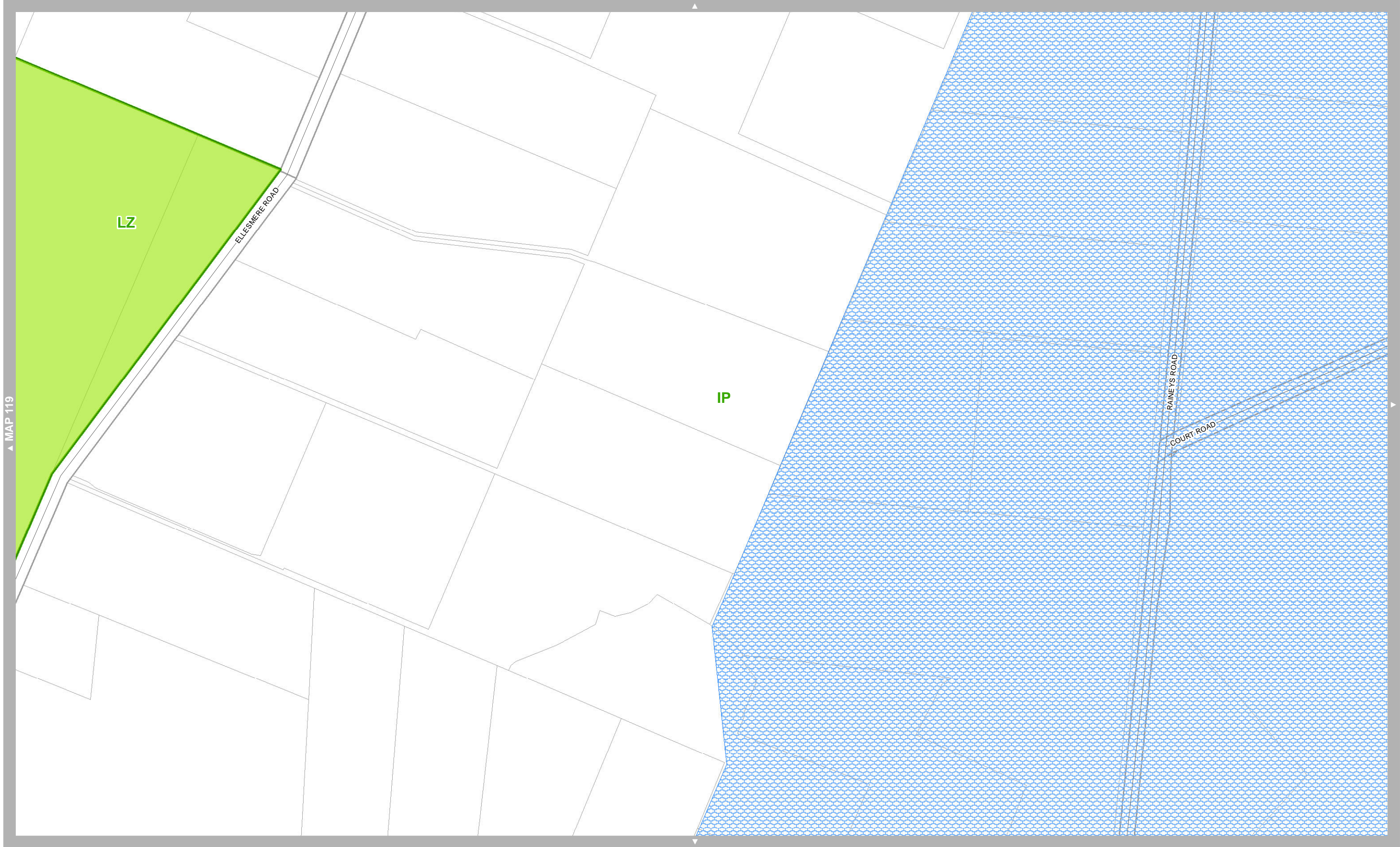
- Business Greenfields - post Land Use Recovery Plan
- Residential Greenfields - post Land Use Recovery Plan

Territorial Authority

- Surrounding Districts
- Selwyn District Boundary

LURP Priority and Greenfield Areas  
Plan Change  
**NOT YET OPERATIVE**  
Map Made On: 8/05/2013





MAP  
**120**  
SHEET 1

SELWYN DISTRICT PLAN

Scale: 1:4,000 at A3



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KEY

- SDC Designated Site
- Other Designations
- ChCh International Airport Noise Contour
- NZTA Widening Designation

- Fault Lines
- Coastal Hazard
- Transpower Main Lines
- Zones
- Forestry Exclusions
- Outstanding Landscape

- West Melton Observatory Zone
- Outstanding Natural Feature
- Flood Zone**
- Waimakariri Flood Plain
- Lower Plains Flood Area
- Lake Ellesmere Flood Area

Priority Areas - Business

- Business Zoned
- Business Not Yet Zoned

Priority Areas - Residential

- Residential Zoned
- Residential Not Yet Zoned

Greenfields Areas

- Business Greenfields - post Land Use Recovery Plan
- Residential Greenfields - post Land Use Recovery Plan

Territorial Authority

- Surrounding Districts
- Selwyn District Boundary

LURP Priority and Greenfield Areas  
Plan Change  
**NOT YET OPERATIVE**  
Map Made On: 8/05/2013





MAP  
**121**  
SHEET 1

SELWYN DISTRICT PLAN

Scale: 1:4,000 at A3



Prepared by Selwyn District Council.  
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**KEY**

- SDC Designated Site
- Other Designations
- ChCh International Airport Noise Contour
- NZTA Widening Designation

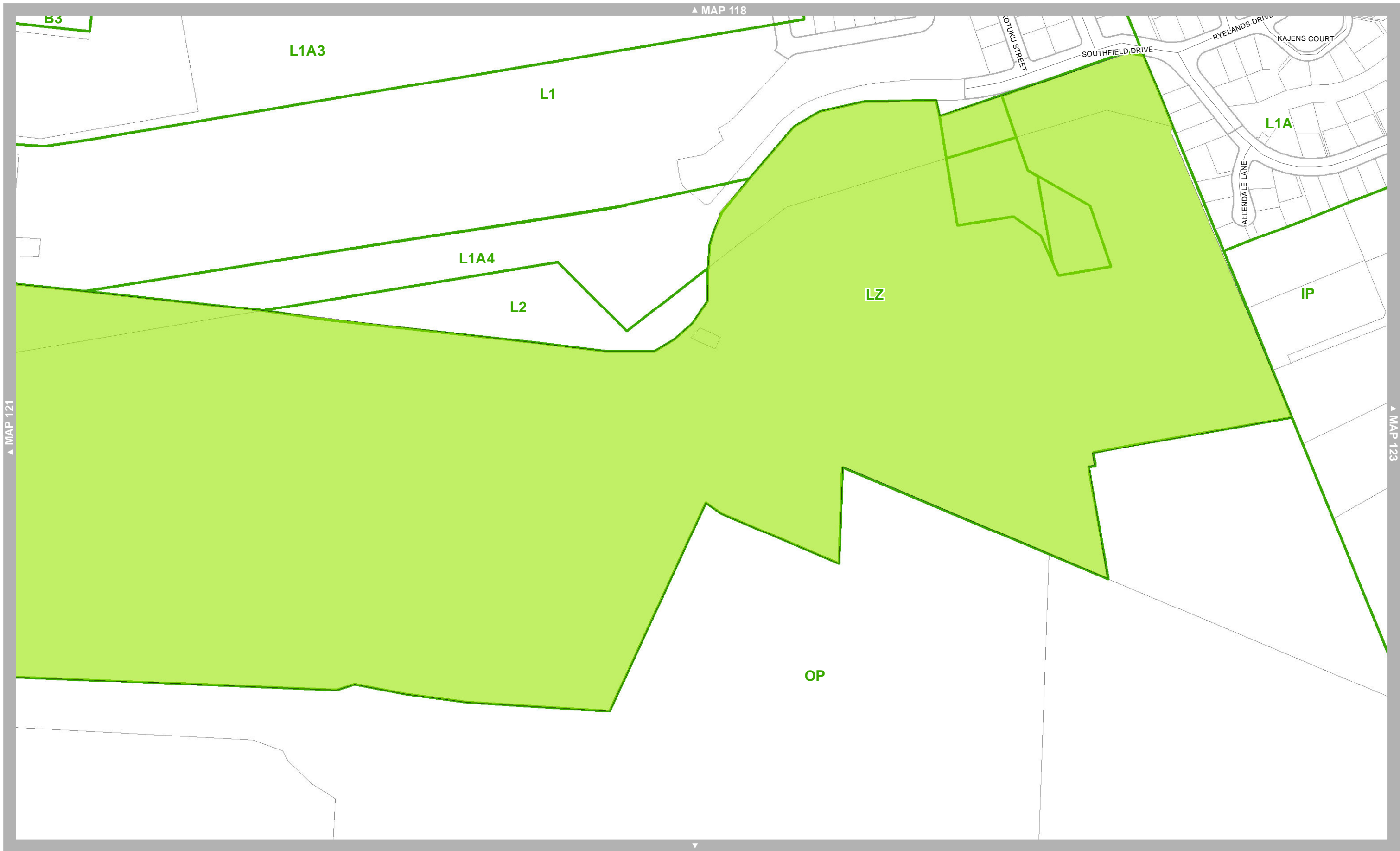
- Fault Lines
- Coastal Hazard
- Transpower Main Lines
- Zones
- Forestry Exclusions
- Outstanding Landscape

- West Melton Observatory Zone
- Outstanding Natural Feature
- Flood Zone**
- Waimakariri Flood Plain
- Lower Plains Flood Area
- Lake Ellesmere Flood Area

- Priority Areas - Business**
- Business Zoned
- Business Not Yet Zoned
- Priority Areas - Residential**
- Residential Zoned
- Residential Not Yet Zoned

- Greenfields Areas**
- Business Greenfields - post Land Use Recovery Plan
- Residential Greenfields - post Land Use Recovery Plan
- Territorial Authority**
- Surrounding Districts
- Selwyn District Boundary

LURP Priority and Greenfield Areas  
Plan Change  
**NOT YET OPERATIVE**  
Map Made On: 8/05/2013



MAP  
**122**  
SHEET 1

SELWYN DISTRICT PLAN

Scale: 1:4,000 at A3  
0 100 m

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- KEY**
- SDC Designated Site
  - Other Designations
  - ChCh International Airport Noise Contour
  - NZTA Widening Designation

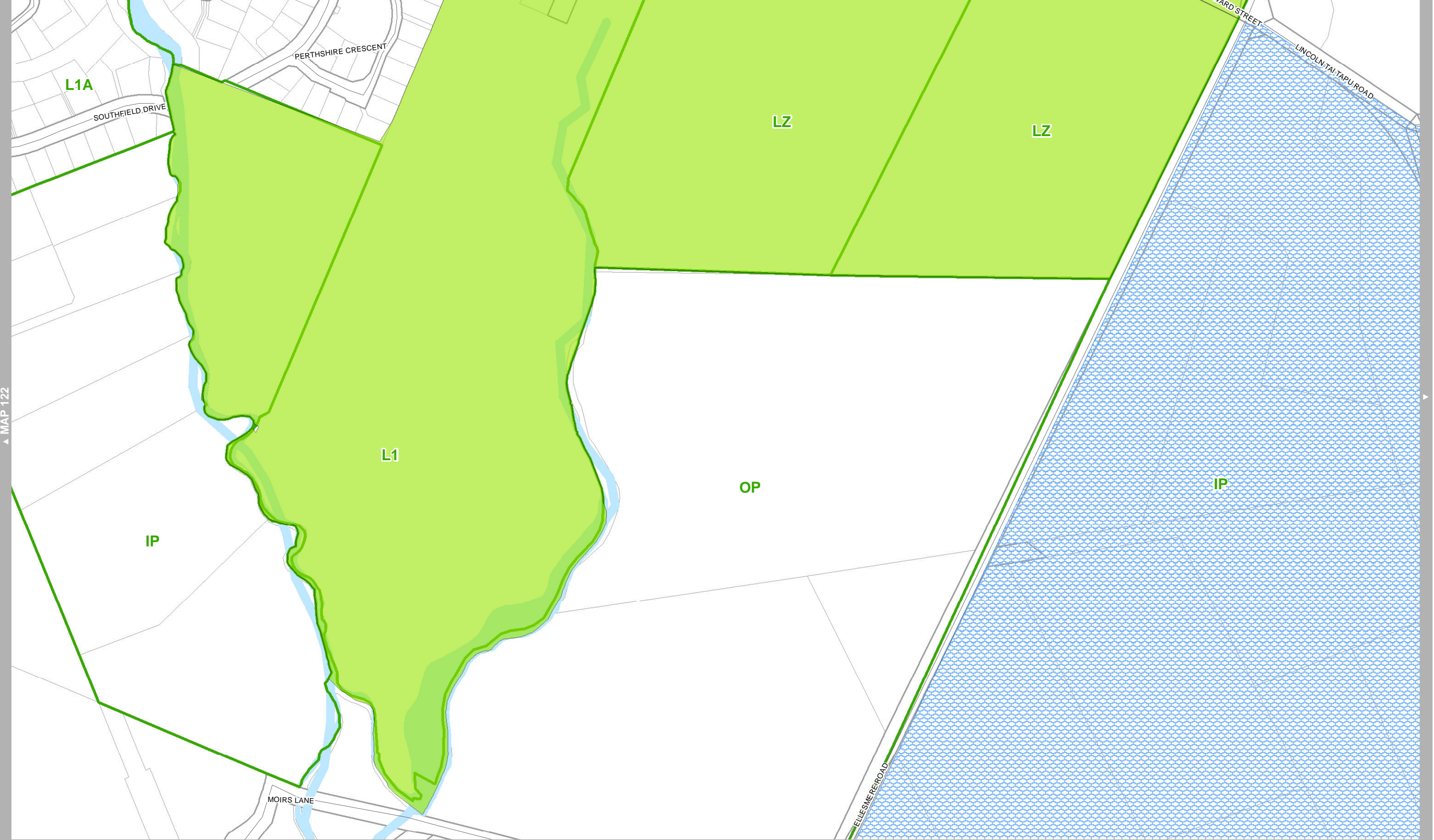
- Fault Lines
- Coastal Hazard
- Transpower Main Lines
- Zones
- Forestry Exclusions
- Outstanding Landscape
- West Melton Observatory Zone
- Outstanding Natural Feature
- Flood Zone**
- Waimakariri Flood Plain
- Lower Plains Flood Area
- Lake Ellesmere Flood Area

- Priority Areas - Business**
- Business Zoned
- Business Not Yet Zoned
- Priority Areas - Residential**
- Residential Zoned
- Residential Not Yet Zoned

- Greenfields Areas**
- Business Greenfields - post Land Use Recovery Plan
- Residential Greenfields - post Land Use Recovery Plan
- Territorial Authority**
- Surrounding Districts
- Selwyn District Boundary

LURP Priority and Greenfield Areas  
Plan Change  
**NOT YET OPERATIVE**  
Map Made On: 6/05/2013





MAP  
**123**  
SHEET 1

SELWYN DISTRICT PLAN

Scale: 1:4,000 at A3  
0 100 m

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- KEY**
- SDC Designated Site
  - Other Designations
  - ChCh International Airport Noise Contour
  - NZTA Widening Designation

- Fault Lines
- Coastal Hazard
- Transpower Main Lines
- Zones
- Forestry Exclusions
- Outstanding Landscape
- West Melton Observatory Zone
- Outstanding Natural Feature
- Flood Zone**
- Waimakariri Flood Plain
- Lower Plains Flood Area
- Lake Ellesmere Flood Area

- Priority Areas - Business**
- Business Zoned
  - Business Not Yet Zoned
- Priority Areas - Residential**
- Residential Zoned
  - Residential Not Yet Zoned

- Greenfields Areas**
- Business Greenfields - post Land Use Recovery Plan
  - Residential Greenfields - post Land Use Recovery Plan
- Territorial Authority**
- Surrounding Districts
  - Selwyn District Boundary

LURP Priority and Greenfield Areas  
Plan Change  
**NOT YET OPERATIVE**  
Map Made On: 8/05/2013



# MAP 125 SHEET 1

## SELWYN DISTRICT PLAN

Scale: 1:4,000 at A3



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### KEY

- SDC Designated Site
- Other Designations
- ChCh International Airport Noise Contour
- NZTA Widening Designation

- Fault Lines
- Coastal Hazard
- Transpower Main Lines
- Zones
- Forestry Exclusions
- Outstanding Landscape

- West Melton Observatory Zone
- Outstanding Natural Feature
- Flood Zone**
- Waimakariri Flood Plain
- Lower Plains Flood Area
- Lake Ellesmere Flood Area

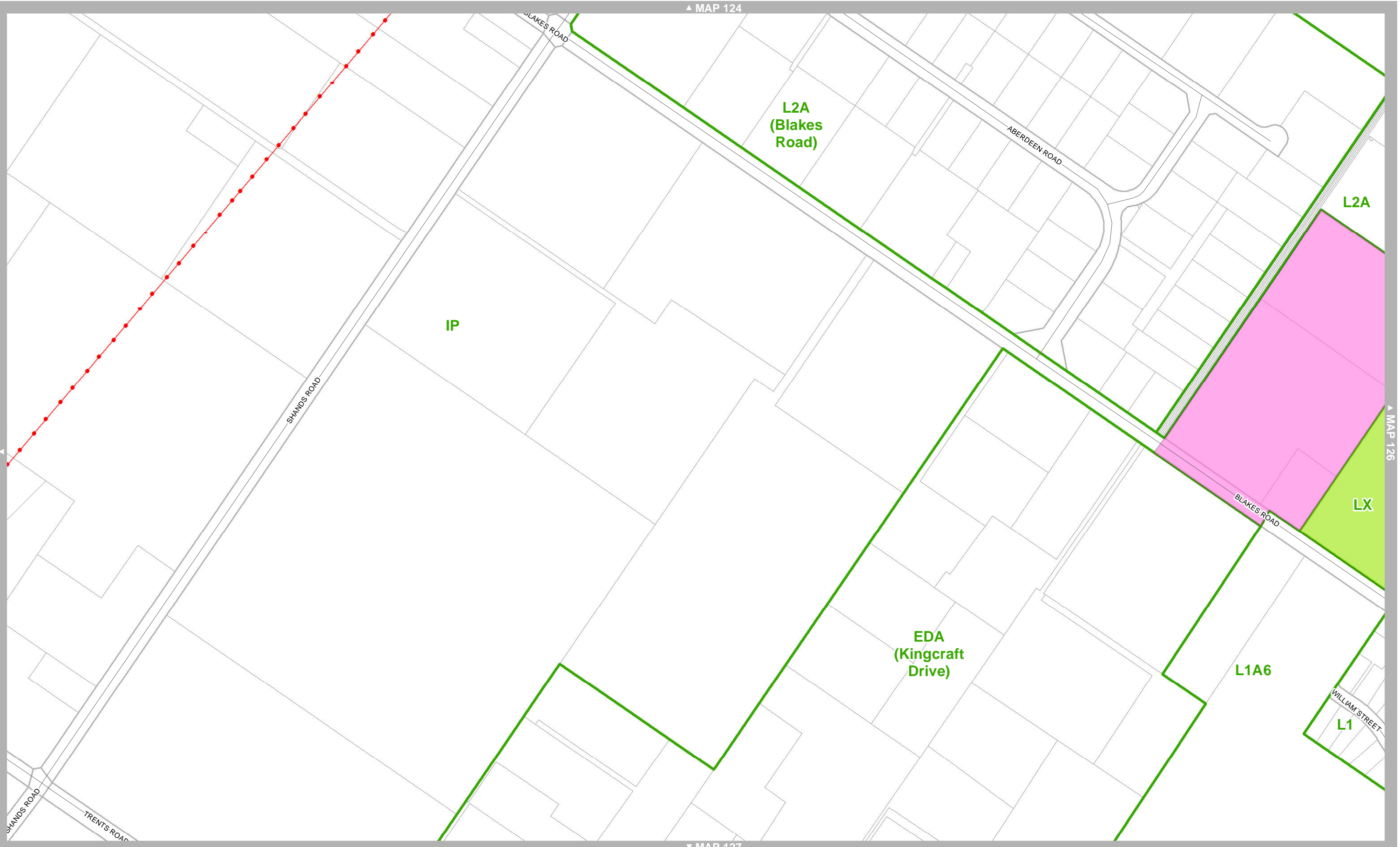
- Priority Areas - Business**
- Business Zoned
- Business Not Yet Zoned
- Priority Areas - Residential**
- Residential Zoned
- Residential Not Yet Zoned

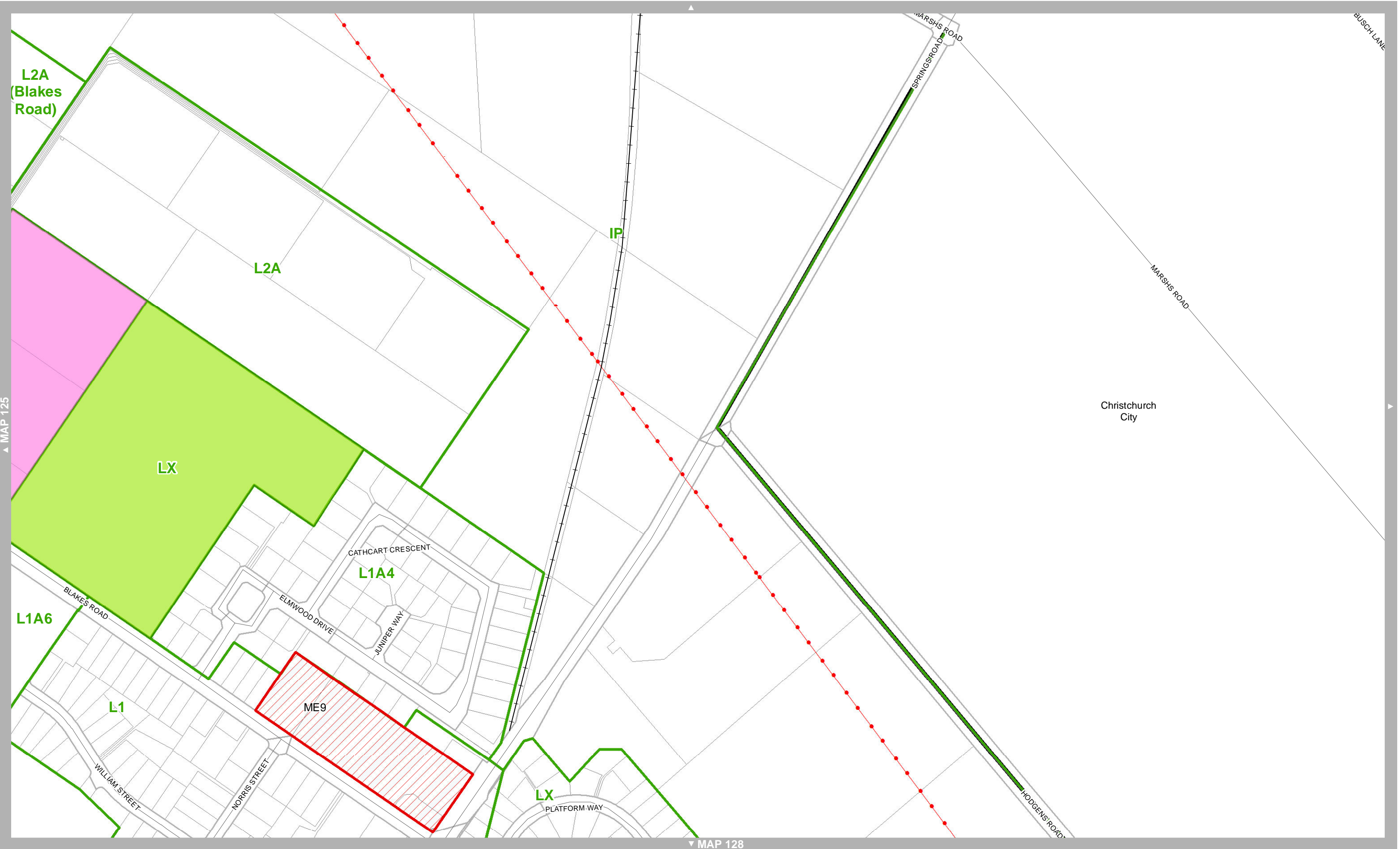
- Greenfields Areas**
- Business Greenfields - post Land Use Recovery Plan
- Residential Greenfields - post Land Use Recovery Plan
- Territorial Authority**
- Surrounding Districts
- Selwyn District Boundary

LURP Priority and Greenfield Areas Plan Change

**NOT YET OPERATIVE**

Map Made On: 6/05/2013





MAP  
**126**  
SHEET 1

**SELWYN DISTRICT PLAN**

Scale: 1:4,000 at A3  
0 100 m

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- KEY**
- SDC Designated Site
  - Other Designations
  - ChCh International Airport Noise Contour
  - NZTA Widening Designation

- Fault Lines
- Coastal Hazard
- Transpower Main Lines
- Zones
- Forestry Exclusions
- Outstanding Landscape
- West Melton Observatory Zone
- Outstanding Natural Feature
- Waimakariri Flood Plain
- Lower Plains Flood Area
- Lake Ellesmere Flood Area

- Priority Areas - Business**
- Business Zoned
  - Business Not Yet Zoned
- Priority Areas - Residential**
- Residential Zoned
  - Residential Not Yet Zoned

- Greenfields Areas**
- Business Greenfields - post Land Use Recovery Plan
  - Residential Greenfields - post Land Use Recovery Plan
- Territorial Authority**
- Surrounding Districts
  - Selwyn District Boundary

LURP Priority and Greenfield Areas  
Plan Change  
**NOT YET OPERATIVE**  
Map Made On: 6/05/2013



# MAP 127 SHEET 1

## SELWYN DISTRICT PLAN

Scale: 1:4,000 at A3



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### KEY

- SDC Designated Site
- Other Designations
- ChCh International Airport Noise Contour
- NZTA Widening Designation

- Fault Lines
- Coastal Hazard
- Transpower Main Lines
- Zones
- Forestry Exclusions
- Outstanding Landscape

- West Melton Observatory Zone
- Outstanding Natural Feature
- Flood Zone**
- Waimakariri Flood Plain
- Lower Plains Flood Area
- Lake Ellesmere Flood Area

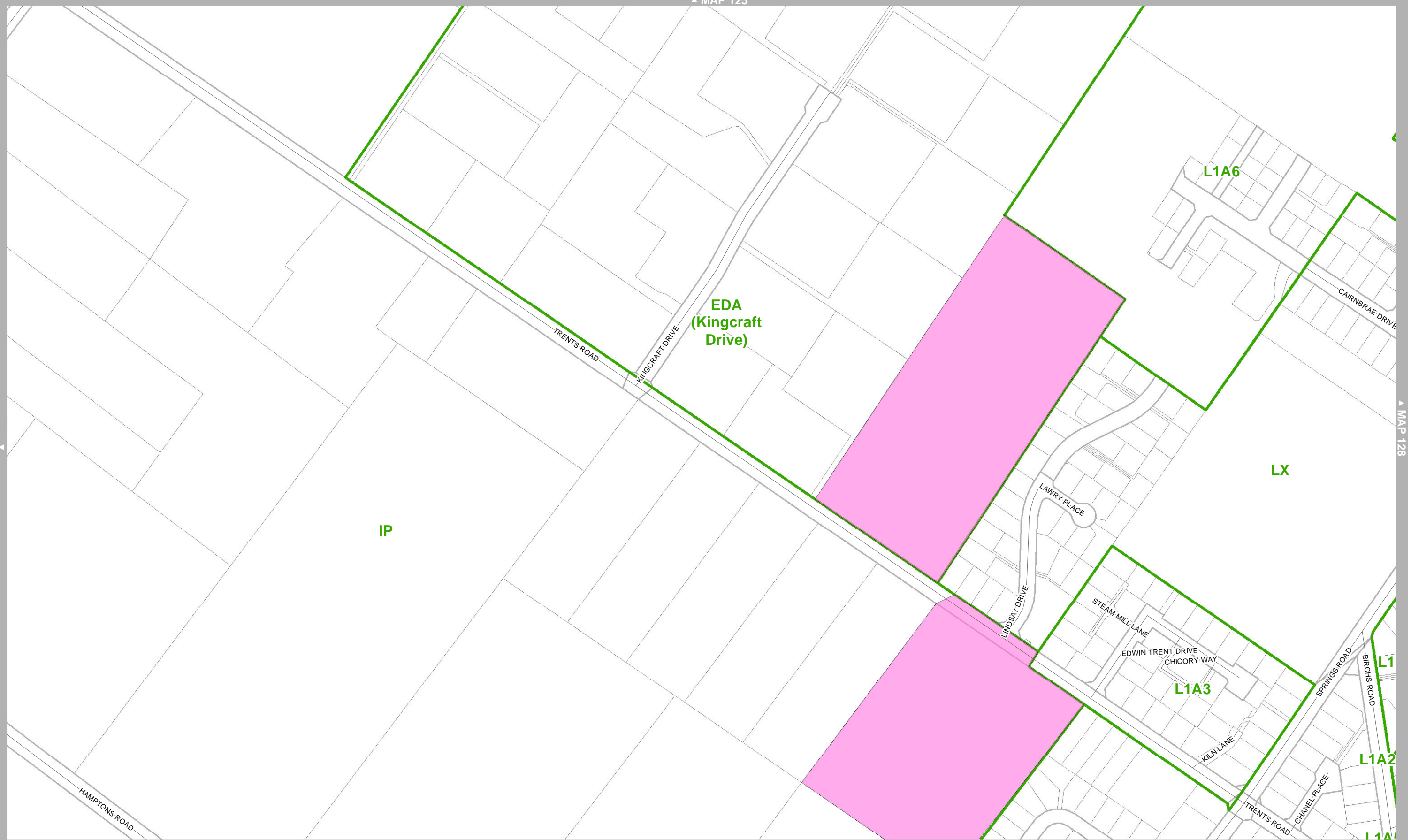
- Priority Areas - Business**
- Business Zoned
- Business Not Yet Zoned
- Priority Areas - Residential**
- Residential Zoned
- Residential Not Yet Zoned

- Greenfields Areas**
- Business Greenfields - post Land Use Recovery Plan
- Residential Greenfields - post Land Use Recovery Plan
- Territorial Authority**
- Surrounding Districts
- Selwyn District Boundary

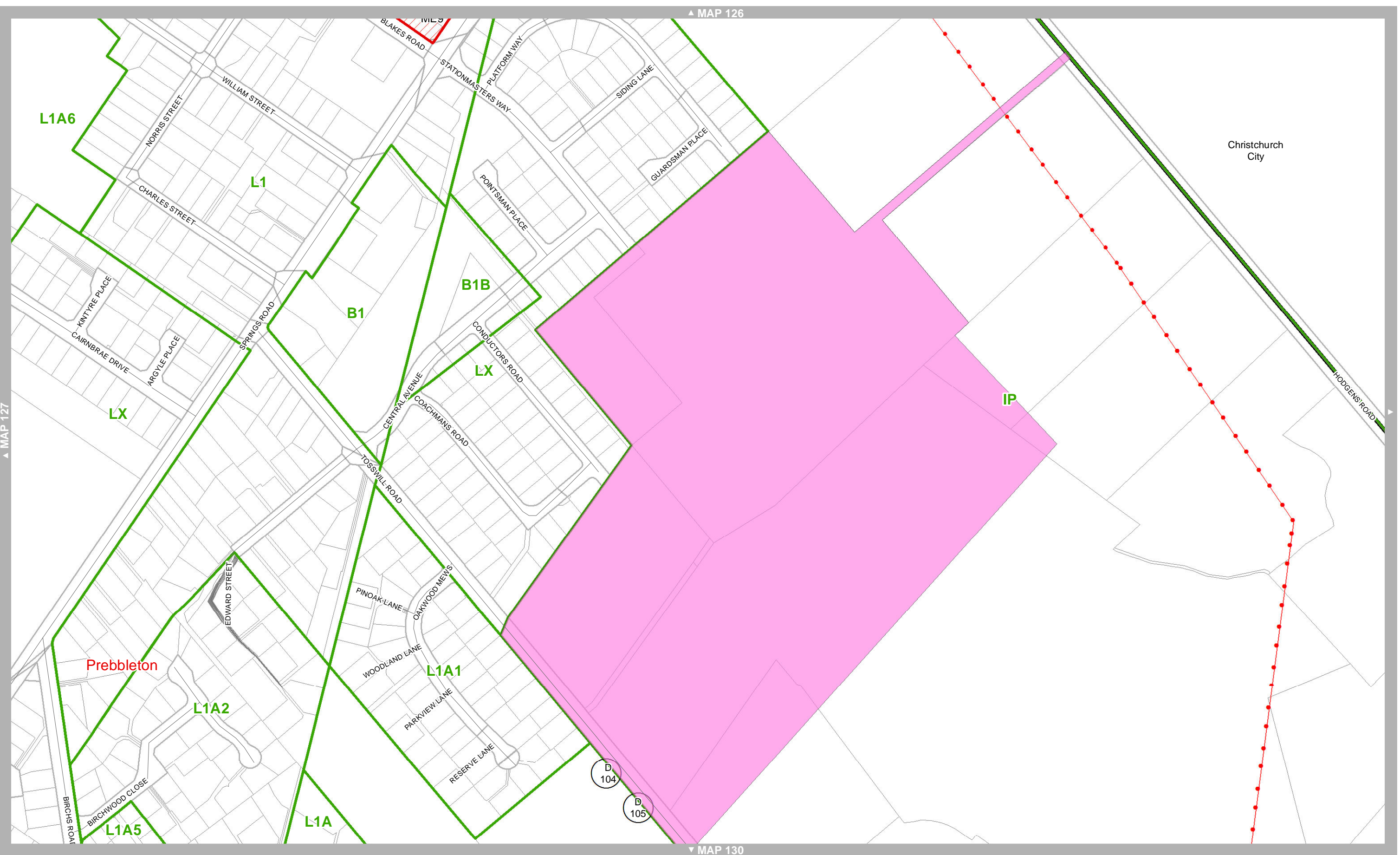
LURP Priority and Greenfield Areas Plan Change

**NOT YET OPERATIVE**

Map Made On: 6/05/2013







MAP  
**128**  
SHEET 1

SELWYN DISTRICT PLAN

Scale: 1:4,000 at A3  
0 100 m

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- KEY**
- SDC Designated Site
  - Other Designations
  - ChCh International Airport Noise Contour
  - NZTA Widening Designation

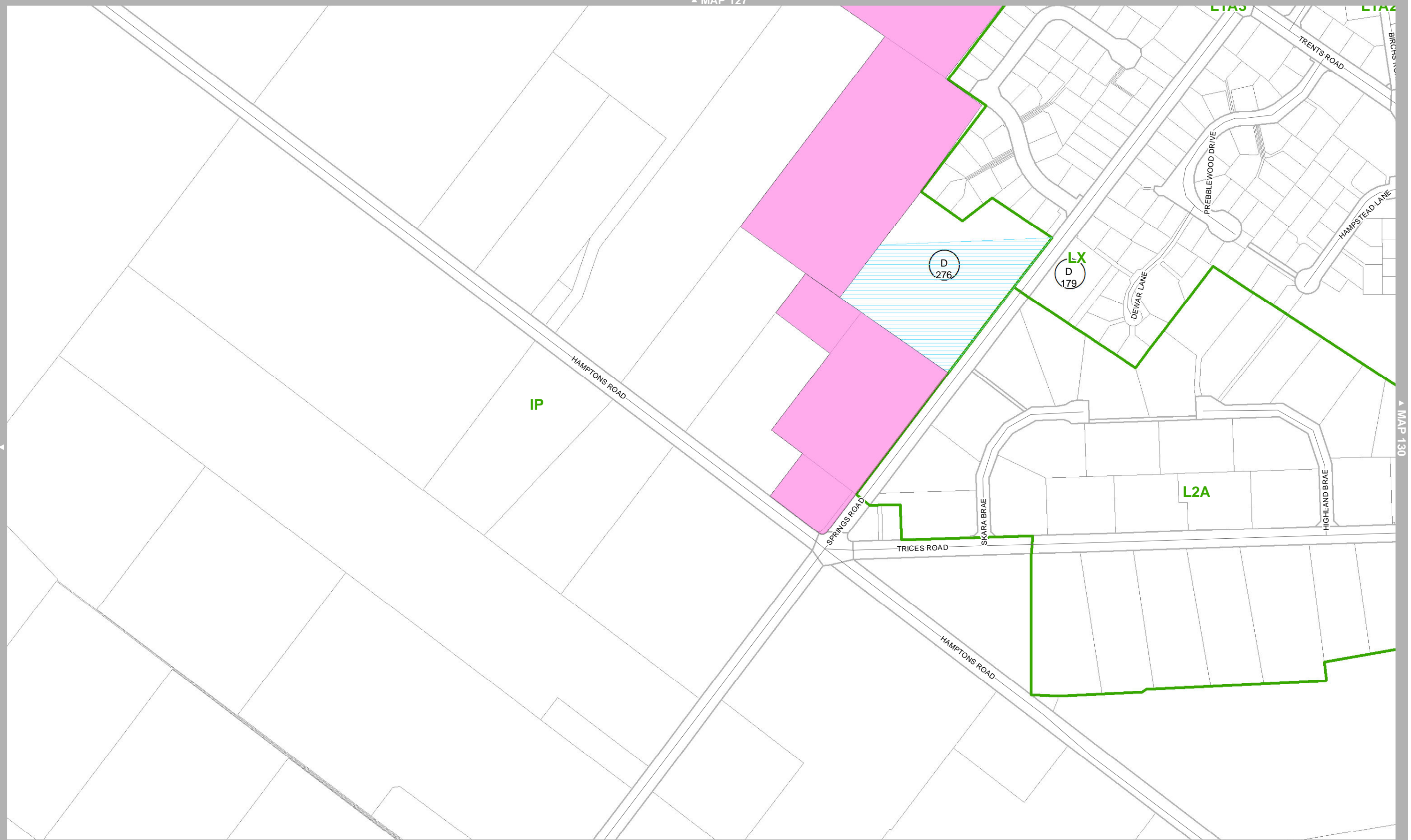
- Fault Lines
- Coastal Hazard
- Transpower Main Lines
- Zones
- Forestry Exclusions
- Outstanding Landscape
- West Melton Observatory Zone
- Outstanding Natural Feature
- Waimakariri Flood Plain
- Lower Plains Flood Area
- Lake Ellesmere Flood Area

- Priority Areas - Business**
- Business Zoned
  - Business Not Yet Zoned
- Priority Areas - Residential**
- Residential Zoned
  - Residential Not Yet Zoned

- Greenfields Areas**
- Business Greenfields - post Land Use Recovery Plan
  - Residential Greenfields - post Land Use Recovery Plan
- Territorial Authority**
- Surrounding Districts
  - Selwyn District Boundary

LURP Priority and Greenfield Areas  
Plan Change  
**NOT YET OPERATIVE**  
Map Made On: 6/05/2013





MAP  
**129**  
SHEET 1

SELWYN DISTRICT PLAN

Scale: 1:4,000 at A3  
0 100 m

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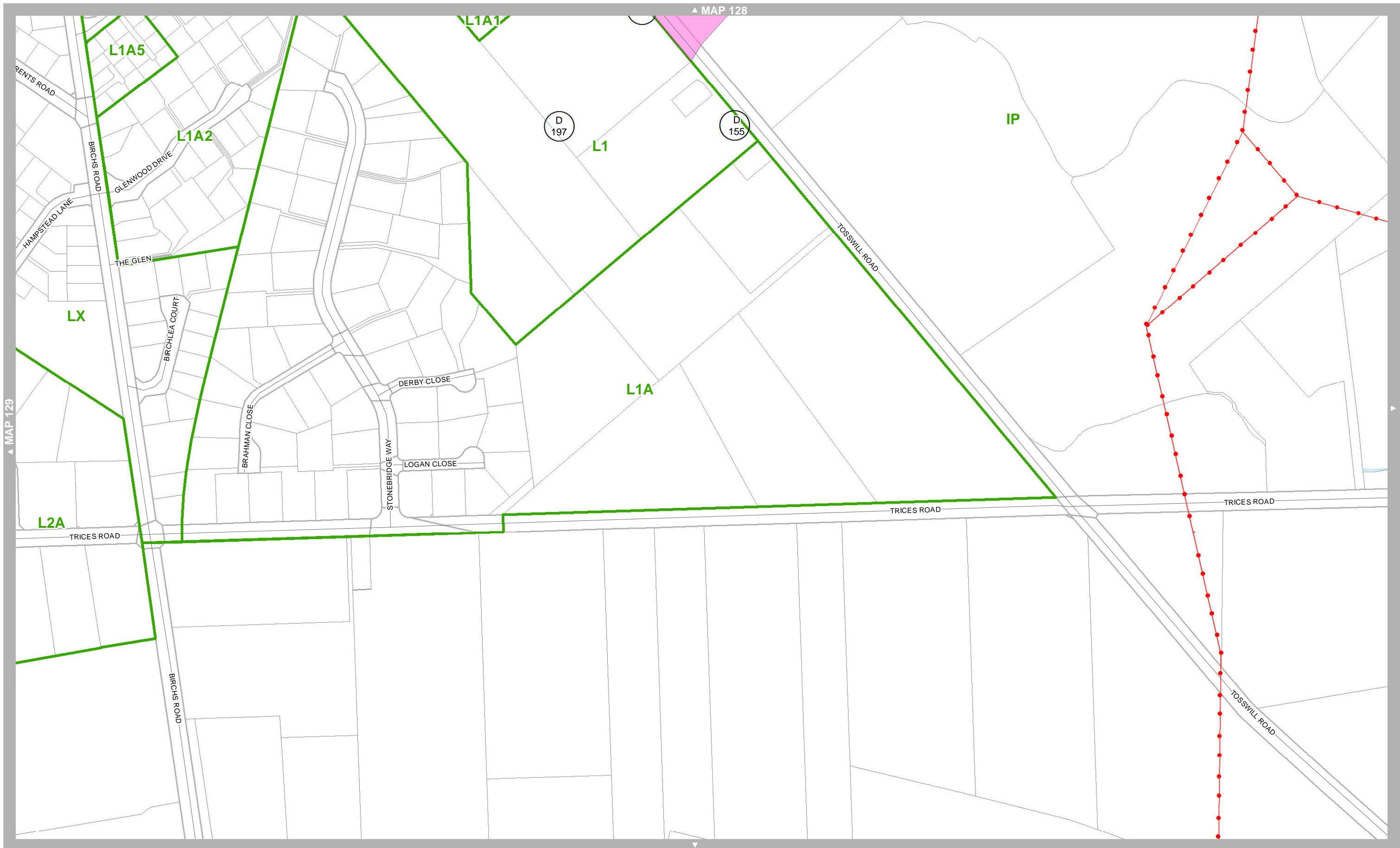
- KEY**
- SDC Designated Site
  - Other Designations
  - ChCh International Airport Noise Contour
  - NZTA Widening Designation

- Fault Lines
- Coastal Hazard
- Transpower Main Lines
- Zones
- Forestry Exclusions
- Outstanding Landscape
- West Melton Observatory Zone
- Outstanding Natural Feature
- Flood Zone**
- Waimakariri Flood Plain
- Lower Plains Flood Area
- Lake Ellesmere Flood Area

- Priority Areas - Business**
- Business Zoned
  - Business Not Yet Zoned
- Priority Areas - Residential**
- Residential Zoned
  - Residential Not Yet Zoned

- Greenfields Areas**
- Business Greenfields - post Land Use Recovery Plan
  - Residential Greenfields - post Land Use Recovery Plan
- Territorial Authority**
- Surrounding Districts
  - Selwyn District Boundary

LURP Priority and Greenfield Areas  
Plan Change  
**NOT YET OPERATIVE**  
Map Made On: 6/05/2013



MAP  
**130**  
SHEET 1

SELWYN DISTRICT PLAN

Scale: 1:4,000 at A3



Prepared by Selwyn District Council.  
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KEY

- SDC Designated Site
- Other Designations
- ChCh International Airport Noise Contour
- NZTA Widening Designation

- Fault Lines
- Coastal Hazard
- Transpower Main Lines
- Zones
- Forestry Exclusions
- Outstanding Landscape
- West Melton Observatory Zone
- Outstanding Natural Feature
- Waimakariri Flood Plain
- Lower Plains Flood Area
- Lake Ellesmere Flood Area

Priority Areas - Business

- Business Zoned
- Business Not Yet Zoned

Priority Areas - Residential

- Residential Zoned
- Residential Not Yet Zoned

Greenfields Areas

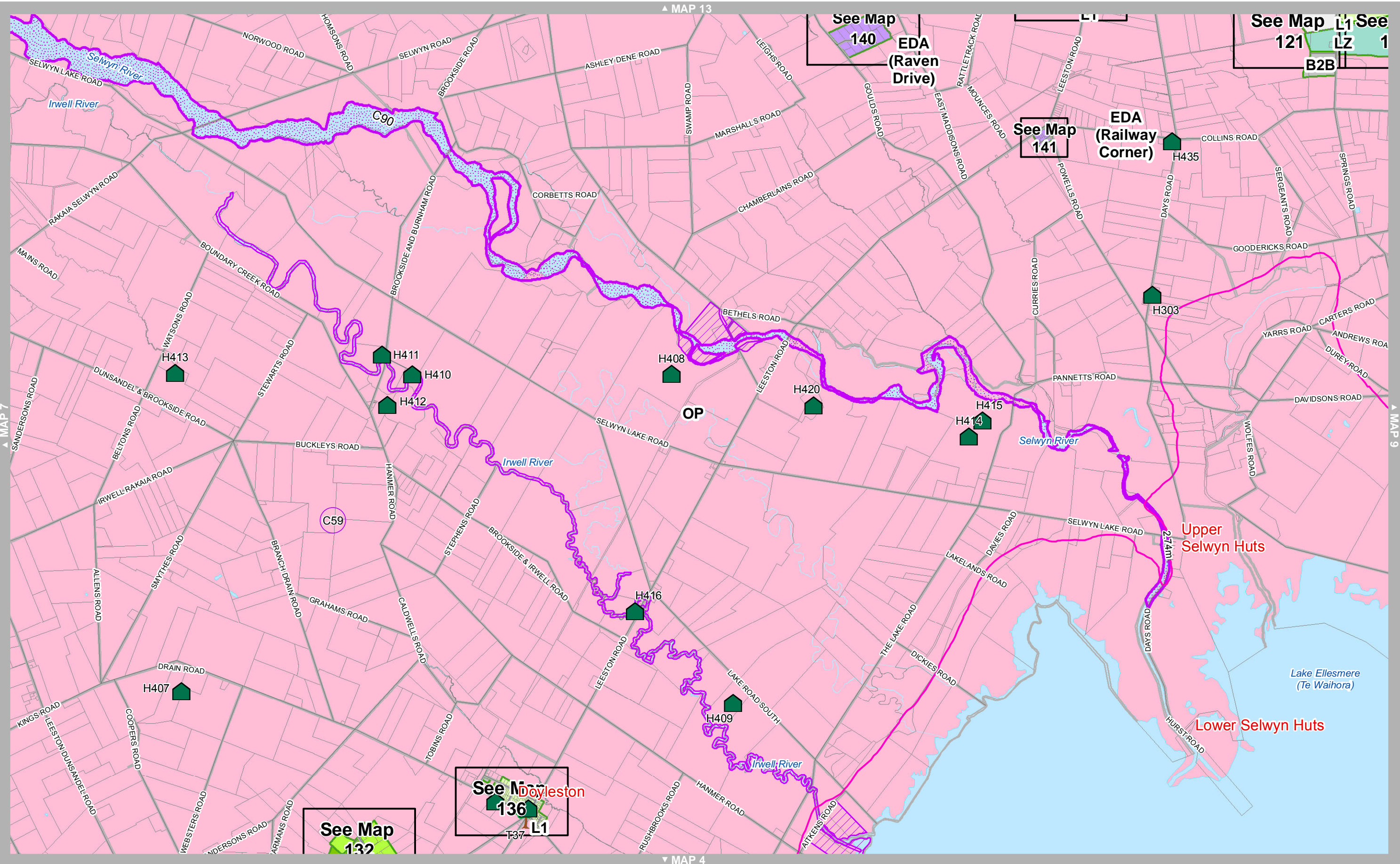
- Business Greenfields - post Land Use Recovery Plan
- Residential Greenfields - post Land Use Recovery Plan

Territorial Authority

- Surrounding Districts
- Selwyn District Boundary

LURP Priority and Greenfield Areas  
Plan Change  
**NOT YET OPERATIVE**  
Map Made On: 6/05/2013





KEY

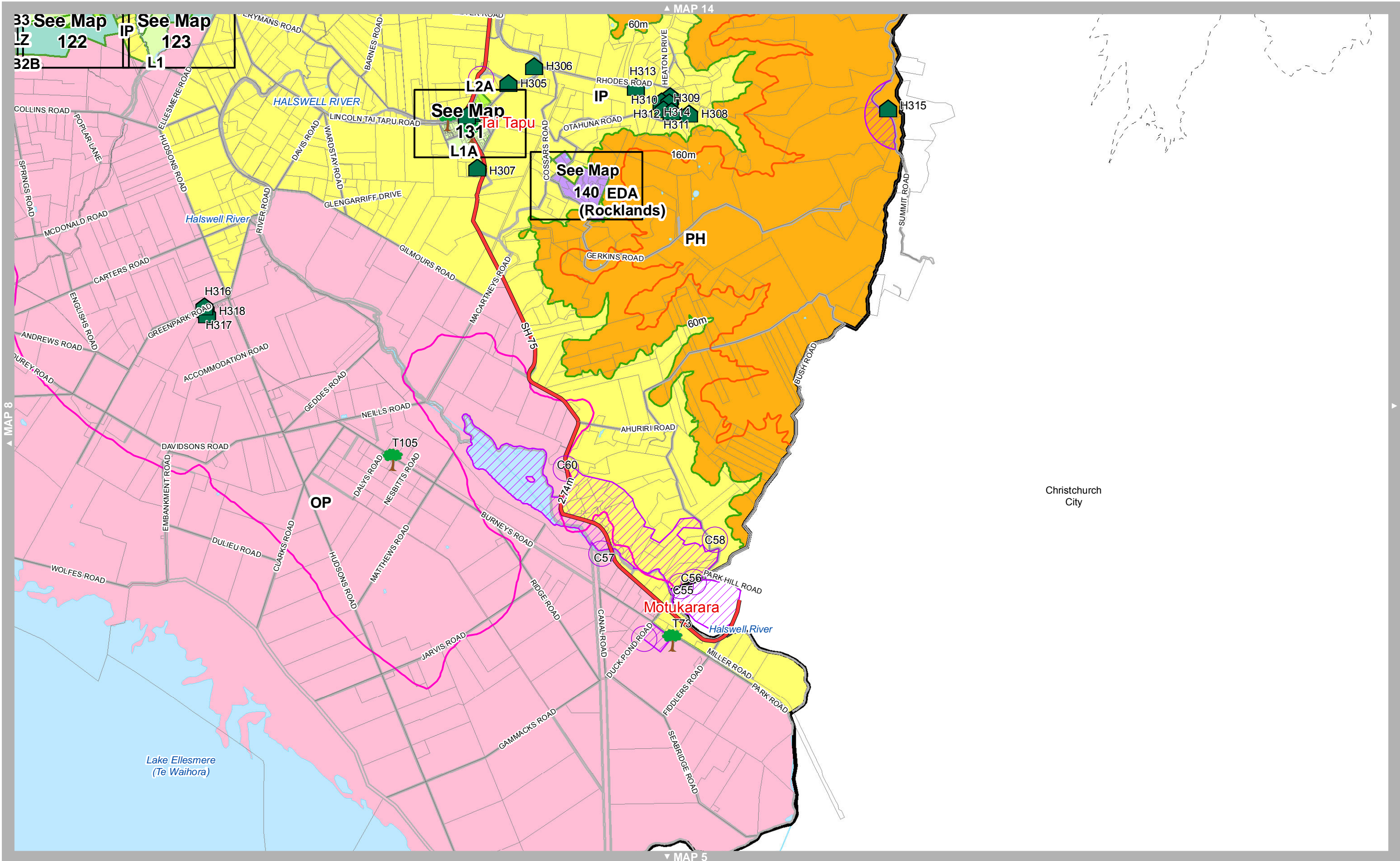
	Heritage Building		Protected Tree		Scheduled Site		State Highway		Railway		Road Parcel		Land Parcel		Water Body		Archaeological Site
	Wahi Taonga Management Site		Statutory Acknowledgement Site														
	Silent File Area																
	Wahi Taonga Management Area																
<b>Zones</b>																	
	High Country		Living 1		Living WM		Business 1		Business 2		Business 3		Inner Plains		Outer Plains		Malvern Hills
	Port Hills		Living 2		Living Z		Living Z Deferred		Deferred Living								
	Existing Development Area		Living 3		Living X												

**Contours Above Sea Level**

- 
- 
- 

**LURP Priority and Greenfield Areas Plan Change**  
**NOT YET OPERATIVE**  
 Map Made On: 8/05/2013





**KEY**

Heritage Building	Protected Tree	Scheduled Site	State Highway	Railway	Road Parcel	Land Parcel	Water Body	Archaeological Site
Wāhi Taonga Management Site	Statutory Acknowledgement Site							
Silent File Area								
Wāhi Taonga Management Area								

**Zones**

Living 1	Living WM	Business 1	Outer Plains
Living 2	Living Z	Business 2	Malvern Hills
Living 3	Living Z Deferred	Business 3	
Living X	Deferred Living	Inner Plains	

**Contours Above Sea Level**

- 160m
- 60m
- 2.74m

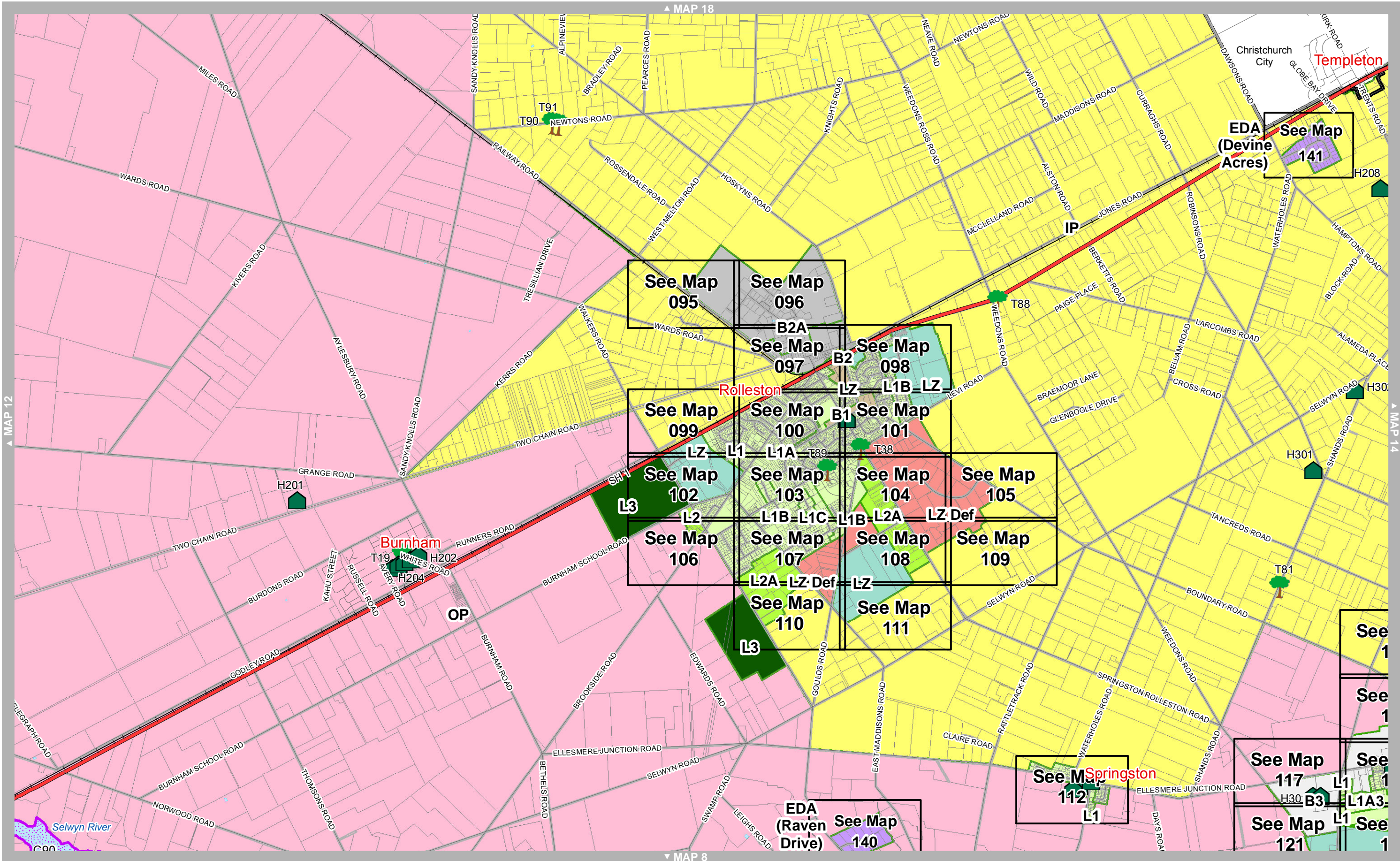
**Selwyn**  
DISTRICT COUNCIL

LURP Priority and Greenfield Areas  
Plan Change  
**NOT YET OPERATIVE**  
Map Made On: 8/05/2013

MAP  
**009**  
SHEET 2

Printed on:  
Wednesday, 8 May 2013





**KEY**


**Contours Above Sea Level**

- 160m
- 60m
- 2.74m

**Zones**


**Selwyn DISTRICT COUNCIL**

LURP Priority and Greenfield Areas Plan Change

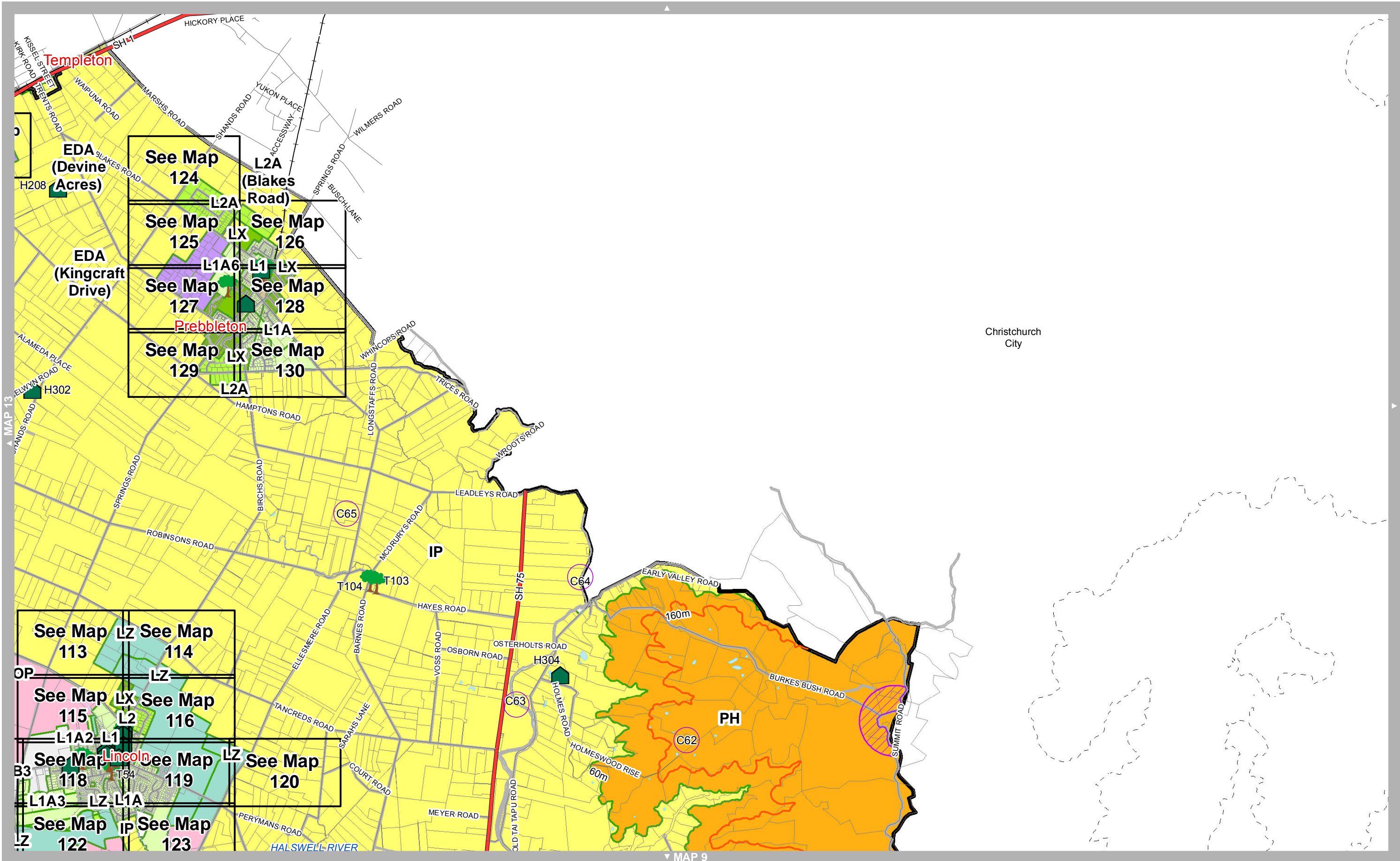
**NOT YET OPERATIVE**

Map Made On: 8/05/2013

MAP 013 SHEET 2

Printed on: Wednesday, 8 May 2013





Christchurch City

**KEY**

Heritage Building	Protected Tree	Scheduled Site	State Highway	Railway	Road Parcel	Land Parcel	Water Body	Archaeological Site
Wāhi Taonga Management Site	Statutory Acknowledgement Site							
Silent File Area								
Wāhi Taonga Management Area								

**Contours Above Sea Level**

- 160m
- 60m
- 2.74m

**Zones**

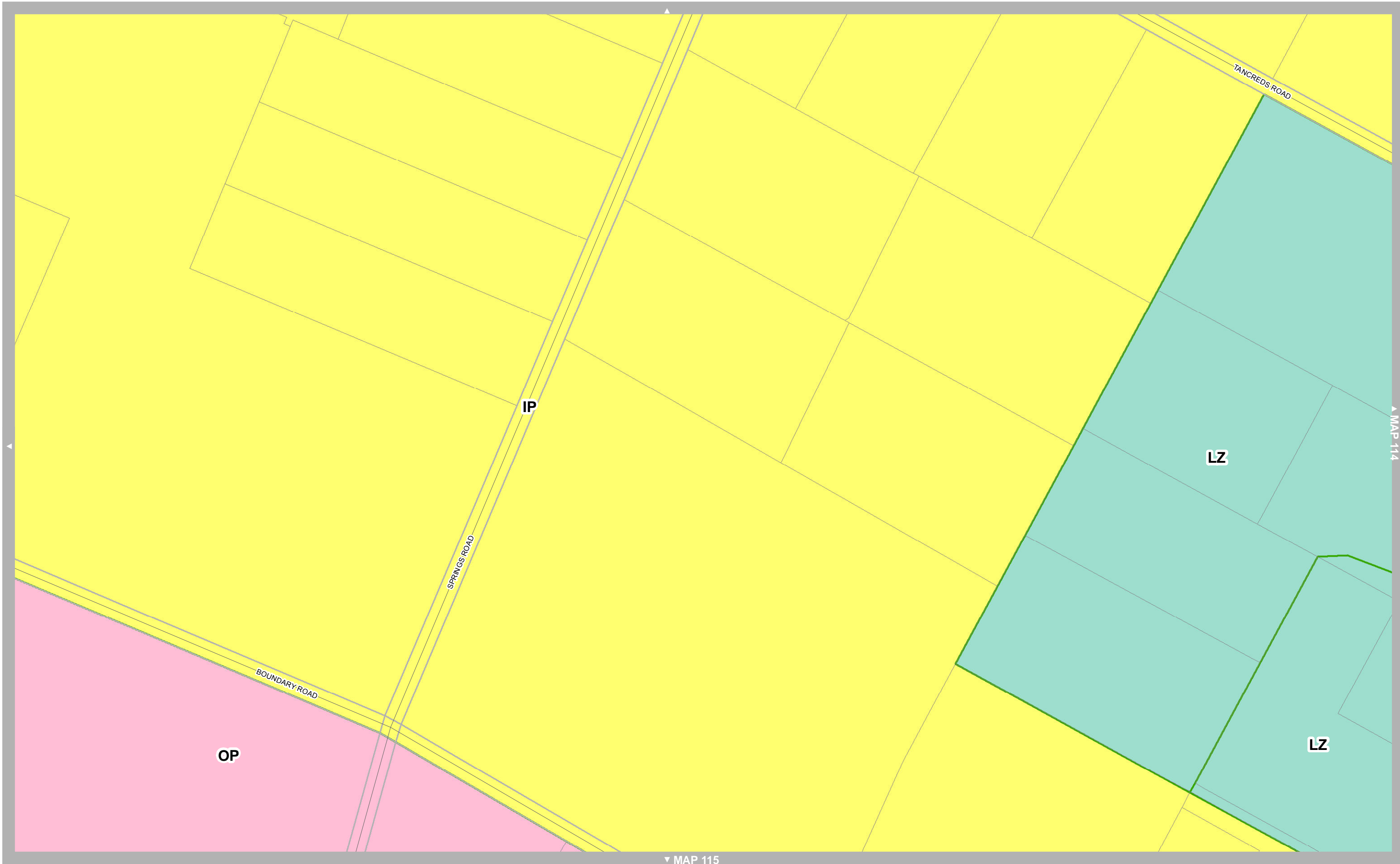
Living 1	Living WM	Business 1	Outer Plains
Living 2	Living Z	Business 2	Malvern Hills
Living 3	Living Z Deferred	Business 3	
Existing Development Area	Living X	Deferred Living	Inner Plains

**Selwyn**  
DISTRICT COUNCIL

LURP Priority and Greenfield Areas  
Plan Change  
**NOT YET OPERATIVE**  
Map Made On: 8/05/2013

MAP  
**014**  
SHEET 2

Printed on:  
Wednesday, 8 May 2013



MAP 115

MAP 114

**KEY**

Heritage Building	Protected Tree	Scheduled Site	State Highway	Railway	Road Parcel	Land Parcel	Water Body	Archaeological Site
Wāhi Taonga Management Site	Statutory Acknowledgement Site							
Silent File Area	<b>Contours Above Sea Level</b>		<b>Zones</b>		Living 1	Living WM	Business 1	Outer Plains
Wāhi Taonga Management Area	160m	High Country	Port Hills	Living 2	Living 3	Living Z	Business 2	Malvern Hills
	60m	Existing Development Area	Living X	Living Z Deferred	Deferred Living	Business 3	Inner Plains	
	2.74m							

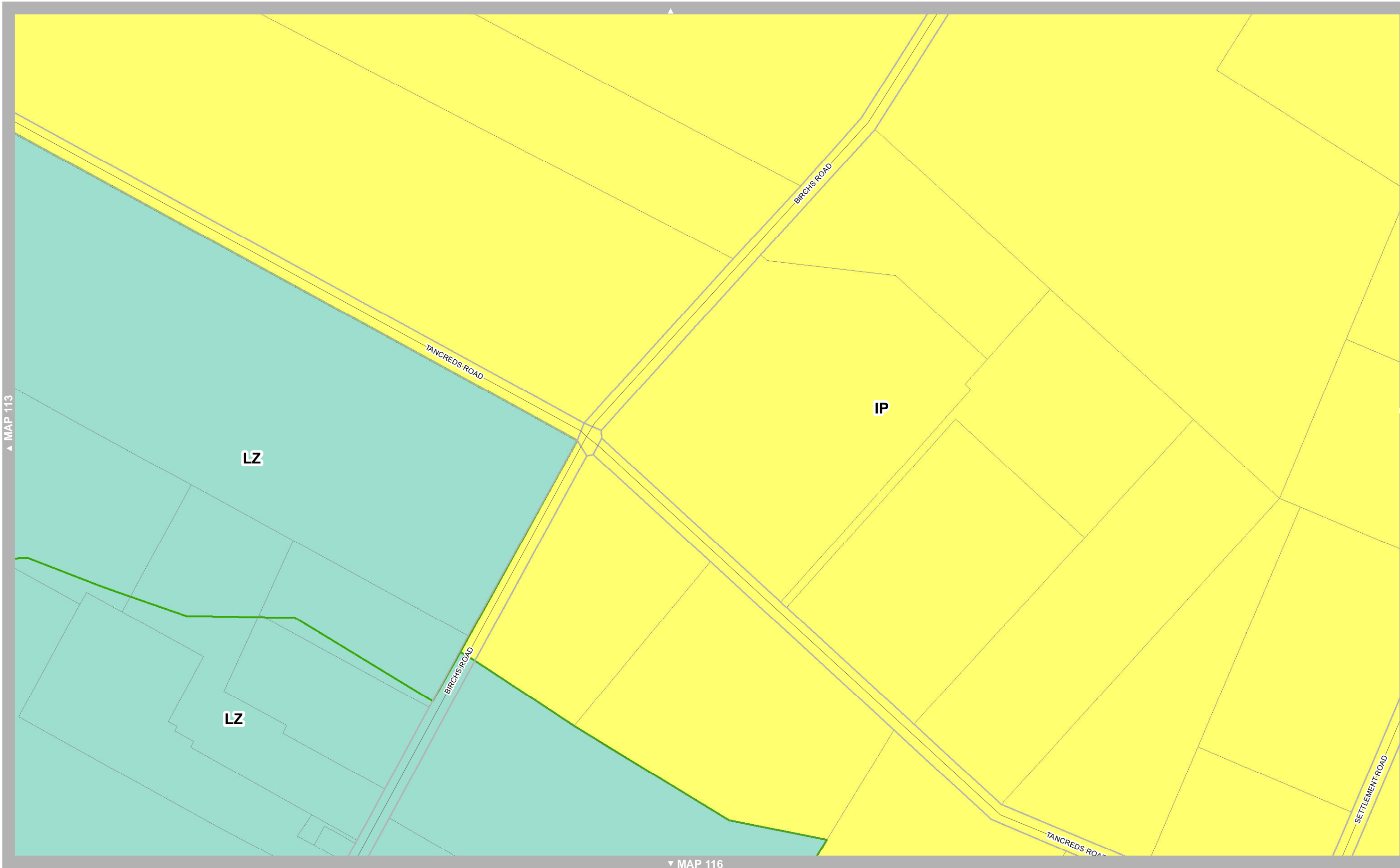


LURP Priority and Greenfield Areas  
Plan Change  
**NOT YET OPERATIVE**  
Map Made On: 8/05/2013

MAP  
**113**  
SHEET 2

Printed on:  
Wednesday, 8 May 2013





MAP 113

MAP 116

**KEY**

Heritage Building	Protected Tree	Scheduled Site	State Highway	Railway	Road Parcel	Land Parcel	Water Body	Archaeological Site	
Wāhi Taonga Management Site	Statutory Acknowledgement Site	<b>Contours Above Sea Level</b>		<b>Zones</b>		Living 1	Living WM	Business 1	Outer Plains
Silent File Area	160m	High Country	Living 2	Living Z	Business 2	Living 3	Living Z Deferred	Business 3	Malvern Hills
Wāhi Taonga Management Area	60m	Port Hills	Living X	Deferred Living	Inner Plains				
	2.74m	Existing Development Area							

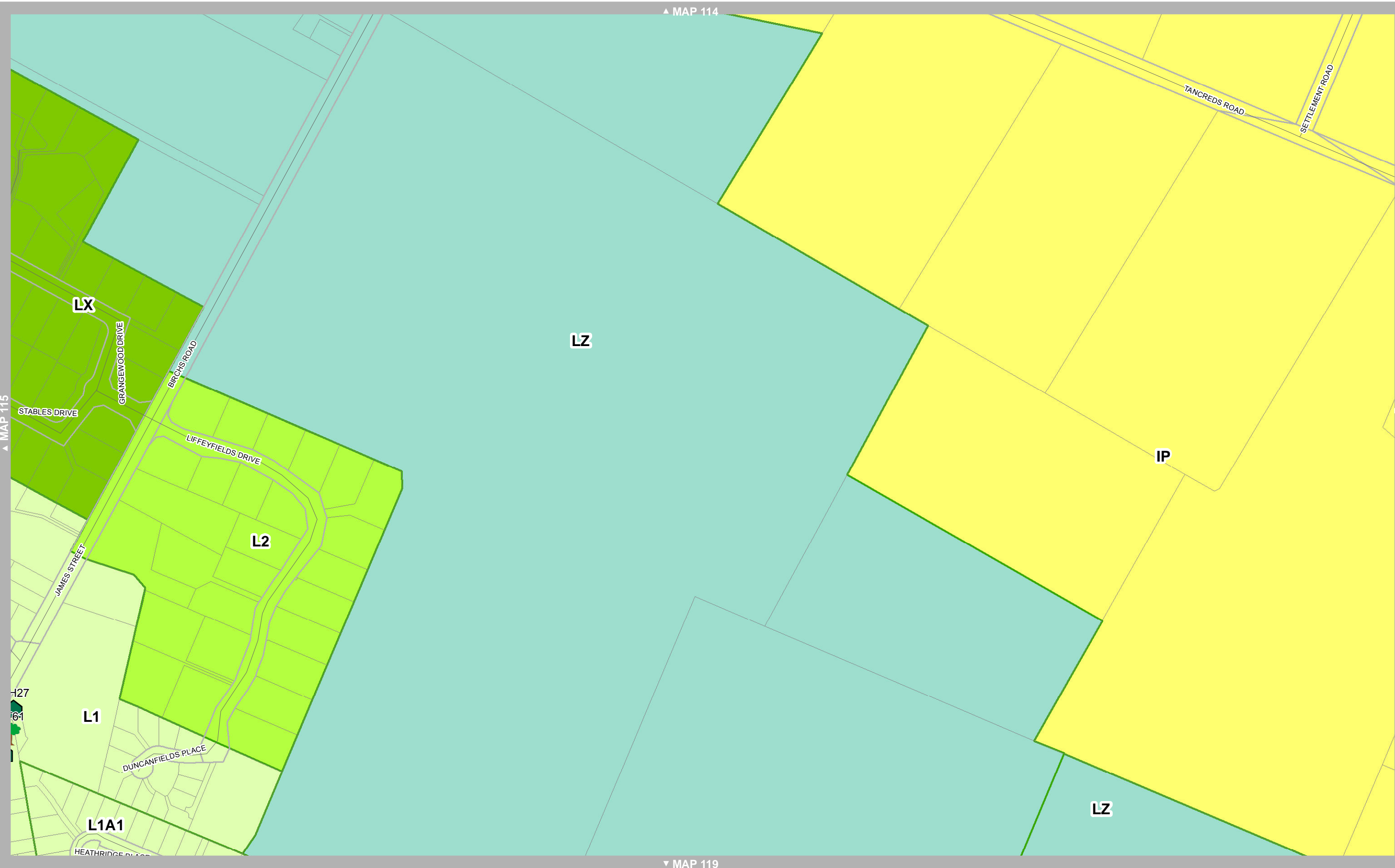


LURP Priority and Greenfield Areas  
Plan Change  
**NOT YET OPERATIVE**  
Map Made On: 8/05/2013

MAP  
**114**  
SHEET 2

Printed on:  
Wednesday, 8 May 2013





**KEY**

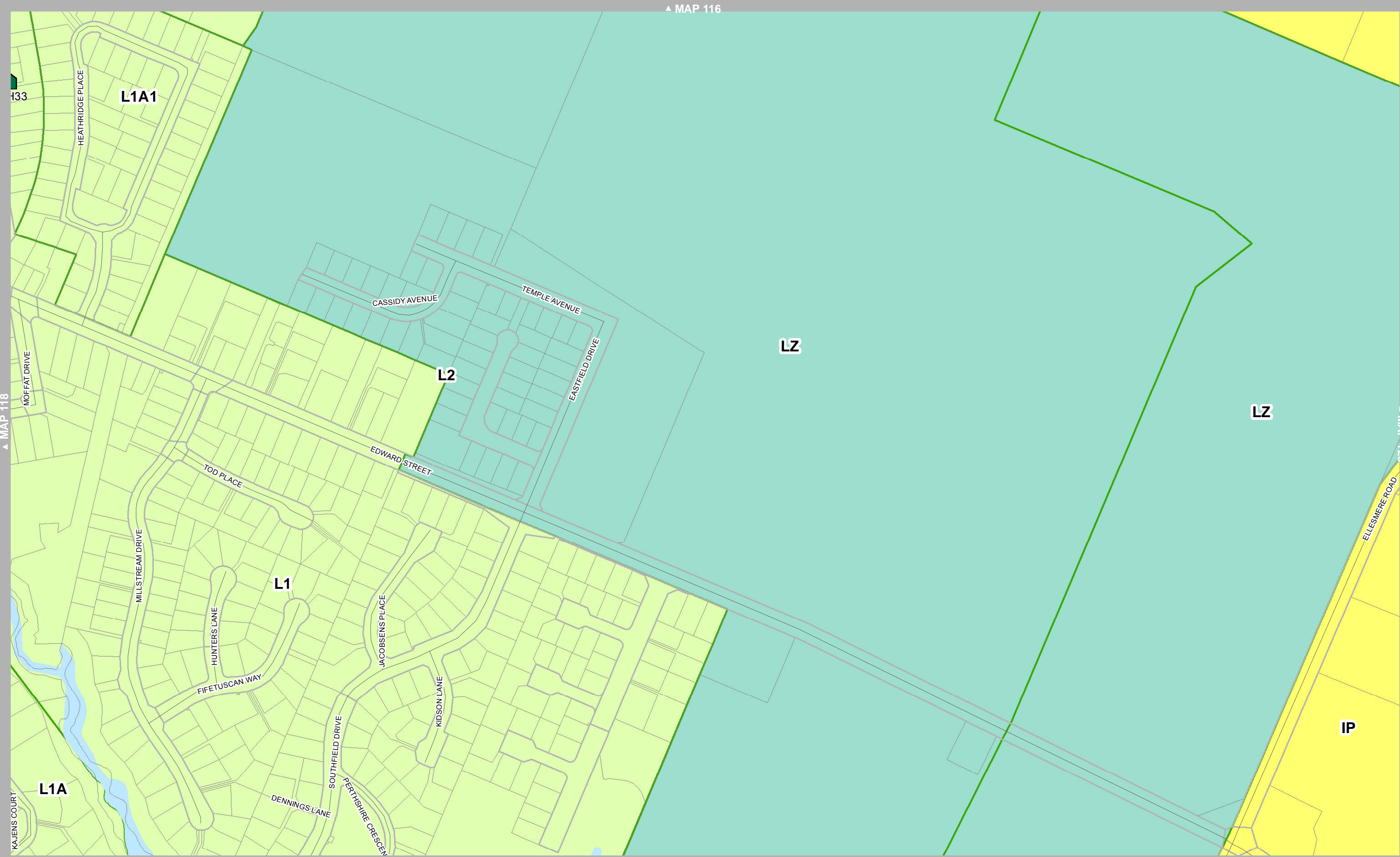
- |                                 |                                |                 |                           |              |              |             |            |                     |
|---------------------------------|--------------------------------|-----------------|---------------------------|--------------|--------------|-------------|------------|---------------------|
| Heritage Building               | Protected Tree                 | Scheduled Site  | State Highway             | Railway      | Road Parcel  | Land Parcel | Water Body | Archaeological Site |
| Wāhi Taonga Management Site     | Statutory Acknowledgement Site | <b>Zones</b>    |                           | High Country | Living 1     | Living WM   | Business 1 | Outer Plains        |
| Silent File Area                | Wāhi Taonga Management Area    | Port Hills      | Existing Development Area | Living 2     | Living 3     | Living X    | Business 2 | Malvern Hills       |
| <b>Contours Above Sea Level</b> |                                | Living Z        | Living Z Deferred         | Business 3   | Inner Plains |             |            |                     |
| 160m                            |                                | Deferred Living |                           |              |              |             |            |                     |
| 60m                             |                                |                 |                           |              |              |             |            |                     |
| 2.74m                           |                                |                 |                           |              |              |             |            |                     |



LURP Priority and Greenfield Areas  
Plan Change  
**NOT YET OPERATIVE**  
Map Made On: 8/05/2013

MAP  
**116**  
SHEET 2

Printed on:  
Wednesday, 8 May 2013



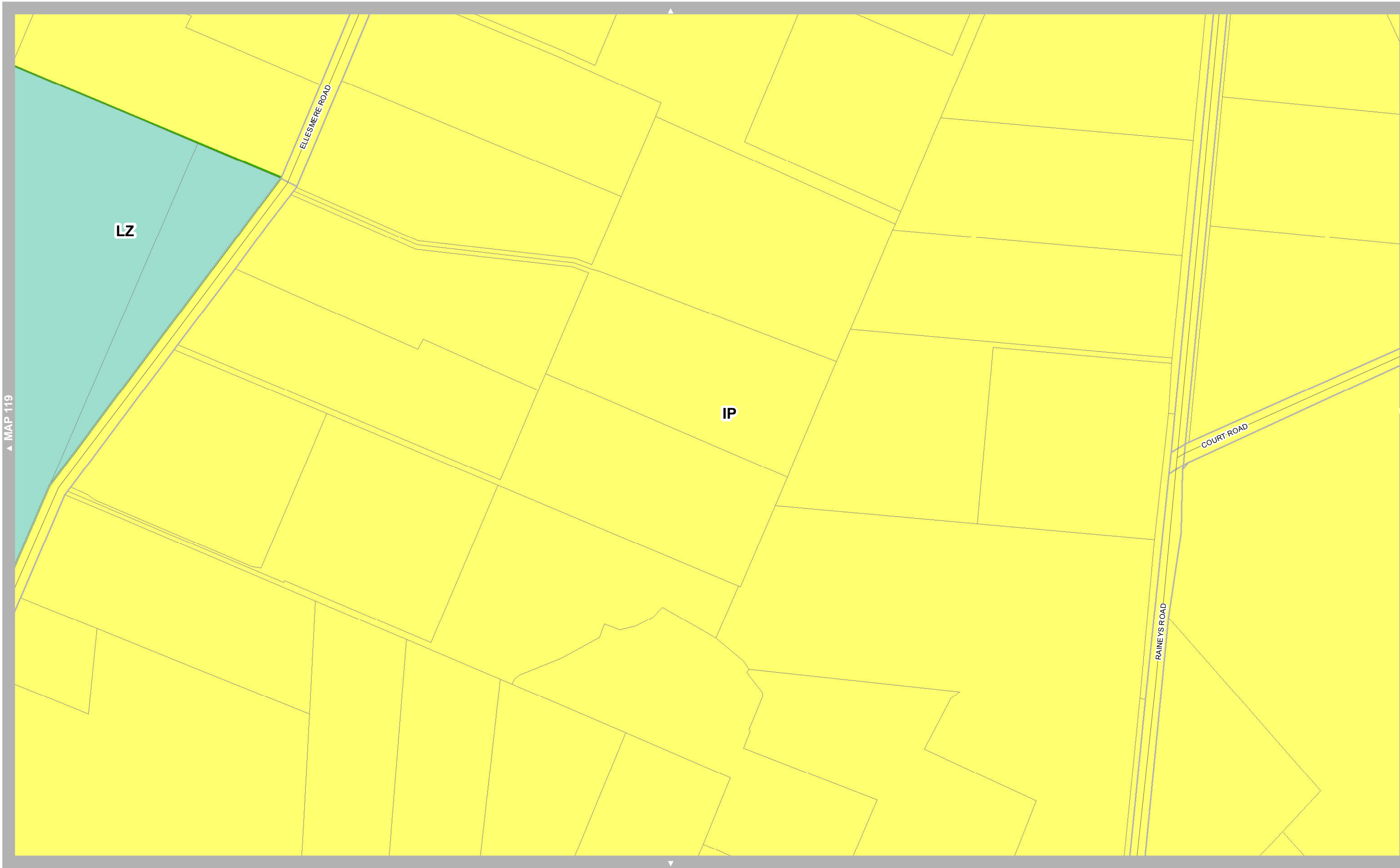
KEY

Heritage Building	Protected Tree	Scheduled Site	State Highway	Railway	Road Parcel	Land Parcel	Water Body	Archaeological Site
Wāhi Taonga Management Site	Statutory Acknowledgement Site	<b>Zones</b>		Living 1	Living WM	Business 1	Outer Plains	
Silent File Area	<b>Contours Above Sea Level</b>	High Country	Port Hills	Living 2	Living Z	Business 2	Malvern Hills	
Wāhi Taonga Management Area	160m	Existing Development Area	Living X	Living 3	Living Z Deferred	Business 3	Inner Plains	
	60m		Deferred Living					
	2.74m							



LURP Priority and Greenfield Areas  
Plan Change  
**NOT YET OPERATIVE**  
Map Made On: 8/05/2013





**KEY**

Heritage Building	Protected Tree	Scheduled Site	State Highway	Railway	Road Parcel	Land Parcel	Water Body	Archaeological Site
Wāhi Taonga Management Site	Statutory Acknowledgement Site	<b>Zones</b>		Living 1	Living WM	Business 1	Outer Plains	
Silent File Area	Wāhi Taonga Management Area	High Country	Port Hills	Living 2	Living Z	Business 2	Malvern Hills	
		Existing Development Area	Living X	Living 3	Living Z Deferred	Business 3	Inner Plains	
		Deferred Living						

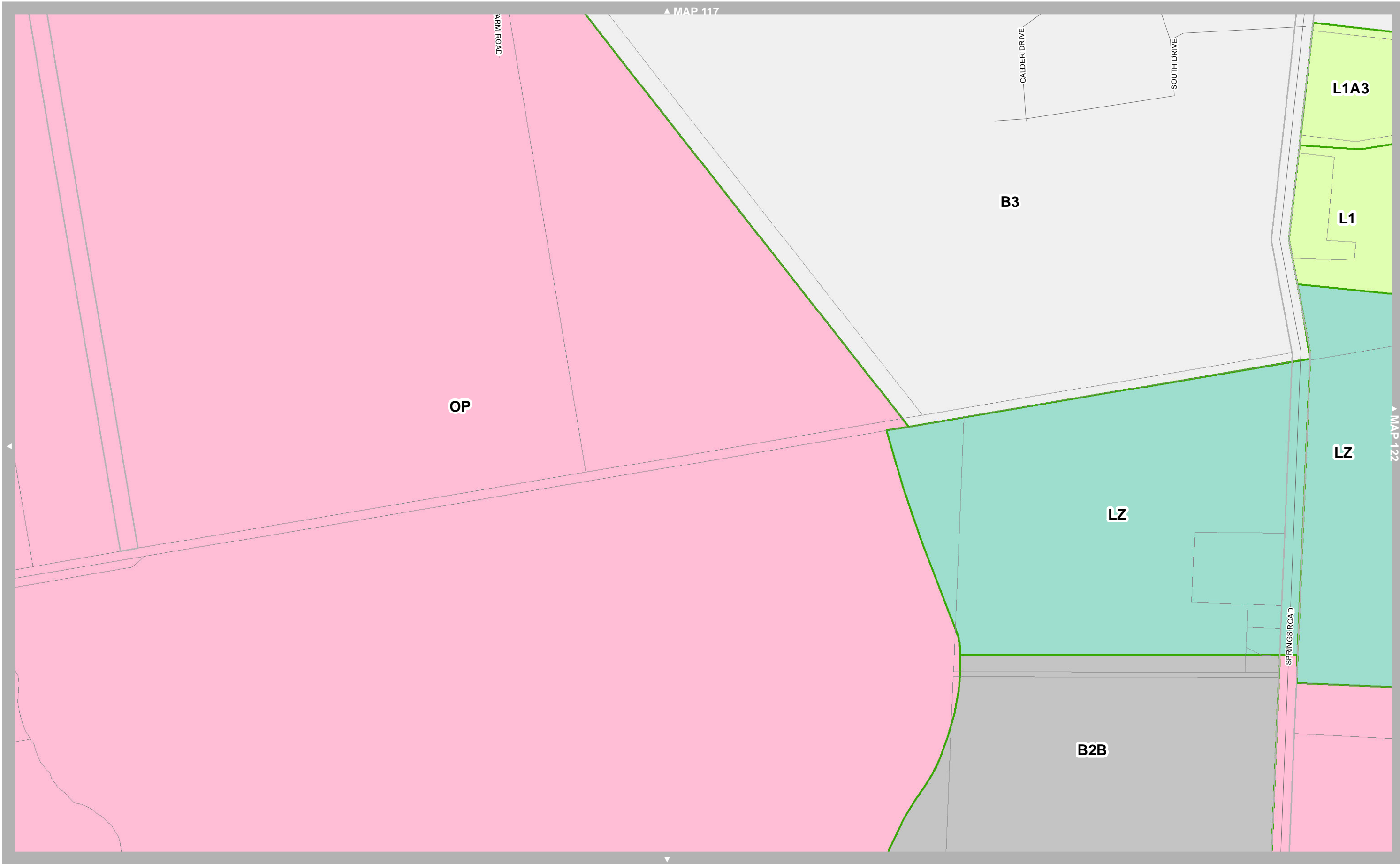
**Contours Above Sea Level**  
 160m  
 60m  
 2.74m



LURP Priority and Greenfield Areas  
 Plan Change  
**NOT YET OPERATIVE**  
 Map Made On: 8/05/2013

MAP  
**120**  
 SHEET 2

Printed on:  
 Wednesday, 8 May 2013

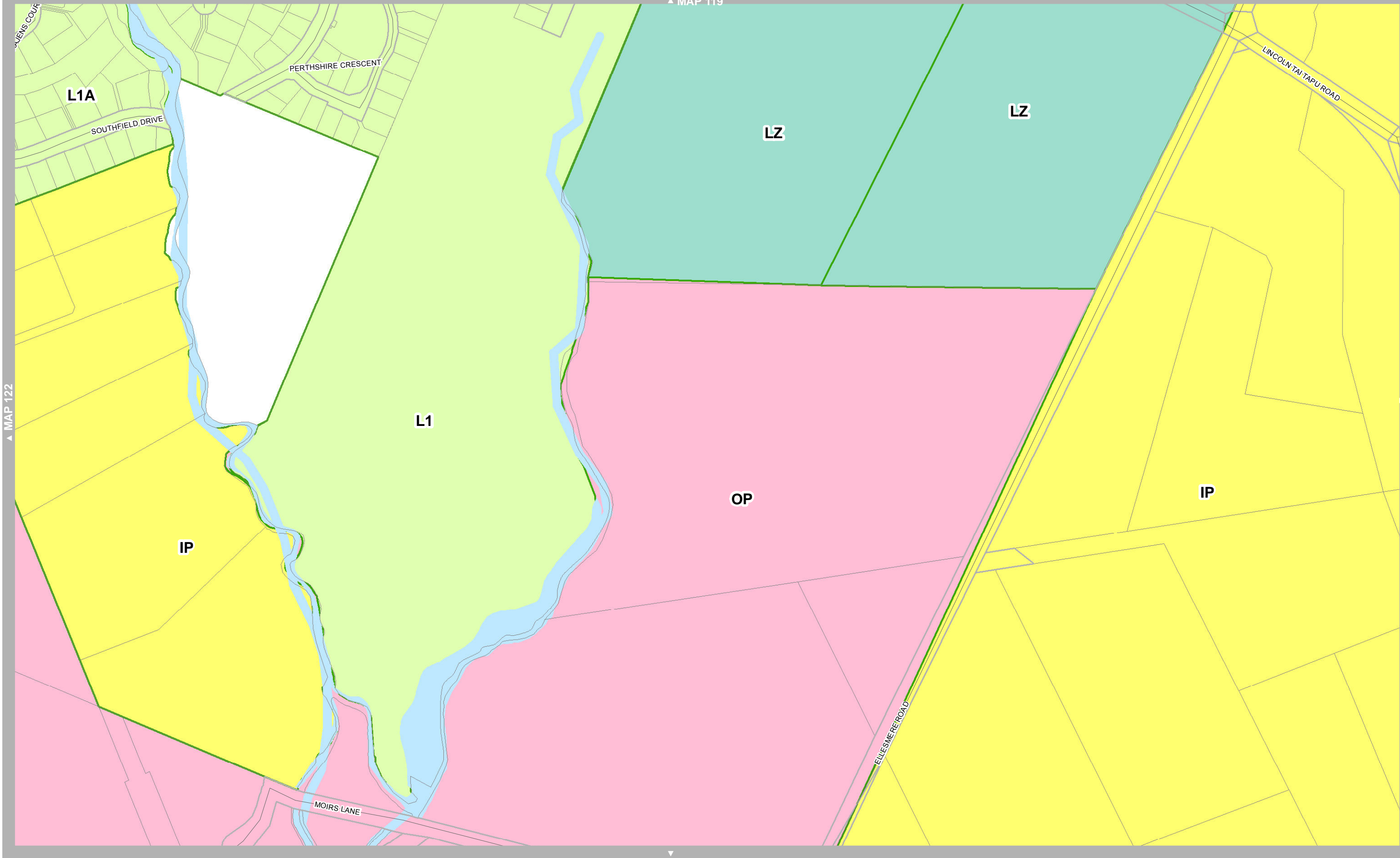


KEY

Heritage Building	Protected Tree	Scheduled Site	State Highway	Railway	Road Parcel	Land Parcel	Water Body	Archaeological Site
Wāhi Taonga Management Site	Statutory Acknowledgement Site	<b>Zones</b>		Living 1	Living WM	Business 1	Outer Plains	
Silent File Area	<b>Contours Above Sea Level</b>	High Country	Living 2	Living Z	Business 2	Malvern Hills		
Wāhi Taonga Management Area	160m	Port Hills	Living 3	Living Z Deferred	Business 3			
	60m	Existing Development Area	Living X	Deferred Living	Inner Plains			
	2.74m							



LURP Priority and Greenfield Areas  
Plan Change  
**NOT YET OPERATIVE**  
Map Made On: 8/05/2013



**KEY**

Heritage Building	Protected Tree	Scheduled Site	State Highway	Railway	Road Parcel	Land Parcel	Water Body	Archaeological Site
Wāhi Taonga Management Site	Statutory Acknowledgement Site							
Silent File Area								
Wāhi Taonga Management Area								

**Contours Above Sea Level**

- 160m
- 60m
- 2.74m

**Zones**

High Country	Living 1	Living WM	Business 1	Outer Plains
Port Hills	Living 2	Living Z	Business 2	Malvern Hills
Existing Development Area	Living 3	Living Z Deferred	Business 3	
	Living X	Deferred Living	Inner Plains	



LURP Priority and Greenfield Areas  
Plan Change  
**NOT YET OPERATIVE**  
Map Made On: 8/05/2013

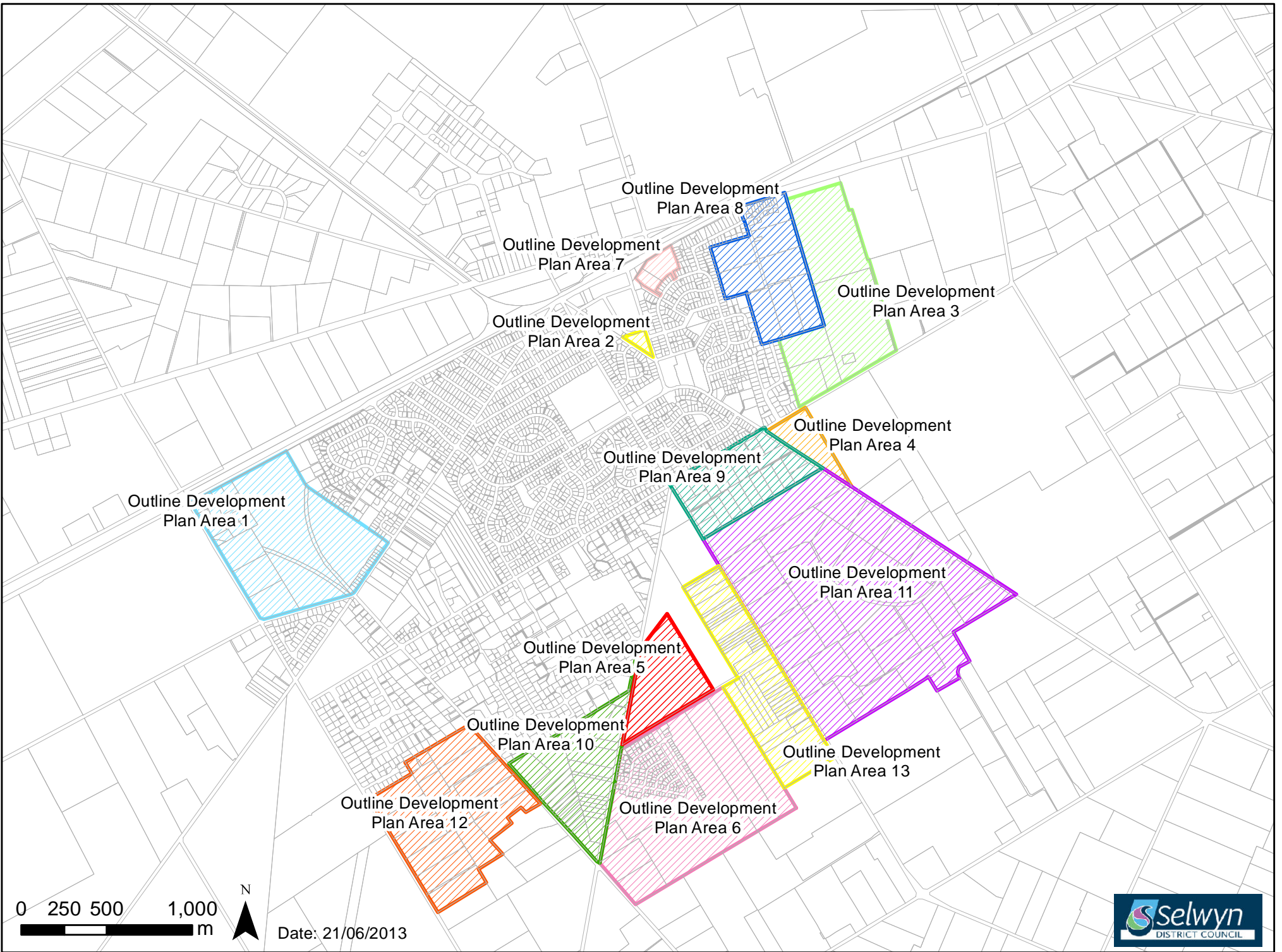




## **Appendix 6: Outline Development Plan Areas for Rolleston**

*Note: This Appendix indicates the location of the Outline Development Plans indicated in Action 18 of the Land Use Recovery Plan. For further information please contact the Strategy and Policy Team of the Selwyn District Council.”*

# Outline Development Areas for Rolleston



Date: 21/06/2013

